

MAYOR
Timothy Woolley

Cynthia A. Bower
CLERK

Nicone Dragone Sr.
TREASURER

CITY OF TAYLOR



*23555 Goddard Road
Taylor, MI 48180
(734) 287 6550
www.cityoftaylor.com*

COUNCIL
Charley Johnson
CHAIRMAN

Ron Thiede
CHAIR PRO-TEM

*Christian Armstrong
Chris Clark
William Patts
Gerald P. Thomas
Dan Wallace*

The Regular Council Meeting of the Taylor City Council will be held on Tuesday April 07, 2026 in the Taylor Municipal Building at 23555 Goddard Rd., Taylor, MI to begin promptly at 6:30 PM

AGENDA

1. Call to order
2. Pledge of Allegiance
3. Roll Call
4. **AGENDA AND MINUTES**
 - 4.1 Motion to approve the Agenda.
 - 4.2 Motion to approve the acceptance of the minutes of the meeting held March 17, 2026 and dispense with oral reading.
[2026_03_17 Final.pdf](#)
5. **MAYOR COMMUNICATIONS**
 - 5.1 Motion to approve the tribute to honor Ron Thiede by State Representative Jamie Thompson.
 - 5.2 Motion to approve Personal Service Contracts for the Fire Chief and the Deputy Fire Chief
6. **REGULAR AGENDA**
 - 6.1 Motion to approve City of Taylor budget amendment 2026-005 dated April 7,2026
[Amendment 2026-005.pdf](#)
 - 6.2 Motion to approve KCI Printing for the final postage and processing costs associated with mailing the 2026 Assessment Change Notices for Real Property, in an amount not to exceed \$3,237. Funded through General Fund.
[Agenda Cover Letter-Final Payment ACN Processing-Postage Check Approval.doc](#)
[KCI Invoice Taylor - ACN final billing \(1\).pdf](#)
[2026 ACN Final Billing.pdf](#)
[2026 Final KCI billing.pdf](#)

[2026 Summary of Charges.pdf](#)

- 6.3 Motion to amend CCR#2.51-26 to increase authority of Magnet Forensics for the Advanced AXIOM Software by \$2,255. Funded through General Fund Police IT.
[CCR#2.51-26.pdf](#)
[Magnet AXIOM Advancefd Software Bundle Quote.pdf](#)
- 6.4 Motion to receive & file the first reading of Case #26-03(ZC) - 8715 Telegraph- An ordinance to amend Appendix A of the City of Taylor, Michigan Code of Ordinances, regulating the development and use of land to conditionally rezone parcel #60-027-99-0004-004 of the SW ¼ Section 9, T.3S., R.10E., City of Taylor, Wayne County, Michigan from B-3(General Business) to I-1(Light Industrial). The property consists of approximately 2.25 acres and is located on the east side of Telegraph Road between Wohifiel and Mary streets.
[26.01.12 - Rezoning Application - 8715 Telegraph.pdf](#)
[26.01.12_Taylor Conditional Rezoning Request.pdf](#)
[P16637 - Designhaus 8715 Telegraph Trip Gen FINAL Proposal and PSA.pdf](#)
[Revised ZC agreement 8715 Telegraph.pdf](#)
[Planning Consultant's rec..pdf](#)
[March 4, 2026 MINUTES.pdf](#)
[8715 -previously approved site plan.pdf](#)
- 6.5 Motion to approve the second reading of Case #26-02(ZC) - the southern approximately 881 feet of Parcels #60-48-99-0013-000 and #60-048-99-0014-000 of the SE 1/4 Section 18, T.3S., R.10E, City of Taylor, Wayne County, Michigan to be conditionally rezoned from O-1/R-1C (Office/Single Family Residential) to I-1 (Light Industrial). The property is located on the north side of Goddard Road Between Beech Daly and Continental Drive. This is a conditional (contract) rezoning.
[Case # 26-02 ZC Proof of Own 2.pdf](#)
[Case # 26-02 ZC 300 ft. mailing.pdf](#)
[Case # 26-02 ZC Legal Publication.pdf](#)
[Case # 26-02 ZC Plan Drawings.pdf](#)
[Case # 26-02 ZC Recommendation Letter.pdf](#)
[Case # 26-02 ZC Resident Notice.pdf](#)
[Case # 26-02 ZC Traffic Study.pdf](#)
[February 18, 2026.pdf](#)
- 6.6 Motion to approve McKenna Associates, Planning Consultant, preferred vendor, to provide professional services to the Planning Department on an as needed basis.
[McKenna fee update.pdf](#)
[Fees.pdf](#)
[McKenna Invoice.pdf](#)
- 6.7 Motion to approve ProPump and Controls, Inc., sole source bid, for the replacement of golf course irrigation pumps and piping at Taylor Meadows Golf Club for an amount not to exceed \$17,328. Funded through Golf Course, Capital Outlay Account.
[Taylor Meadows Q010799 \(1\).pdf](#)
[Taylor Meadows Q011134 \(2\).pdf](#)
[Taylor Meadows Pump house work \(1\).pdf](#)

[Sole source emergency Golf 32426.pdf](#)

- 6.8 Motion to approve 2026 Summer green fee rates at Lakes of Taylor and Taylor Meadows Golf Courses.
[scan_agrant_2026-03-28-09-07-13.pdf](#)
[2025 rates workbook.pdf](#)
- 6.9 Motion to approve Water Landscapes, preferred vendor, to install new control panel on Golf course fountain for an amount not to exceed \$3,120. Funded through Golf Course, Lakes Operating Supplies.
[Blanket PO Approval Form_FY26_LAKES.pdf](#)
[Invoice_10108_from_Water_Landscapes_LLC \(1\).pdf](#)
- 6.10 Motion to amend CCR# 7.295-25 Taylor Sportsplex purchase agreement for Be Well My Friends to My Friends DJ Service LLC.
- 6.11 Motion to approve
CCR# 9.430-25 increase authority an additional \$65,218 for required power and CAT6 installation. Funded through TBA Capital Outlay.

[2025_09_16 Draft \(3\).pdf](#)
[CO1_COT_120V & Cat6.pdf](#)
- 6.12 Motion to approve Cornerstone Fence, low quote, for installation of fencing at the Splash Pad and Pavilion C at Heritage Park for an amount not to exceed \$20,136. Funded through TBA Capital Outlay.
[Splash Pad Fence Quotes Summary.pdf](#)
[Fence Quotes.pdf](#)
[Pavilion C Fence Quotes Summary.pdf](#)
- 6.13 Motion to approve Wayne Lawn & Garden, low quote, for two (2) lawnmowers for the DPW, in an amount not to exceed \$ 36,999. Funded through Act 48 Right of Way.
[lawnmower quotes](#)
- 6.14 Motion to approve American Generator Sales and Service LLC for the emergency rental of generator for the Animal Shelter for an amount not to exceed \$23,661. Funded through Act 179 Contractual Services.
[Email From Parks and Rec Director.pdf](#)
- 6.15 Motion to approve American Generators Sales and Service LLC, current vendor, for a replacement generator for the animal Shelter in an amount not to exceed \$21,923. Funded through Act 179 Capital Outlay
[Email From Building and Grounds Director](#)
[Email From Building and Grounds Director.pdf](#)
- 6.16 Motion to approve authorizing the City Clerk to sign the resolution to remove roads from the Act 51 Map, per the attached document.

[MDOT 2008B form.pdf](#)
[MDOT Annual Road Mileage Resolution](#)

- 6.17 Motion to amend CCR #7.288-25 Purchase Agreement for Compost Site for Fiscal Year 2025/2026
[VM PA 2025-2026.pdf](#)

- 6.18 Motion to approve

Al's Asphalt Paving Company, current vendor to perform asphalt pavement resurfacing contract work in an amount not to exceed \$ 4,870,000.

Funded through Local Street Fund, Major Street Fund, Taylor Building Authority (TBA), and Michigan Economic Development Corporation Grant (MEDC Grant)

[Taylor Compliance Plan \(TAMP\) Final \(Executed & Submitted\) 01-23-2025 \(121 Pages\) CLG.pdf](#)

- 6.19 Motion to approve

Audia Concrete Construction, Inc, current vendor, to perform concrete panel replacement contract work in an amount not to exceed \$1,765,000.

Funded through Local Street Fund, Major Street Fund, and Taylor Building Authority (TBA).

[Taylor Compliance Plan \(TAMP\) Final \(Executed & Submitted\) 01-23-2025 \(121 Pages\) CLG.pdf](#)

- 6.20 Motion to approve payment to State of Michigan for the City of Taylor MS4 NPDES Permit 2026 Annual Fee, for an amount now to exceed \$5,000. Funded through Major Road Fund.
[State of Michigan EGLE MS4 Annual Permit Fee \(2026\).pdf](#)

- 6.21 Motion to approve to direct Corporation Counsel to draft a ballot proposal to amend the City of Taylor Charter, Chapter 15, Section 15.2, Section 15.3 and Section 15.4(a), per the attached documents.

[Threshold_Recomendation.pdf](#)
[Ballot_Proposal_Motion_Budget_Limitations.pdf](#)

- 6.22 Motion to approve the adoption of the Resolution to oppose House Bills 5529-5532 and 5581-5585.

[Resolution Opposing House Bills 4-1-26.docx](#)

7. **OPEN BUSINESS**

8. **ADJOURNMENT**

Motion By: Gerald Thomas **Supported By:** Christian Armstrong
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve the appointment of Dennis Herwig to the Taylor Veterans Museum
Commission for another four (4) year term to expire March 17, 2030.

Unanimously carried
CCR #: 3.110-26

Motion By: Chris Clark **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To receive & file the City of Taylor Early Voting Plan as approved by the Election
Commission on March 11, 2026.

Unanimously carried
CCR #: 3.111-26

Motion By: Chris Clark **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve the attendance of two (2) Clerk's Office employees at the 2026 Michigan
Association of Municipal Clerks Spring Education Day in the amount not to exceed \$575
plus out of pocket expenses, funded through General Fund, Clerk's Office Training &
Transportation.

Unanimously carried
CCR #: 3.112-26

Motion By: Chris Clark **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve the attendance of two (2) City Clerk's Office employees at the 2026 Michigan
Association of Municipal Clerks Summer Conference in the amount not to exceed \$3,100
plus out of pocket expenses, funded through General Fund, Clerk's Office, Training &
Transportation.

Unanimously carried
CCR #: 3.113-26

Motion By: Chris Clark **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve sending one (1) Building Department employee for an overnight stay to attend the Spring Code Officials Conference of Michigan (COCM) for an amount not to exceed \$1,100, plus incidental expenses. Funded through Building Department Training and Transportation account.

Unanimously carried
CCR #: 3.114-26

Motion By: Chris Clark **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To receive & file the February 2026 Budget Performance Report.

Unanimously carried
CCR #: 3.115-26

Motion By: Chris Clark **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To receive & file the February 2026 Paid Invoice Report.

Unanimously carried
CCR #: 3.116-26

Motion By: Dan Wallace **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve going into closed session at 6:35p.m. to discuss pending litigation.

Roll Call Vote

Unanimously carried
CCR #: 3.117-26

Reconvened at 7:14 p.m.

Motion By: William Patts **Supported By:** Chris Clark
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve settlement of Risk Management claim QL-1278, as recommended by the attorney in closed session.

Roll Call Vote

Unanimously carried
CCR #: 3.118-26

Motion By: William Patts **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To receive & file the first reading of Case #26-02(Zc)- the southern approximately feet of parcels #60-048-99-0013-000 and #60-048-99-0014-000 of the SE ¼ Section 18, T.3S., R.10E., City of Taylor, Wayne County, Michigan to be conditionally rezoned from O-1/R-1C (Office/Single Family Residential) to I-1 (Light Industrial). The property is located on the north side of Goddard Road between Beech Daly and Continental Drive. This is a conditional (contract) rezoning.

Unanimously carried
CCR #: 3.119-26

Motion By: Christian Armstrong **Supported By:** Dan Wallace
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve Cynergy Products, sole source, to install five (5) docking stations, pedestals and five (5) 360 lighting packages on five (5) new Building Department vehicles for an amount not to exceed \$21,320, funded through Building Department, Vehicle Account.

Unanimously carried
CCR #: 3.120-26

Motion By: Gerald Thomas **Supported By:** Christian Armstrong
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve compensating Kery Heating, Cooling, and Electrical, and other contractual Building, Electrical, Plumbing and Mechanical Inspectors designated by the Building Official, at the rate not to exceed \$45 an inspection. Funded through Building Department, Contractual Services.

Unanimously carried
CCR #: 3.121-26

Motion By: Gerald Thomas **Supported By:** Chris Clark
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve payment to Michigan Law Enforcement Training Associates, LLC., for providing the CPE required Constitutional Policing Trainings to the Taylor Police Officers, in an amount not to exceed \$38,340, funded through Continuing Professional Education Training Fund.

Unanimously carried
CCR #: 3.122-26

Motion By: Gerald Thomas **Supported By:** Chris Clark
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve a member of Taylor Fire Department to attend the Explosive Handler & Breaching Course in June 2026.

Unanimously carried
CCR #: 3.123-26

Motion By: Christian Armstrong **Supported By:** Dan Wallace
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve payment to West Shore for the Annual Inspection and Preventative Maintenance Agreement on the Warning System Alert Sirens, in the amount of \$6,000 funded through General Fund Police Repair and Maintenance and Fire Repair and Maintenance.

Unanimously carried
CCR #: 3.124-26

Motion By: William Patts **Supported By:** Christian Armstrong
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve sending one (1) K -9 officer and K-9 to attend the Explosive K-9 Recertification Training in Clare, MI for a cost not to exceed \$835, funded through General Fund Police Training.

Unanimously carried
CCR #: 3.125-26

Motion By: Gerald Thomas **Supported By:** Dan Wallace
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve payment to Lafontaine Chrysler, State Contract, for MNET vehicle purchase, in the amount of \$35,876, funded through MNET portion of Federal Forfeiture Funds.

Unanimously carried
CCR #: 3.126-26

Motion By: Christian Armstrong **Supported By:** William Patts
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede Christian
Armstrong, Gerald Thomas
Nays: None
Resolved: To approve sending two (2) officers to attend the Cellular Analysis Survey Team Training
in Traverse City, for a cost not to exceed \$1,092, funded through General Fund Police
Training.

Unanimously carried
CCR #: 3.127-26

Motion By: Christian Armstrong **Supported By:** Chris Clark
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve authorizing the Mayor to sign the Purchase Agreement on behalf of the City of
Taylor for the sale of Parcel ID #60 -081-03-0001-300 for the total amount of \$11,000,
plus closing costs. Proceeds go to the General Fund.

Unanimously carried
CCR #: 3.128-26

Motion By: Gerald Thomas **Supported By:** Christian Armstrong
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve authorizing the Mayor to sign the Purchase Agreement on behalf of the City of
Taylor for the sale of Parcel ID #60 -037-01-0017-000 for the total amount of \$10,000,
plus closing costs. Proceeds go to the General Fund.

Unanimously carried
CCR #: 3.129-26

Motion By: Christian Armstrong **Supported By:** William Patts
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve the Mayor to sign and enter into subrecipient agreements per the approved
2025-2026 Community Development Block Grant (CDBG) Action Plan/Budget in the
amount of \$12,500 each for Fish & Loaves, The Guidance Center and the Senior Alliance.
Funded through CDBG.

Unanimously carried
CCR #: 3.130-26

Motion By: Gerald Thomas **Supported By:** Dan Wallace
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To receive & file the Board of Ethics Resolution, per the attached document.
Interested Parties: M. Hargaves
Unanimously carried
CCR #: 3.131-26

Motion By: Dan Wallace **Supported By:** Chris Clark
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede,
Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve adjournment at 7:36 p.m.
Unanimously carried
CCR #: 3.132-26

Charley Johnson, Chairman

Cynthia A. Bower, City Clerk

FY26 Budget Amendment 2026-005, dated 4/7/26

	<u>FROM</u>	<u>TO</u>	<u>INCREASE (DECREASE)</u>	<u>Justification</u>
202- Major Road Fund				
Revenue Changes:				
	-	-	-	
Total Revenue Changes	-	-	-	
Total Fund Revenue	\$ 6,137,569.00	\$ 6,137,569.00	\$ -	
Appropriation Changes:				
202-966-995-000-000-203 Transfer to Local Road Fund	3,037,784.00	3,712,784.00	675,000.00	Transfer additional funds to Local Road Fund
Total Appropriation Changes:	3,037,784.00	3,712,784.00	675,000.00	
Total Fund Appropriations	\$ 7,297,484.00	\$ 7,972,484.00	\$ 675,000.00	
Net Change in Fund Balance	(1,159,915.00)	(1,834,915.00)	(675,000.00)	
Estimated Beginning Fund Balance	3,486,288.00	3,486,288.00	-	
Estimated Ending Fund Balance	2,326,373.00	1,651,373.00	(675,000.00)	
% of Expenditures	31.9%	20.7%		

FY26 Budget Amendment 2026-005, dated 4/7/26

	<u>FROM</u>	<u>TO</u>	<u>INCREASE (DECREASE)</u>	<u>Justification</u>
203- Local Road Fund				
Revenue Changes:				
203-000-699-000-000-202 Transfer from Major Street Fund	3,037,784.00	3,712,784.00	675,000.00	Record Transfer from Major Road Fund
Total Revenue Changes	3,037,784.00	3,712,784.00	675,000.00	
Total Fund Revenue	\$ 7,438,420.00	\$ 8,113,420.00	\$ 675,000.00	
Appropriation Changes:				
203-460-934-180-000-000 Repair and Maint. - Asphalt	2,300,000.00	2,725,000.00	425,000.00	Additional Road improvements being considered at current meeting
203-460-934-181-000-000 Repair and Maint. - Concrete	1,065,000.00	1,315,000.00	250,000.00	Additional Road improvements being considered at current meeting
Total Appropriation Changes:	3,365,000.00	4,040,000.00	675,000.00	
Total Fund Appropriations	\$ 8,555,000.00	\$ 9,230,000.00	\$ 675,000.00	
Net Change in Fund Balance	(1,116,580.00)	(1,116,580.00)	-	
Estimated Beginning Fund Balance	2,662,538.00	2,662,538.00	-	
Estimated Ending Fund Balance	1,545,958.00	1,545,958.00	-	
% of Expenditures	18.1%	16.7%		



3901 East Paris SE
Grand Rapids, MI 49512
616.957.2120 phone
616.957.3026 fax
kentcommunications.com

WCA Assessing
38110 Executive Dr, Ste 200
Westland, MI 48185

Invoice: 355464, 355682, 355719, 355738, 355737, 356004, 356289, 356344, 356392, 356757

Description of Work: **Final billing for 2026 mailing of Assessment Change Notices
Printing & Processing – Real & Personal**

Municipality: City of Taylor
Qty: 23,500 Parcels
Total Due: \$3,236.66
Due Date: On or Before 4/15/2026

Make a separate check payable to: KCI
Memo Line: Final ACN Billing

Mail to: WCA, Attn: Tonya Shea, 38110 Executive Dr N, Ste 200, Westland, MI, 48185

***DO NOT COMBINE THIS INVOICE WITH ANY OTHER
INVOICE***

Final Billing - Assessment Change Notices	KCI Set up Charges:			Real Property		Personal Property		POSTAGE CALCULATION for Real & Personal Property Mailing						FINAL AMOUNT DUE
	A: Set up fee - total all invoices \$2,500.00	B: AN Version Change - total all invoices \$1,400.00	C: USPS Shared Incentive \$7,124.73	Parcel Count:	D: Process/printing charge Avg rate per piece \$55,434.12	Parcel Count:	E: Process/printing charge \$1,647.80	Parcel Counts:		Pre-paid rate estimated at .593	USPS Avg Rate per piece .5501	F: Postage adjustment	G: Other:	
	\$50.00 per unit	\$28.00 per unit	\$142.4946 unit		0.14		0.14	Estimated Parcel Count	Actual Parcel Count	Prepaid: \$235,269.79	Actual: \$229,099.21	Additional due/credit	Credits / adjustments	
Albion	\$50.00	\$28.00	\$142.4946	2,924	\$409.36	95	\$13.30	2,900	3,019	\$1,719.70	\$1,696.352	-\$23.35	\$0.00	\$619.81
Ann Arbor Twp	\$50.00	\$28.00	\$142.4946	1,847	\$258.58	64	\$8.96	1,857	1,911	\$1,101.20	\$1,073.776	-\$27.43	\$0.00	\$460.61
Battle Creek	\$50.00	\$28.00	\$142.4946	20,383	\$2,853.62	764	\$106.96	20,325	21,147	\$12,052.73	\$11,882.330	-\$170.39	\$0.00	\$3,010.68
Belleville	\$50.00	\$28.00	\$142.4946	1,309	\$183.26	57	\$7.98	1,232	1,366	\$730.58	\$767.544	\$36.97	\$0.00	\$448.70
Berkley	\$50.00	\$28.00	\$142.4946	7,036	\$985.04	140	\$19.60	7,035	7,176	\$4,171.76	\$4,032.137	-\$139.62	-\$35.05	\$1,050.47
Brownstown	\$50.00	\$28.00	\$142.4946	12,251	\$1,715.14	876	\$122.64	12,266	13,127	\$7,273.74	\$7,375.956	\$102.22	\$0.00	\$2,160.49
Canton	\$50.00	\$28.00	\$142.4946	31,580	\$4,421.20	889	\$124.46	31,719	32,469	\$18,809.37	\$18,244.071	-\$565.30	\$0.00	\$4,200.86
Chelsea	\$50.00	\$28.00	\$142.4946	2,266	\$317.24	80	\$11.20	2,264	2,346	\$1,342.55	\$1,318.199	-\$24.35	\$0.00	\$524.58
Flat Rock	\$50.00	\$28.00	\$142.4946	3,113	\$435.82	113	\$15.82	3,116	3,226	\$1,847.79	\$1,812.664	-\$35.12	\$0.00	\$637.01
Galesburg	\$50.00	\$28.00	\$142.4946	558	\$78.12	20	\$2.80	557	578	\$330.30	\$324.774	-\$5.53	\$0.00	\$295.89
Garden City	\$50.00	\$28.00	\$142.4946	11,731	\$1,642.34	0	\$0.00	11,731	11,731	\$6,956.48	\$6,591.555	-\$364.93	\$0.00	\$1,497.91
Gibraltar	\$50.00	\$28.00	\$142.4946	1,896	\$265.44	35	\$4.90	1,897	1,931	\$1,124.92	\$1,085.013	-\$39.91	\$0.00	\$450.93
GP City	\$50.00	\$28.00	\$142.4946	2,357	\$329.98	88	\$12.32	2,357	2,445	\$1,397.70	\$1,373.826	-\$23.88	\$0.00	\$538.92
GP Farms	\$50.00	\$28.00	\$142.4946	4,172	\$584.08	79	\$11.06	4,173	4,251	\$2,474.59	\$2,388.603	-\$85.99	\$0.00	\$729.65
GP Park	\$50.00	\$28.00	\$142.4946	4,203	\$588.42	46	\$6.44	4,206	4,249	\$2,494.16	\$2,387.479	-\$106.68	\$0.00	\$708.68
GP Shores	\$50.00	\$28.00	\$142.4946	1,161	\$162.54	10	\$1.40	1,162	1,171	\$689.07	\$657.976	-\$31.09	\$0.00	\$353.34
GP Woods	\$50.00	\$28.00	\$142.4946	6,915	\$968.10	82	\$11.48	6,914	6,997	\$4,100.00	\$3,931.558	-\$168.44	\$0.00	\$1,031.63
Hamburg	\$50.00	\$28.00	\$142.4946	9,843	\$1,378.02	61	\$8.54	9,848	9,904	\$5,839.86	\$5,564.978	-\$274.89	\$0.00	\$1,332.17
Harper Woods	\$50.00	\$28.00	\$142.4946	5,983	\$837.62	100	\$14.00	5,915	6,083	\$3,507.60	\$3,417.989	-\$89.61	\$0.00	\$982.51
Highland Park	\$50.00	\$28.00	\$142.4946	3,782	\$529.48	439	\$61.46	3,710	4,221	\$2,200.03	\$2,371.746	\$171.72	\$0.00	\$983.15
Highland Twp	\$50.00	\$28.00	\$142.4946	7,616	\$1,066.24	154	\$21.56	7,614	7,770	\$4,515.10	\$4,365.901	-\$149.20	\$0.00	\$1,159.09
Huntington Woods	\$50.00	\$28.00	\$142.4946	2,461	\$344.54	42	\$5.88	2,462	2,503	\$1,459.97	\$1,406.416	-\$53.55	-\$22.28	\$495.08
Huron	\$50.00	\$28.00	\$142.4946	6,407	\$896.98	78	\$10.92	6,412	6,485	\$3,802.32	\$3,643.870	-\$158.45	\$0.00	\$969.95
Inkster	\$50.00	\$28.00	\$142.4946	9,983	\$1,397.62	202	\$28.28	9,977	10,185	\$5,916.36	\$5,722.870	-\$193.49	\$0.00	\$1,452.90
Kalamazoo	\$50.00	\$28.00	\$142.4946	21,807	\$3,052.98	823	\$115.22	21,825	22,630	\$12,942.23	\$12,715.616	-\$226.61	\$0.00	\$3,162.09
Leroy Twp	\$50.00	\$28.00	\$142.4946	2,042	\$285.88	29	\$4.06	2,040	2,071	\$1,209.72	\$1,163.678	-\$46.04	\$0.00	\$464.39
Lyndon	\$50.00	\$28.00	\$142.4946	1,617	\$226.38	0	\$0.00	1,612	1,617	\$955.92	\$908.690	-\$47.23	\$0.00	\$399.65
Milan	\$50.00	\$28.00	\$142.4946	2,412	\$337.68	63	\$8.82	2,412	2,475	\$1,430.32	\$1,390.683	-\$39.63	\$0.00	\$527.36
Milford	\$50.00	\$28.00	\$142.4946	7,371	\$1,031.94	204	\$28.56	7,612	7,575	\$4,513.92	\$4,256.332	-\$257.58	-\$264.44	\$758.97
Northville	\$50.00	\$28.00	\$142.4946	10,936	\$1,531.04	194	\$27.16	10,984	11,130	\$6,513.51	\$6,253.858	-\$259.65	\$0.00	\$1,519.04
Oak Park	\$50.00	\$28.00	\$142.4946	10,349	\$1,448.86	412	\$57.68	10,385	10,761	\$6,158.31	\$6,046.520	-\$111.79	\$0.00	\$1,615.25
Pennfield Twp	\$50.00	\$28.00	\$142.4946	3,967	\$555.38	66	\$9.24	3,944	4,033	\$2,338.79	\$2,266.110	-\$72.68	\$0.00	\$712.43
Plymouth City	\$50.00	\$28.00	\$142.4946	4,153	\$581.42	176	\$24.64	4,155	4,329	\$2,463.92	\$2,432.430	-\$31.48	\$0.00	\$795.07
Plymouth Twp	\$50.00	\$28.00	\$142.4946	10,848	\$1,518.72	461	\$64.54	10,784	11,309	\$6,394.91	\$6,354.437	-\$40.48	\$0.00	\$1,763.28
Portage	\$50.00	\$28.00	\$142.4946	17,811	\$2,493.54	627	\$87.78	17,825	18,438	\$10,570.23	\$10,360.165	-\$210.06	\$0.00	\$2,591.75
Putnam Twp	\$50.00	\$28.00	\$142.4946	4,039	\$565.46	59	\$8.26	4,032	4,098	\$2,390.98	\$2,302.633	-\$88.34	\$425.73	\$1,131.60
Redford	\$50.00	\$28.00	\$142.4946	21,913	\$3,067.82	524	\$73.36	21,918	22,437	\$12,997.37	\$12,607.171	-\$390.20	\$0.00	\$2,971.47
River Rouge	\$50.00	\$28.00	\$142.4946	3,259	\$456.26	136	\$19.04	3,244	3,395	\$1,923.69	\$1,907.623	-\$16.07	\$0.00	\$679.73
Riverview	\$50.00	\$28.00	\$142.4946	4,014	\$561.96	96	\$13.44	4,017	4,110	\$2,382.08	\$2,309.376	-\$72.70	\$0.00	\$723.19
Saline	\$50.00	\$28.00	\$142.4946	3,684	\$515.76	98	\$13.72	3,715	3,782	\$2,203.00	\$2,125.076	-\$77.92	-\$127.10	\$544.96
Southgate	\$50.00	\$28.00	\$142.4946	11,317	\$1,584.38	482	\$67.48	11,317	11,799	\$6,710.98	\$6,629.764	-\$81.22	\$0.00	\$1,791.14
Springfield	\$50.00	\$28.00	\$142.4946	1,738	\$243.32	61	\$8.54	1,736	1,799	\$1,005.03	\$1,010.844	\$5.81	-\$3.95	\$474.22
Sumpter	\$50.00	\$28.00	\$142.4946	4,191	\$586.74	50	\$7.00	4,186	4,241	\$2,482.30	\$2,382.984	-\$99.31	\$0.00	\$714.92
Taylor	\$50.00	\$28.00	\$142.4946	22,727	\$3,181.78	773	\$108.22	22,729	23,500	\$13,478.30	\$13,204.462	-\$273.83	\$0.00	\$3,236.66
Van Buren	\$50.00	\$28.00	\$142.4946	9,531	\$1,334.34	272	\$38.08	9,845	9,803	\$5,838.09	\$5,508.227	-\$329.86	\$0.00	\$1,263.06
Wayne	\$50.00	\$28.00	\$142.4946	6,133	\$858.62	194	\$27.16	6,141	6,327	\$3,641.61	\$3,555.091	-\$86.52	\$0.00	\$1,019.75
Westland	\$50.00	\$28.00	\$142.4946	28,259	\$3,956.26	661	\$92.54	28,271	28,920	\$16,764.70	\$16,249.917	-\$514.79	\$0.00	\$3,754.51
Wixom	\$50.00	\$28.00	\$142.4946	4,353	\$609.42	464	\$64.96	4,462	4,817	\$2,645.97	\$2,706.634	\$60.67	-\$39.50	\$916.04
Wyandotte	\$50.00	\$28.00	\$142.4946	10,987	\$1,538.18	138	\$19.32	11,181	11,125	\$6,630.33	\$6,251.049	-\$379.28	\$0.00	\$1,398.71
Ypsilanti	\$50.00	\$28.00	\$142.4946	4,723	\$661.22	193	\$27.02	4,724	4,916	\$2,801.33	\$2,762.261	-\$39.07	\$0.00	\$868.81
Column Totals:	\$2,500.00	\$1,400.00	\$7,124.73	395,958	\$55,434.12	11,770	\$1,647.80	396,745	407,728	\$235,245.37	\$229,099.21	-\$6,146.15	-\$66.59	\$61,893.05



3901 East Paris Ave. SE
 Grand Rapids, MI 49512
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Customer Remittance Copy ¹⁷
Invoice
355682

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/15/2026

Invoice Due 3/17/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing for Drops #3, #4 & #6 (mailed 2/13)							
6	Assessment Set-up PDF File			50.0000	ea	300.00		300.00
1	AN-Version Change			140.0000	ea	140.00		140.00
26,671	Process & Mail Assessments		450.00	140.0000	/m	3,733.94		3,733.94
1	USPS Promotions Shared Incentive			466.6800		466.68		466.68

Postage Used includes 494 pcs. @ .74 and 8 foreign pcs. @ \$1.70

Prepaid postage balance is \$142,031.40 to be used for future drops.

Postage Earmarked for Job	\$157,033.07
Postage Used from Suspense	\$15,001.67
Postage Usage for Job	(\$15,001.67)
Postage Balance	\$0.00
Held in Suspense	\$142,031.40

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$4,640.62

Sales Tax

TOTAL DUE \$4,640.62

Customer #: 10991

Tax #:



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Customer Remittance Copy ¹⁸
Invoice
355464

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 1/31/2026

Invoice Due 3/2/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
Assessments Notices (Real) - Drop #1 (Wyandotte)								
1	Assessment Set-up PDF File			50.0000	ea	50.00		50.00
1	AN-Version Change			140.0000	ea	140.00		140.00
10,987	Process & Mail Assessments		450.00	140.0000	/m	1,538.18		1,538.18
1	USPS Promotions Shared Incentive			192.5300	ea	192.53		192.53

Postage Used includes 170 pcs. @ .74 and 1 foreign pc. @ \$1.70

Note: 1 bad addresss was not mailed.

Prepaid postage balance is \$116,005.76 to be used for future drops.

Postage Earmarked for Job	\$122,165.73
Postage Used from Suspense	\$6,159.97
Postage Usage for Job	(\$6,159.97)
Postage Balance	\$0.00
Held in Suspense	\$116,005.76

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$1,920.71

Sales Tax

TOTAL DUE \$1,920.71

Customer #: 10991

Tax #:



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 Grand Rapids, MI 49512
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 616.957.3026 fax
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Customer Remittance Copy ¹⁹
Invoice
356392

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/26/2026

Invoice Due 3/28/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing for Drop #27 and Drop #30							
2	Assessment Set-up PDF File (Drop #27 and Drop #30)			50.0000	ea	100.00		100.00
1	AN-Version Change (for Mailing Date 2/25/26)			140.0000	ea	140.00		140.00
28,860	Process & Mail Assessments (Drop #27 and Drop #30)		450.00	140.0000	/m	4,040.40		4,040.40
1	USPS Promotions Shared Incentive (Drop #27 and Drop #30)			506.2700		506.27		506.27

Postage Used includes 49 pcs. @ .74 and

Postage Earmarked for Job	\$0.00
Postage Used from Suspense	\$0.00
Postage Usage for Job	(\$16,236.44)
Postage Balance	\$16,236.44

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$4,786.67

Sales Tax

Postage Balance \$16,236.44

TOTAL DUE **\$21,023.11**

Customer #: 10991

Tax #:



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 Grand Rapids, MI 49512
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Customer Remittance Copy ²⁰
Invoice
356344

Page 1 of 1

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/25/2026

Invoice Due 3/27/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing Drop #16 (Huron)							
1	Assessment Set-up PDF File (Drop #16)			50.0000	ea	50.00		50.00
1	AN-Version Change (Mailing Date 11/24/26)			140.0000	ea	140.00		140.00
6,407	Process & Mail Assessments (Drop #16)		450.00	140.0000	/m	896.98		896.98
1	USPS Promotions Shared Incentives (Drop #16)			112.3500		112.35		112.35

Postage Used includes 99 pcs. @ .74

Note: 1 bad address was not mailed.

Postage Earmarked for Job	\$0.00
Postage Used from Suspense	\$0.00
Postage Usage for Job	(\$3,593.68)
Postage Balance	\$3,593.68

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$1,199.33

Sales Tax

Postage Balance \$3,593.68

TOTAL DUE \$4,793.01

Customer #: 10991

Tax #:

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/25/2026

Invoice Due 3/27/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing Drop #16 (Huron)							
1	Assessment Set-up PDF File (Drop #16)			50.0000	ea	50.00		50.00
1	AN-Version Change (Mailing Date 11/24/26)			140.0000	ea	140.00		140.00
6,407	Process & Mail Assessments (Drop #16)		450.00	140.0000	/m	896.98		896.98
1	USPS Promotions Shared Incentives (Drop #16)			112.3500		112.35		112.35

Postage Used includes 99 pcs. @ .74

Note: 1 bad address was not mailed.

Postage Earmarked for Job	\$0.00
Postage Used from Suspense	\$0.00
Postage Usage for Job	(\$3,593.68)
Postage Balance	\$3,593.68

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$1,199.33

Sales Tax

Postage Balance \$3,593.68

TOTAL DUE \$4,793.01

Customer #: 10991

Tax #:



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Customer Remittance Copy ²²
Invoice
356289

Page 1 of 1

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/23/2026

Invoice Due 3/25/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing for Drop #8 and #18							
2	Assessment Set-up PDF File (for Drop #8 and #18)			50.0000	ea	100.00		100.00
1	AN-Version Change (for drops mailed on 2/23/26)			140.0000	ea	140.00		140.00
15,698	Process & Mail Assessments (for Drop #8 and #18)		450.00	140.0000	/m	2,197.72		2,197.72
1	USPS Promotions Shared Incentives (for Drop #8 and #18)			276.8800		276.88		276.88

Postage Used includes 138 pcs. @ .74 and 1 foreign pc. @ \$1.70

Postage Earmarked for Job	\$0.00
Postage Used from Suspense	\$0.00
Postage Usage for Job	(\$8,779.26)
Postage Balance	\$8,779.26

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$2,714.60

Sales Tax

Postage Balance \$8,779.26

TOTAL DUE \$11,493.86

Customer #: 10991

Tax #:



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 Grand Rapids, MI 49512
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Customer Remittance Copy ²³
Invoice
355737

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/17/2026

Invoice Due 3/19/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing Drop #2 (Chelsea) and Drop #12 (Milford)							
2	Assessment Set-up PDF File (Drop #2 & Drop #12)			50.0000	ea	100.00		100.00
1	AN-Version Change (Mailed 2/17)			140.0000	ea	140.00		140.00
9,631	Process & Mail Assessments (Drop #2 & Drop #12)			140.0000	/m	1,348.34		1,348.34
1	USPS Promotions Shared Incentive (Drop #2 and Drop #12)			166.2000		166.20		166.20

Postage Used includes 310 pcs. @ .74 and 2 foreign pcs. @ \$1.70

Prepaid postage balance is \$182,101.75 to be used for future drops.

Postage Earmarked for Job	\$187,541.92
Postage Used from Suspense	\$5,440.17
Postage Usage for Job	(\$5,440.17)
Postage Balance	\$0.00
Held in Suspense	\$182,101.75

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$1,754.54

Sales Tax

TOTAL DUE \$1,754.54

Customer #: 10991

Tax #:

Drop



3901 East Paris Ave. SE
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Customer Copy ²⁴
Invoice
356004

Page 1 of 1

Attention Tonya Shea
Invoice To WCA Assessing
38110 Executive Dr, Ste 200
Westland, MI 48185

Invoice Date 2/21/2026

Invoice Due 3/23/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing for Drops Mailed on 2/20/26							
4,039	Printing - Putnam Insert			105.4048	/m	425.73		425.73
32	Assessment Set-up PDF File (for Drops mailed on 2/20)			50.0000	ea	1,600.00		1,600.00
1	AN-Version Change (for Drops mailed on 2/20)			140.0000	ea	140.00		140.00
288,023	Process & Mail Assessments (for Drops mailed on 2/20)		450.00	140.0000	/m	40,323.22		40,323.22
1	USPS Promotions Shared Incentives (for Drops mailed on 2/20)			5,014.2700		5,014.27		5,014.27

Postage Used includes 5,567 pcs. @ .74 and 168 foreign pcs. @ \$1.70

Note: 84 bad addresses were not mailed.

Note: See Spreadsheet enclosed for detail on drops.

Postage Earmarked for Job	\$182,101.75
Postage Used from Suspense	\$182,101.75
Postage Usage for Job	(\$161,518.29)
Postage Balance Applied to Services	(\$20,583.46)

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$47,503.22

Sales Tax

Postage Balance \$(20,583.46)

TOTAL DUE \$26,919.76

Customer #: 10991

Tax #:



3901 East Paris Ave. SE
 Grand Rapids, MI 49512
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Customer Remittance Copy ²⁵
Invoice
356004

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/21/2026

Invoice Due 3/23/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing for Drops Mailed on 2/20/26							
4,039	Printing - Putnam Insert			105.4048	/m	425.73		425.73
32	Assessment Set-up PDF File (for Drops mailed on 2/20)			50.0000	ea	1,600.00		1,600.00
1	AN-Version Change (for Drops mailed on 2/20)			140.0000	ea	140.00		140.00
288,023	Process & Mail Assessments (for Drops mailed on 2/20)	450.00		140.0000	/m	40,323.22		40,323.22
1	USPS Promotions Shared Incentives (for Drops mailed on 2/20)			5,014.2700		5,014.27		5,014.27

Postage Used includes 5,567 pcs. @ .74 and 168 foreign pcs. @ \$1.70

Note: 84 bad addresses were not mailed.

Note: See Spreadsheet enclosed for detail on drops.

Postage Earmarked for Job	\$182,101.75
Postage Used from Suspense	\$182,101.75
Postage Usage for Job	(\$161,518.29)
Postage Balance	(\$20,583.46)
Applied to Services	

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$47,503.22

Sales Tax

Postage Balance \$(20,583.46)

TOTAL DUE \$26,919.76

Customer #: 10991

Tax #:



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 Grand Rapids, MI 49512
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 616.957.3026 fax
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Customer Remittance Copy ²⁶
Invoice
355719

Page 1 of 1

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/16/2026

Invoice Due 3/18/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - Partial Billing for Drop #11 (Gibraltar)							
1	Assessment Set-up PDF File (Drop 11)			50.0000	ea	50.00		50.00
1	AN-Version Change			140.0000	ea	140.00		140.00
1,896	Process & Mail Assessments (Drop 11)		450.00			450.00		450.00
1	USPS Promotions Shared Incentive (Drop 11)			33.0100		33.01		33.01
	Postage Used includes 48 pcs. @ .74							
	Prepaid postage balance is \$140,961.76 to be used for future drops.							

Postage Earmarked for Job	\$142,031.40
Postage Used from Suspense	\$1,069.64
Postage Usage for Job	(\$1,069.64)
Postage Balance	\$0.00
Held in Suspense	\$140,961.76

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$673.01

Sales Tax

TOTAL DUE \$673.01

Customer # : 10991

Tax # :



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 Grand Rapids, MI 49512
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Customer Copy **27**

Invoice

355738

Page 1 of 1

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/17/2026

Invoice Due 3/19/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real) - CREDIT for Drop #11							
-1,896	CREDIT - Process & Mail Assessments (Drop #11)			184.5600		(184.56)		(184.56)
	Note: Forgot to zero out minimum.							

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total (\$184.56)

Sales Tax

TOTAL DUE (\$184.56)

Customer #: 10991

Tax #:



3901 East Paris Ave. SE
 Grand Rapids, MI 49512
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 616.957.3026 fax
 kentcommunications.com

Customer Remittance Copy ²⁸
Invoice
356757

Attention Tonya Shea
Invoice To WCA Assessing
 38110 Executive Dr, Ste 200
 Westland, MI 48185

Invoice Date 2/28/2026

Invoice Due 3/30/2026

Job Number 184870

Your Order #

Salesrep Autumn Hoffman

Quantity	Description	Setup	Min	Unit Price	U/M	Sub Total	Tax	Total
	Assessments Notices (Real & Personal) - Final Billing							
3	Assessment Set-up PDF File (Real)			50.0000	ea	150.00		150.00
1	AN-Version Change (Mailed on 2/27/26) (Real) (3 Drops - #9, #26 & #28)			140.0000	ea	140.00		140.00
1	AN-Version Change (Mailed on 2/27/26) (Personal) (8 Drops - Drop 31 thru 38)			140.0000	ea	140.00		140.00
19,549	Process & Mail Assessments (Real and Personal)		450.00	140.0000	/m	2,736.86		2,736.86
1	USPS Promotions Shared Incentives			356.5400		356.54		356.54

Postage Used includes 1,363 pcs. @ .74 and 9 foreign pcs. @ \$1.70

Note: 3 bad addresses were not mailed.

Postage Earmarked for Job	\$24,467.19
Postage Used from Suspense	\$24,467.19
Postage Usage for Job	(\$11,484.65)
Postage Balance Applied to Services	(\$12,982.54)

TERMS: PLEASE NOTE OUR TERMS ARE NET 30 DAYS FROM DATE OF INVOICE.

Sub Total \$3,523.40

Sales Tax

Postage Balance \$(12,982.54)

TOTAL DUE **(\$9,459.14)**

Customer #: 10991

Tax #:

KCI Invoices for Assessment Change Notices - Summary of Charges

REAL & PERSONAL PROPERTY: Assessment Change Notices

Invoice #	Assessment Set Up		Quantity (Parcel count)	USPS Shared Incentive	Process & Mail Assessments	Invoice Total:	Postage Usage for Job (this invoice)	Printing Putnam Insert
	BS&A Data File	AN Version Change						
355464	\$50.00	\$140.00	10,987	\$192.53	\$1,538.18	\$1,920.71	\$6,159.97	
355682	\$300.00	\$140.00	26,671	\$466.68	\$3,733.94	\$4,640.62	\$15,001.67	
355719	\$50.00	\$140.00	1896	\$33.01	\$450.00	\$673.01	\$1,069.64	
355738					-\$184.56	-\$184.56		
355737	\$100.00	\$140.00	9637	\$166.20	\$1,348.34	\$1,754.54	\$5,440.17	
356004	\$1,600.00	\$140.00	288023	\$5,014.27	\$40,323.22	\$26,919.76	\$161,518.29	425.73
356289	\$100.00	\$140.00	15698	\$276.88	\$2,197.72	\$11,493.86	\$8,779.26	
356344	\$50.00	\$140.00	6407	\$112.35	\$896.98	\$4,793.01	\$3,593.68	
356392	\$100.00	\$140.00	28860	\$506.27	\$4,040.40	\$21,023.11	\$16,236.44	
356757	\$150.00	\$280.00	19,549	\$356.54	\$2,736.86	-\$9,459.14	\$11,484.65	
Totals:	\$2,500.00	\$1,400.00	407,728	\$7,124.73	\$57,081.08	\$63,574.92	\$229,283.77	

PERSONAL PROPERTY: Assessment Change Notices

Invoice #	Set Up BS&A Data File	AN Version Change	Quantity/ # Statements	Process/Mail Assessments	Total for Printing:	Postage Charged
			407,728	\$57,081.08		

Total All Invoices:

Assessment Set Up BS&A Data File	\$ 2,500.00	(Column A- Final Bill Spreadsheet)
AN Version Change	\$ 1,400.00	(Column B- Final Bill Spreadsheet)
Process/Mail Assessments	\$ 57,081.08	(Column D & E - Final Bill Spreadsheet)
Postage Charged by USPS	\$ 229,099.21	
USPS Shared Incentive	\$ 7,124.73	(Column C - Final Bill Spreadsheet)
Pre-paid Postage collected Feb 2025	\$ (235,245.37)	
Credits/Adjustments	(\$66.59)	
Total due all invoices to KCI:	\$ 61,893.06	

Motion By: Christian Armstrong **Supported By:** William Patts
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve Magnet Forensics, preferred vendor, for the purchase of Magnet AXIOM software and specialized training for an amount not to exceed \$12,994, funded through General Fund - Police Contractual Services and Police Training.

Unanimously carried

CCR #: 2.51-26

Motion By: William Patts **Supported By:** Gerald Thomas
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To approve accepting the MCOLES Public Safety Academy Assistance Grant to cover academy tuition and salary reimbursements of one (1) sponsored Police Officer Candidate from the Eligibility List, attending the Wayne County Regional Police Training Academy.

Unanimously carried

CCR #: 2.52-26

Motion By: Chris Clark **Supported By:** Dan Wallace
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To receive & file 0 % interest deferred loan for project #2025-10 for work performed by Optimum Contracting Solutions, qualified low quote, for Emergency HVAC Replacement in the amount of \$11,125. Funded through the Community Development Block Grant Home Rehabilitation account. Not to exceed program limits.

Unanimously carried

CCR #: 2.53-26

Motion By: Gerald Thomas **Supported By:** Chris Clark
Ayes: Charley Johnson, Chris Clark, Dan Wallace, William Patts, Ron Thiede, Christian Armstrong, Gerald Thomas
Nays: None
Resolved: To amend CCR#12.537-25 to increase authority to Detroit Salt, current vendor, for an amount not to exceed \$40,000. Funded through General Fund-Department of Public Works-Operating Supplies Salt.

Unanimously carried

CCR #: 2.54-26

Q-437276 - USD 7,250.00



Quotation

Address:
 Magnet Forensics, LLC
 931 Monroe Drive NE
 Suite A102-340
 Atlanta, Georgia 30308
 United States

Phone: 519-342-0195

Quote #: Q-437276-1
Issue Date: 2 Mar, 2026
Expires On: 1 Apr, 2026

Bill To
 Jordan Chamberlain
 Taylor Police Department (MI)
 23515 Goddard
 Taylor, Michigan 48180
 United States
 734-287-6611 EXT 2011
 jchamberlain@cityoftaylorml.gov

Ship To
 Jordan Chamberlain
 Taylor Police Department (MI)
 23515 Goddard
 Taylor, Michigan 48180
 United States
 734-287-6611 EXT 2011
 jchamberlain@cityoftaylorml.gov

End User
 Jordan Chamberlain
 Taylor Police Department (MI)
 23515 Goddard
 Taylor Michigan 48180
 United States
 734-287-6611 EXT 2011
 jchamberlain@cityoftaylorml.gov

PREPARED BY	PHONE	EMAIL	PAYMENT TERM
John Bigler	551-574-5607	john.bigler@magnetforensics.com	Net 30

ITEM #	PRODUCT NAME	SMS DATES	UNIT SELLING PRICE	QTY	EXTENDED PRICE
6AXB200	Magnet AXIOM Advanced Bundle		USD 0.00	1	USD 0.00
6AX210	Magnet AXIOM Advanced	12	USD 7,225.00	1	USD 7,225.00
5D000	Shipping - Domestic		USD 25.00	1	USD 25.00

Sub-Total USD 7,250.00
 Taxes USD 0.00
Grand Total USD 7,250.00

Prices subject to change upon quote expiry. Accurate sales tax will be calculated at the time of invoicing when applicable. If your company is tax exempt, please provide appropriate support with your signed quote. Hardware may be subject to additional fees related to delivery, import and export.

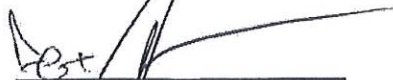
Terms & Conditions

Unless you have an existing written agreement with Magnet Forensics for the products and/or services listed in this quotation, by: (a) signing below, (b) submitting an Order to Magnet Forensics referencing this quotation, or (c) making payment for the products and/or related services listed in this quotation, you agree to the terms and conditions at <http://magnetforensics.com/legal/> applicable to such products and/or services listed in this quotation to the exclusion of any differing or additional terms which may be found on your purchase order or similar document. By signing, you certify that you have the authority to bind your organization.

Q-437276 - USD 7,250.00

Magnet Forensics may adjust the software term start and/or end date, without increasing the total software license price, based on the date Magnet Forensics activates the software and provided that the total software license term length does not change.

Signature:



Date:

3, 2, 2026

Name (Print):

Det. Jordan Chamberlain

Title:

Detective

Please sign and email to John Bigler at john.bigler@magnetforensics.com



**PLANNING COMMISSION
PETITION FOR REZONING**

Filing Fee
\$3,000.00

Case # _____ (Z)

TO THE TAYLOR CITY PLANNING COMMISSION:

Date Filed _____

The undersigned respectfully petition(s) Planning Commission to amend the Taylor Code of Ordinance, Appendix A-Zoning, as amended, and change the zoning map as hereinafter requested, and in support of this petition, the following facts are shown:

PROPERTY IDENTIFICATION

Street Address: 8715 Telegraph Rd.

The property is located on the East side of Telegraph Rd. Street,
Between Wohlfeill Street and Mary Street.
It has frontage of (30') thirty feet, a depth of 315' (irregular) feet, and comprises +/- 2.23 acres.

Legal Description: 09N 1A1B N1B1B PART OF THE SW 1/4 SECTION 9 T3S R10E BEG N0EG 08M 02S W 1.493.73' FROM SW CORNER SEC 9 THENCE N0D 8M 02S W 40' THENCE N89D 28M 50S E 342.44' THENCE N0D 08M 02S W 121.03' THENCE N89D 38M 34S E THENCE S3D 27M 38S E 330.86' THENCE S89D 28M 50S W 2' 10.07' THENCE N0D 08M 02S W 170' THENCE S89D 28M 50S W 466.33' TO THE P.O.B. CONTAINING 2.25 ACRES.

Parcel I.D. # 60-027-99-0004-004

Property Owner: Paolo Pecci

Address: 8715 Telegraph Rd, Taylor, MI 48180-2353

Phone: 734-624-4246 Fax: _____ Cell: _____ Email: _____

PROOF OF OWNERSHIP MUST BE ATTACHED TO THIS APPLICATION

PROPOSED USE:

Clearly describe the proposed project – do not write “refer to plans”: Applying to rezone an existing building to an indoor climate controlled self-storage with condition.

NATURE OF PETITION

Current Zoning: B-3 General Business District Requested Zoning: I-1 - Light Industrial

Proposed Use: indoor climate controlled self-storage with condition

PLEASE INCLUDE EXACT LEGAL DESCRIPTION OF PROPERTY TO BE REZONED AS WELL AS A DIAGRAM OR DRAWING INDICATING THE PROPERTY TO BE REZONED

AFFIDAVIT OF PETITIONER

STATE OF MICHIGAN)
COUNTY OF WAYNE) ss

The undersigned Petitioner, being duly sworn, deposes and says that the statements and information herewith submitted are true and correct to the best of his/her knowledge, information and belief; further, that she/he is authorized to submit this petition.

Printed Name of Petitioner PETER STUHLREYER

Signature of Petitioner [Signature]

Interest in Property ARCHITECT

Firm DESIGNHAUS LLC

Address 3300 AUBURN RD #305 AUBURN HILLS, MI 48326

Phone: 248 201 4422 Fax: _____ Cell: _____ Email: info@designhaus.com

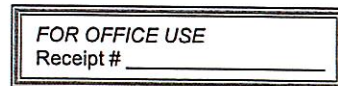
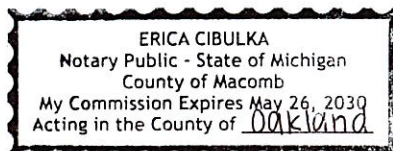
Subscribed and sworn to before me this 12 day of January, 2026

[Signature]

Notary Public, Wayne County, Michigan

My Commission Expires: MAY 26, 2030

Planning Department





January 1st, 2026

City of Taylor
Planning Department
23555 Goddard Rd.
Taylor, MI 48180

Attn. Planning Commission members

Re: 8715 Telegraph Rd. (Coliseum Racquet Club)
Property Tax ID Number: 027-99-0004-004

Storage + Plus Personal Inventory Center - Conditional Re-zoning Request

Attn Commission Members,

Designhaus Architecture of Auburn Hills, Michigan has been retained by Paolo Pecci, owner of Coliseum Racquet Club, whose address is 8715 Telegraph Rd, within the City of Taylor. It is his intent to pursue a conditional rezoning request to allow for a change of use and allow Self-Storage to operate as a permitted use within the buildings current zoning district. We are requesting that the existing property be conditionally be rezoned from B-3 (General Business) to I-1 (Light Industrial). Conditions are attached separately as Exhibit B. This will allow for a change of use and a building addition per ordinance standards. The site is the Coliseum Racquet Club, which is an indoor facility for racquetball and other sport courts. Located on the east side of Telegraph between Wohlfeil St. and Mary St. The 97,139 Sq. Ft. (2.23 acre) property currently occupied with a 2-story building with 20,000 Sq. Ft. on the first floor. A large parking lot also is present on site. Recently, a dramatic shift in the use and demand, along with other economic factors have reduced the facility to operate at an unacceptable level to sustain daily operations. The center is losing money on a monthly basis, and the owner is exploring other business operations within the existing building. Due to the unique layout of the building with its open floorplan and tall 20' ceiling, commercial redevelopment is limited. However, with this building layout, interior climate controlled self-storage is a perfect option. A second level can be built out within the existing walls and individual storage lockers can be easily adapted. Due to the special nature of the current use, redevelopment options are limited. Building setback and the irregular parcel layout limit road frontage to only 30', and visibility from a market standpoint thereby also severely limiting the current re-development potential.

The developer is proposing to renovate the existing building to an indoor climate controlled self-storage center. Market studies have indicated a void in the market within the area, and the unique building layout



such that its open in nature, making the property easily converted to be suitable for such use. An exterior addition would also allow for more rentable area and several exterior drive-up storage units. For customer convenience, a covered loading area is also proposed. Current zoning of B-3 will not permit self-storage. It would be our intent to request conditional rezoning to I-1 which would allow for mini-warehouses, or self-storage facilities. Surrounding uses include restaurant B-3 / single family residential R-1C to the north, R-1C zoning to the east, restaurant B-3 to the south, and Telegraph Rd / B-3 zoning to the west.

We feel that this use would be a decrease in customer and vehicular traffic, utility consumption, and overall use of municipal services. With regards to traffic, the proposed use has been anticipated to have about 17 customer trips per day. This can be seen as a decrease in the current use as a racquet club. High customer turnover typically is associated with this use, and any typical commercial use would also have a much higher traffic count. A traffic impact study was conducted, and the results show a significant decrease in the amount of traffic generated as compared to the previous use. Such memo is attached.

In consideration of the rezoning amendment, we have analyzed the project as it relates to criteria listed in section 27.04 of the zoning ordinance.

Per Section 27.04:

1. Consistent with the Master Plan. In fact, the nature of the proposed use would be that of a less intense use. Such use typically not permitted in the area would allow for those close by to benefit from services provided for both commercial and residential. The site also is located within the Telegraph Road Corridor. Such use would offer a less intense traffic count than other uses typically permitted in the area.
2. Capability of the site's physical, geological and hydrological features with the host of uses permitted. The overall demand on utilities would decrease which can be a positive attribute. One example would be an energy efficient lighting design which included motion sensors for automatic control. No wetland or other water courses exist on site. Water and sewer services would also decrease. Based on past projects and customer usage, bathroom trips would be limited to most 10-15 flushes / day with the use of a single drinking fountain. When compared to adjacent uses such as restaurants, the proposed use would be much lower.



3. Development under current zoning is impracticable and less reasonable than the requested such that the specific and unique design and layout limit conventional commercial development. Irregular lot parcel and 30' road frontage also makes re-development under current zoning impracticable. The proposed use can easily adapt to these adverse site constraints as long as the use is ultimately permitted.

4. The proposed would be compatible with all potential uses allowed in the current zoning district in terms of land suitability, environmental impacts, noise, density, nature of use, traffic, aesthetics, infrastructure, and impact on adjacent property values. The proposed use will set a lower threshold for such performance standards previously listed. Noise generated at similar facilities would not be discernible to adjacent properties. In addition, existing screening and proposed enhancements will also contribute to reduced levels of off-site disturbances. Currently a 6' wall exists adjacent to the R-1C District. The conditions of the wall will be examined, and any repairs will be considered. Increased landscaping where none exists, and an overall property makeover will improve the aesthetics of the site. Landscape buffering and plantings are proposed to create a dense opaque screen at the perimeter where none exist now. Hours of operation can be limited to regular business hours, and property safeguards will be assured by 24-hour security monitoring via remote cameras, sensors, pass code monitoring, and building exterior perimeter movement. Exterior lighting will also be evaluated and updated to comply with zoning standards. Based on similar facilities, customer loading will be limited to boxes of personal items typically fitting in passenger vehicles. No hazardous or flammable substances will be permitted.

5. Capacity of City infrastructure and services sufficient to accommodate proposed use. Previous and similar uses within the zoning district would pose an increased demand than that of a self-storage facility. Customer trips and the nature of the business dictate that, as compared to a typical B-3 use such as a restaurant or medical office. Proposed use of self-storage will not compromise the public health, safety, or welfare. In addition, such use will bring a less demand in all areas of public concern.

6. Demand for the types of uses permitted in the requested zoning district in relation to the amount land currently zoned for such uses. This request is site specific in that the unique layout of the building will easily adapt to interior climate controlled self-storage. Market studies have dictated a present demand in the area. Other uses would require much more renovation such that any modifications would not be cost effective for the site.



- 7 The request has not been previously submitted within the past year, and the rezoning is for the specific use only. Any other use would require additional consideration by the Planning Commission.

Upon reviewing the submitted, we ask that you consider the proposed use of self-storage which has a low customer demand, be considered as an acceptable use within the B-3 zoning district, where it would be typically confined to the I-1 district. Please review the attached drawing and design materials and contact us with questions. We will be happy to discuss the project at the next available Commission meeting.

Regards,

A handwritten signature in blue ink that reads "Peter Stuhlreyer". The signature is written in a cursive, flowing style.

Peter Stuhlreyer
Architect
Designhaus, LLC



February 18, 2021

VIA EMAIL: mike@designhaus.com

Mr. Mike Pizzola
Designhaus
3300 Auburn Rd, Suite 300
Auburn Hills, MI 48326

**RE: Proposed Residential Development
Taylor, Michigan
Trip Generation Analysis Proposal**

Dear Mr. Pizzola:

The professional staff of Fleis & VandenBrink (F&V) appreciates the opportunity to present you with our proposal to provide Traffic Engineering Services for the proposed project in Taylor, Michigan. Our understanding of the project needs, proposed scope of work, and associated fees are outlined below.

Project Understanding

The project site is located at 8715 Telegraph Road in Taylor, Michigan. The proposed project is part of a conditional rezoning request, and includes the conversion of an indoor racquet ball club to self-storage facility. The existing building has a total of 42,000 GSF and the proposed self-storage facility includes the construction of approximately 22,000 GSF, for a new total of 64,000 GSF of building area. F&V proposes to complete a Trip Generation Analysis for this site as requested by the City of Taylor.

Our understanding of the project is based on information you have provided, review of the study road network, professional experience, and pursuant to the requests of the City of Taylor. If our understanding of this project does not fully address your needs, please let us know and we will modify our proposal accordingly.

Scope of Services

F&V proposes to deliver the following scope of services to complete the trip generation memo for this project:

1. Provide a summary of the project background information including: illustrations and a narrative describing the site, surroundings, study area and adjacent roadway system (functional classifications, lanes, speed limits, etc.).
2. Obtain and review the site development plan and information related to the proposed development, including: the planned use, associated densities, and site access location.
3. Provide a description of the requested use, including: the types of development, size (GSF) and number of units.
4. Forecast the number of Weekday, AM and PM peak hour trips that would be generated by the existing proposed land uses. The forecasts shall be based on the data and procedures outlined in the most recent edition of Trip Generation published by the Institute of Transportation Engineering (ITE) *Trip Generation*,

**27725 Stansbury Boulevard, Suite 195
Farmington Hills, MI 48334**

P: 248.536.0080

F: 248.536.0079

www.fveng.co38

10th Edition. The data will be summarized in a table that shows the use, ITE code number, trip rate, and trips in and out.

5. Provide a comparison table of the trips generated by the proposed land use and the thresholds for performing a TIA or TIS for a development. A table will be provided in the report outlining the categories and quantities of land uses, with the corresponding trip generation rates or equations, and the resulting number of trips.

Deliverables

- Complete a technical memorandum suitable for submission to the City of Taylor which outlines the existing road conditions, land uses, and trip generation comparison. A draft memo will be provided for internal review and comment and subsequently finalized for submission.

Fees

Fleis & VandenBrink will complete the proposed services for a **Lump Sum Fee of \$1,500**.

If this proposal is acceptable to you, please have an authorized representative sign and return one copy of the attached Professional Services Agreement (PSA) to authorize commencement of the work proposed. Our proposal is valid for 30 days from the date of this letter.

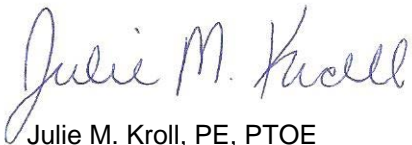
This proposal is presented based on our understanding of the project and information you have provided. F&V professional staff will be available for meetings, public hearings, and presentations related to the completion of this study; however, this fee **does not include meetings** (conducted in-person, via conference call or public meetings) not included in the scope of work above, additional items not specifically outlined in the above scope, modifications of the development plan, or further requirements of the reviewing jurisdictions or their consultants for project approval.

If meeting attendance is required, meetings will be billed on an hourly basis, as necessary. For all other additional services, F&V will provide a Contract Amendment outlining any associated additional fees.

We appreciate the opportunity to present our proposal for Professional Services. If you have any questions, please do not hesitate to contact us at your convenience.

Sincerely,

FLEIS & VANDENBRINK



Julie M. Kroll, PE, PTOE
Traffic Engineering Services Manager

Encl: PSA

JMK:jmk

PROFESSIONAL SERVICES AGREEMENT

FLEIS & VANDENBRINK ENGINEERING, INC.
27725 Stansbury Boulevard, Farmington Hills, Michigan 48334
P: 248.536.0080 F: 248.536.0079

This Professional Services Agreement ("PSA") is entered into between Fleis & VandenBrink Engineering, Inc. ("Engineer") and **Designhaus**, whose address is 3300 Auburn Rd., Suite 300, Auburn Hills, MI 48326 ("Client") where Engineer agrees to provide services for Client and Client agrees to pay Engineer, all in accordance with the terms of this PSA.

DESCRIPTION OF PROJECT AND SCOPE OF SERVICES: The description of the Project ("Project") and the scope of services ("Services") provided under this PSA is as follows: Trip Generation Analysis for Proposed Residential Development in Taylor, MI.

AGREEMENT DOCUMENTS: All obligations covered under this PSA are governed by the Agreement Documents, which specifically include this PSA and all of the following documents, which are all incorporated herein by reference; Engineer's proposal / letter dated February 18, 2021.

COMPENSATION OF ENGINEER:

This contract is a Lump Sum Fee contract in the amount of **\$1,500.00**, billed monthly based on the percentage of Work completed. Authorized additional services will be provided on an hourly basis plus 1.1 times reimbursable expenses unless otherwise negotiated.

Client shall Pay Engineer for all Services and reimbursable expenses on a monthly basis or as otherwise stated herein which shall be due and payable within fifteen (15) days of presentation of the invoice. Invoices shall be past due fifteen (15) days after presentation, and shall then incur interest at the rate of 18% per annum, or the highest rate permitted by law, whichever is lower. Reimbursable expenses include the cost of subconsultants.

PAYMENT. If Client fails to make any payment when due, Engineer may suspend performance of Services hereunder until all past due amounts and accrued interest are paid. Engineer shall have no liability of any type as a result of suspension of services caused by Client's failure to pay. The suspension of Services shall not limit any other remedy available to Engineer.

If Client objects to any portion of an invoice, Client shall notify Engineer in writing within five (5) days of presentation. Client shall identify the disputed charges and shall pay when due that portion of the invoice not in dispute. If the disputed amount of the invoice is resolved in Engineer's favor and not paid by the invoice due date, interest as stated in the agreement shall be paid by Client on the disputed amount from the original due date.

The Client's Payment of Engineer's invoices shall not be subject to any right of setoff, and payment shall be due regardless of suspension or termination of this Agreement by either party. If any payment obligation is not paid when due, Client agrees to pay all costs of the collection, including actual attorney's fees through all levels of appeal, whether or not a legal proceeding for collection is commenced as part of the collection process.

CLIENT REPRESENTATIVE. The Client's representative for this Project shall be Mr. Mike Pizzola, who shall have complete actual authority on behalf of the Client and its governing body to make all decisions in connection with the PSA.

CLIENT RESPONSIBILITIES. Client shall timely provide Engineer with all applicable information in its possession and to secure information in the possession of others for Engineer to complete the Scope of Services. Client shall give Engineer prompt oral and written notice whenever Client notices, discovers or is made aware of any alleged fault, defect or nonconformance in Engineer's services.

CHANGES. For all services that were completed due to changes to the Description of the Project and/or the Scope of Services, Engineer shall be paid by Client on an hourly basis at Engineer's customary hourly rates, plus 1.1 times reimbursable expenses, unless otherwise negotiated. If the construction period extends beyond the contracted period in the Scope of Services or the contracted completion date, all services of Engineer thereafter shall continue to be performed and shall be paid by Client on an hourly basis plus 1.1 times reimbursable expenses. Client understands and accepts that field techniques and analytical capabilities are evolving and that the standards and regulations are subject to rapid change such that currently acceptable investigative approaches and techniques may become superseded after the time of the signing of this PSA. Such changes will constitute changed conditions requiring adjustment in the Services and Engineer's Compensation.

DELAYS. Engineer shall not be responsible to Client for any delay of any type or kind unless caused in whole by Engineer.

CONSULTANTS. Engineer may engage Consultants and subcontractors to perform, in its sole discretion, all or any portion of the Services.

COST ESTIMATES. Engineer has no control over the costs of labor and material for construction or over competitive bidding and market conditions. All cost estimates provided by Engineer are based on Engineer's experience and are considered opinions of probable cost. Engineer does not warrant the accuracy of any cost estimate. If project costs exceed the Client's expectations and the Client decides to re-design or re-bid any or all portions of the Work, all re-design, re-bid or other services provided by Engineer shall be paid by Client on an hourly basis at Engineer's customary hourly rates, plus 1.1 times reimbursable expenses.

INDEMNITY. Client indemnifies, defends and holds harmless Engineer and its agents, consultants and employees, from and against any claim, injury, damage, cost, expense or liability, regardless of the legal theory, including actual attorneys' fees, whether arising before, during or after completion of Services performed under the PSA, caused by, arising out of, resulting from or occurring in connection with the performance of the Services or any activity associated with the Services, whether or not caused in part by the active or passive negligence or other fault of Engineer excepting only injury to person or damage to property caused by the sole negligence of Engineer. In the case of claims against Engineer or any of its consultants, agents or employees by anyone for whose acts Client may be liable, this indemnification obligation shall not be limited in any way by any limitation on the amount or type of damages, compensation or benefits payable under workers' compensation acts and/or disability benefit acts. This indemnity includes, but is not limited to, any claims resulting from interpretation of or changes to the documents prepared as a result of this PSA. This indemnity survives termination of this PSA.

In addition to the indemnity provided herein by Client, Client shall indemnify and hold harmless Engineer and its officers, directors, partners, agents, employees and consultants from and against any and all claims, costs, losses, and damages (including but not limited to all fees and charges of Engineer, architects, attorneys and other professionals, and all court, arbitration, or other dispute resolution costs) caused by, arising out of, relating to or resulting from any and all environmental contamination on the Project.

Engineer, to the extent covered by insurance, indemnifies, defends and holds harmless Client and its agents and employees, from and against any claim, injury, damage, cost, expense or liability, arising out of or relating to the Services provided by Engineer for the Project, provided that any such claim, cost,

loss, or damage is attributable to bodily injury, sickness, disease, or death, or injury to or destruction of tangible property other than the work itself, including the loss of use resulting therefrom, but only to the extent caused solely by any negligent act or omission of Engineer or Engineer's officers, directors, partners, employees or consultants.

To the fullest extent permitted by law, a party's total liability to the other party under the terms and conditions of this PSA including any indemnity, as well as to anyone claiming by, through or under the other party, for any cost, loss, or damages caused in part by the negligence of the party and in part by the negligence of the other party, and any other negligent entity or individual, shall not exceed the percentage share that the party's negligence bears to the total negligence of all of the responsible parties.

PERFORMANCE STANDARDS. In the performance of this PSA, Engineer shall exercise the ordinary standard of care of Engineers in the city, municipality or political subdivision where the Project is located.

LIMITATION OF LIABILITY. Engineer shall not be liable for any claim, damage, cost, expense or other liability not caused by negligent acts, errors or omissions of Engineer. The total liability of Engineer under any legal theory whatsoever, in the aggregate, as well as any claimed liability of Engineer's officers, directors, employees, or agents or consultants, for any claims arising out of the PSA, shall not exceed the fees actually paid by the Client for the particular Service which forms the basis of the claimed liability.

Engineer makes no warranties, express or implied, with respect to the Services under the PSA, and disclaims any liability for implied warranties of any type or kind, including but not limited to implied warranties of fitness or merchantability, and disclaims any liability for special or consequential damages of any type or kind. Within these limitations, Engineer shall not be liable in any way for errors, omissions or negligence unless caused by the sole and exclusive negligence of Engineer. For all PSAs which involve multiple projects or general consultations, or various services for various projects over a period of time, liability shall not exceed the fee actually paid by the Client for the particular Services on the specific Project or consultation or assignment which forms the basis of the claimed liability and any statute of limitations shall commence upon the completion of the task giving rise to the claim, not the last unrelated service provided under the PSA for general consultation services. To the extent that Engineer may be found liable under the terms of this paragraph, and only to such extent, Engineer's liability shall not exceed the percentage share of Engineer's responsibility.

INSURANCE. Upon Client's request, Engineer will furnish Client with a written statement of insurance coverage. No oral representations regarding insurance shall be binding.

SITE ACCESS. Client shall be solely responsible for obtaining all site access, easements, and permission from third party property owners for Engineer to access the site to perform the Services herein. Client is solely responsible for any claims arising from the disturbance of surface or subsurface soil or water conditions caused by the performance of Engineer's services, excepting damages caused by the sole negligence of Engineer. Engineer will take reasonable precautions to avoid damage to underground structures and utilities. Client indemnifies Engineer from any damage caused by or to underground structures and utilities not called to Engineer's attention, all in accordance with the indemnity provisions herein. Client shall provide Engineer with a list of all known hazardous substances on site and a list of protective measures in case of exposure, all in compliance with the current Federal, State and Local Right to Know laws and Federal Hazard Communication Standards.

DRAWINGS, SHOP DRAWINGS AND SUBMITTALS. At all times, and for all purposes, Engineer is the sole and exclusive owner of all drawings and Contract Documents prepared by it. Engineer shall have no liability of any type or kind to any person for Client's use of any Engineer prepared drawings or Contract Documents following completion or termination. Engineer shall have no liability for erroneous record drawings or electronically manipulated drawings based on information provided by others. If shop drawing or submittal review is part of the Services Engineer provides, Engineer will review the shop drawings and submittals only for conformance with the design concept of the project and compliance with the Contract Documents. Unless specifically indicated in writing, this PSA does not include the preparation of record drawings.

CONSTRUCTION PHASE SERVICES. Unless specifically included in the Scope of Services, there are no construction phase services as part of this PSA and Engineer assumes no liability with regard to construction being in compliance with construction Contract Documents. Client assumes all responsibility for construction observation and any errors discovered during construction while Engineer is not involved with the Project. If Engineer has no construction phase responsibilities, Client assumes all responsibilities for document interpretation and, unless specifically indicated to the contrary in writing describing construction observation services and the resultant compensation, Client waives any claim against Engineer in any way connected to document interpretation or construction observation or the lack thereof. Engineer does not warrant construction quality.

REJECTION OF WORK. In the event that the Scope of Services includes construction phase services in the form of site observation, then Engineer shall have the authority to reject any work which is not, in the judgment of the Engineer, in conformance with the Contract Documents, Plans and Specifications. Neither this authority nor Engineer's good faith judgment to reject or not reject any work shall subject Engineer to any liability or cause of action to any contractor, subcontractor, supplier, Owner or Client on the Project.

SPREAD OF CONTAMINATION. Client understands and agrees that Engineer shall not be responsible for any claims or damages which may arise as a result of or from the spread of contamination caused by drilling, sampling or any other activity unless such spread or contamination is substantially caused by the negligence of Engineer. To the extent that Engineer may be found liable under the terms of this Paragraph, and only to such extent, Engineer's liability shall not exceed the percentage share of Engineer's responsibility.

FAILURE TO ENCOUNTER HAZARDOUS MATERIALS. Client understands that the failure to discover hazardous materials does not guarantee that; (1) hazardous materials do not exist at the project site, and/or (2) that a non-contaminated site may later become contaminated. Although Engineer will use reasonable care and a level of skill ordinarily exercised by members of the profession currently practicing in the city, municipality or political subdivision where the Project is located under similar conditions, Client agrees that Engineer shall not be responsible for the failure to detect the presence of hazardous materials through techniques and practices commonly used for those purposes.

PERMITS AND APPROVALS. Unless otherwise specifically stated in the Scope of Services, obtaining permits and approvals for the Project is the responsibility of the Client. For an additional fee, Engineer may assist the Client provided the assistance shall consist of completing and submitting forms as to the results of certain work included in the Scope of Services and the assistance does not include special studies, special research, attendance at meetings with public authorities, special testing or special documentation not normally required for similar projects. If Engineer participates in any way with any permitting process, Engineer provides no guaranty or warranty that any permits or approvals will be provided. Client shall pay Engineer for all fees and reimbursable expenses under this PSA regardless of the outcome of approval or denial of permits or other approvals.

ADA AND CODE COMPLIANCE. The Americans with Disabilities Act ("ADA") provides that alterations to a facility must be made in such a manner that, to the maximum extent feasible, the altered portions of the facility are accessible to persons with disabilities. The Client acknowledges that the requirements of the ADA will be subject to various and possibly contradictory interpretations. To the extent applicable, the Engineer will use its reasonable professional efforts and judgment to interpret applicable ADA requirements and other federal, state and local laws, rules, codes, ordinances and regulations as they may apply to the Project. The Engineer does not warrant or guarantee that the Project will comply with all interpretations of the ADA requirements and/or

the requirements of other federal, state and local codes, rules, laws, ordinances and regulations as they may apply to the Project. Client shall pay Engineer its customary hourly fees plus 1.1 times reimbursable expenses for any design changes made necessary by newly enacted laws, codes and regulations, or changes to any existing laws, codes or regulations after the date that this PSA is last signed by the parties.

WAIVER. No delay on the part of any party hereto in the exercise of any right or remedy shall operate as a waiver of such right or remedy and a waiver on any one (1) occasion shall not be construed as a bar to or a waiver of any subsequent breach of the same or any other provision of the agreement on a future occasion. No waiver by Engineer of any breach by Client of a provision of this PSA shall be deemed a waiver of any other provision hereof or of any subsequent breach by Client of such provision.

ENFORCEABILITY. This agreement shall be binding upon the parties hereto and their respective successors and assigns.

SEVERABILITY. In the event that any one (1) or more provisions contained in the agreement shall be declared invalid, illegal or unenforceable in any respect, the validity, legality and enforceability of the remaining provisions of the agreement shall not be affected or impaired.

OWNERSHIP OF INSTRUMENTS OF SERVICE. Engineer's documents prepared pursuant to this PSA, including those in electronic format, are instruments of service. All reports, plans, specifications, computer files, field data, notes and other documents prepared by Engineer as instruments of service shall remain the property of Engineer. Engineer shall retain all common law, statutory and other reserved rights, including the copyright thereto and all other intellectual property rights. Client shall not use or permit the use of said documents on any other project. Client fully indemnifies Engineer against any and all claims for unauthorized use.

STAMP CERTIFICATIONS. Client shall indemnify Engineer from claims arising out of any certifications which may be required to be signed on behalf of Client during the course of the Project.

ONLY AFTER PAYMENT. Engineer will not stamp drawings produced for any phase of the Project under the terms of this PSA until all invoices billed to that point in the project have been paid in full.

TERMINATION: This PSA may be terminated by either party upon seven (7) days written notice. Upon termination, Engineer shall be paid by Client for all Services performed up to the notice of termination, as well as all costs necessary to demobilize from the site.

DISPUTE RESOLUTION: Claims and disputes arising out of or relating to this PSA involving claims in the aggregate of less than twenty-five thousand dollars, (\$25,000.00) without interest or attorneys fees and without consideration of counterclaims, shall be decided by a court of competent jurisdiction exclusively in Kent County, Michigan. Claims and disputes arising out of or relating to this PSA involving claims in the aggregate of greater than twenty-five thousand dollars (\$25,000.00) shall be decided by arbitration in accordance with the applicable rules of the American Arbitration Association. There shall be a single arbitrator. The award shall be final and binding and enforceable in a court of competent jurisdiction. In either arbitration or litigation, the prevailing party shall be entitled to recover its attorney's fees and costs through all levels of appeal. Jurisdiction, venue and the hearing locale for all arbitrations or litigation shall lie exclusively in Kent County, Michigan.

NO THIRD PARTY BENEFICIARIES. There are no third party beneficiaries to this PSA and the Services provided herein are exclusively for the direct benefit of the Client indicated above. Client shall ensure that all other agreements relating to this project reflect that there are no third party beneficiaries to this PSA.

ASSIGNMENT. This is a professional services contract and is non-assignable without the express written consent of Engineer.

MISCELLANEOUS. No additional or contrary terms, whether contained in an order, acknowledgment, or other document from Client, shall be binding upon Engineer unless agreed to in writing signed by an authorized representative of Engineer, and Engineer expressly rejects all such additional or contrary terms as may be contained in Client's documents. The terms in this PSA will have precedence over any other terms expressed by the Client's authorization process such as a purchase order. Engineer's performance is conditioned on Client's unmodified consent exclusively to this PSA. Engineer shall have the right to correct any errors, whether clerical or mathematical, which are contained in this PSA. Unless otherwise specifically indicated in writing or otherwise required by law and paid for by Client, there are no Performance or Payment bonds required on this Project. This PSA shall be binding upon and shall inure to the benefit of the parties hereto and their successors and permitted assigns. This PSA shall be governed by the laws of the State of Michigan. This contract sets forth the entire agreement between Engineer and Client. This is a fully integrated contract.

ELECTRONIC/FACSIMILE SIGNATURES. The signatures on this PSA shall be deemed to be original signatures when transmitted electronically or by facsimile machine or by any other medium. No party shall be required to produce a PSA with an original signature in order to enforce any provision of this PSA.

IN WITNESS WHEREOF, the parties hereto have made and entered into this PSA. To be valid, this PSA must be signed by an authorized representative of Fleis & VandenBrink Engineering, Inc.

CLIENT
DESIGNHAUS

ENGINEER
FLEIS & VANDENBRINK ENGINEERING, INC.

By: _____
Mike Pizzola

Title: _____

Date: _____

By: 
Julie Kroll

Title: **Traffic Engineering Services Manager**

Date: **February 18, 2021**

By: 
Gary Bartow

Title: **Group Manager**

Date: **February 18, 2021**

January 26, 2026

City of Taylor Planning Commission
City of Taylor
23555 Goddard Road
Taylor, Michigan 48180

**RE: Request for Condition Rezoning for Property located at
8715 Telegraph Road**

Dear Members of the Planning Commission:

Designhaus, as applicants representing the property owner respectfully request conditional rezoning for the property located at 8715 Telegraph Road from the zoning current zoning designation B-3 to 1-1. The property is located in the City of Taylor, Michigan. The legal descriptions is as follows: See 'Exhibit A' for written description and property sketch.

The rezoning of the subject property to 1-1 is proposed with the following conditions:

(1) See attached 'Exhibit B'

Designhaus, as the applicant, representing the property owner, agree and understand the following items found in Section 27.05 of the City of Taylor Zoning Ordinance:

- Agreement and understanding that the property in question shall not be developed or used in a manner inconsistent with the conditional rezoning agreement.
Agreement and understanding that the approval and conditional rezoning agreement shall be binding upon and inure to the benefit of the property owner and the City, and their respective heirs, successors, assigns, and transferees.
Agreement and understanding that, if this rezoning with conditions becomes void in the manner provided in section 27.05 of the City of Taylor Zoning Ordinance, no development shall be undertaken or permits for development issued until a new zoning district classification of the property has been established.
Agreement and understanding that each of the requirements and conditions in the conditional rezoning agreement represents a necessary and reasonable measure which, when considered with all other conditions and requirements, is roughly proportional to the increased impact created by the use represented in the approved rezoning with conditions, taking into consideration the changed zoning district classification and the specific use authorization granted.
Agreement and understanding that revocation of the approval provisions returning the property to its original zoning designation will occur if the developer violates the terms of this agreement.

Sincerely,



PLANNING COIVIMISSION REVIEW AND PUBLIC HEARING

CITY OF TAYLOR PLANNING COMMISSION REVIEW AND RECOMMENDATION

After proper public notice was given and a public hearing held on _____ 2026, before the City of Taylor Planning Commission, a motion was duly made and approved as recorded in the minutes of the Planning Commission meeting of the same date recommending the City Council approve this Conditional Rezoning Agreement.

Robert Szwed, Chair

Christopher Siemion, Secretary

SIGNATURE AND NOTARIZATION

IN WITNESS WHEREOF, the undersigned has executed this CONDITIONAL REZONING AGREEMENT for and on behalf of the date first above written.

Applicant:

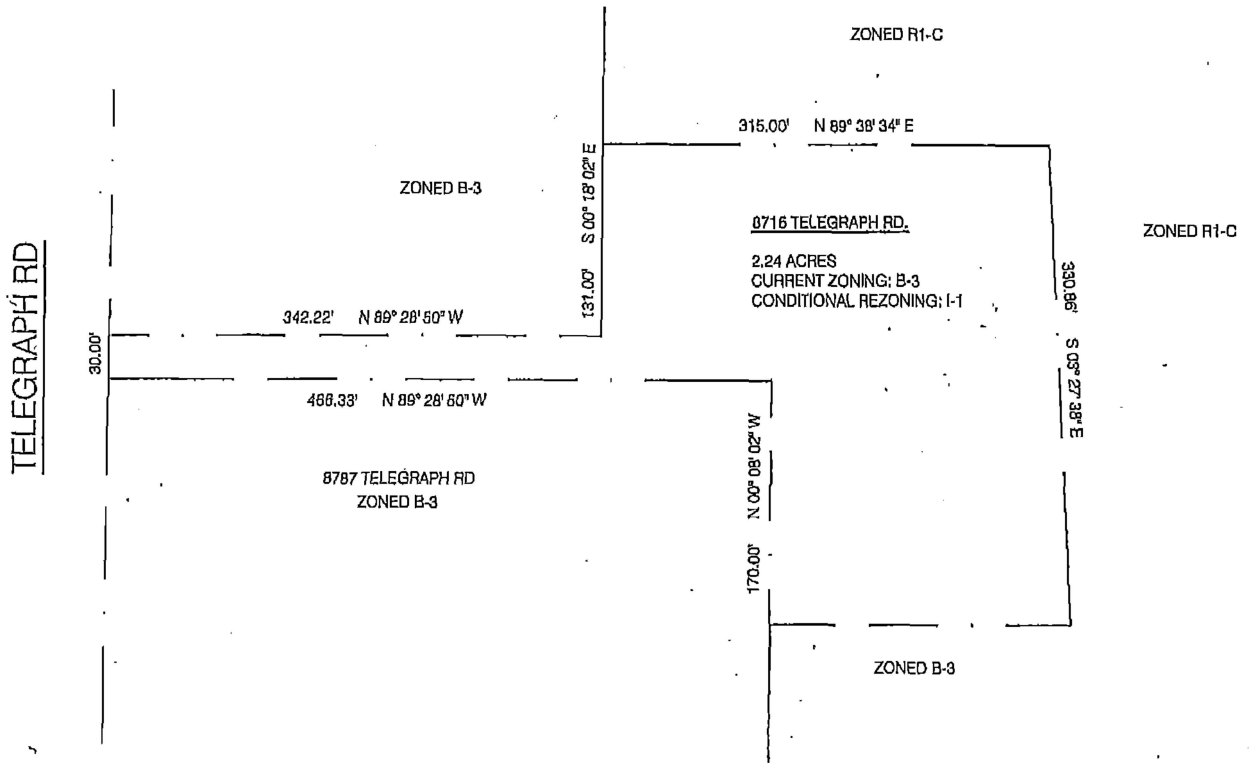
Peter Stuhlreyer
3300 Auburn Rd.
Auburn Hills, MI 48326

STATE OF MICHIGAN)
)SS.
COUNTY OF)

On this ____ day of _____, 2021, before me, a Notary Public, in and for said County, personally appeared Peter Stuhlreyer.

Notary Public
Acting in _____ County, Michigan
My Commission Expires:

Exhibit A



Legal Description: 09N1A1B N1B1B PART OF THE SW 1/4 SECTION 9 T3S R10E BEG N0DEG 08M 02S W
 1,493.73' FROM SW CORNER SEC 9 THENCE N0D 0M 02S W 40' THENCE N89D 28M 50S E
 342.44' THENCE N0D 08M 02S W 121.03' THENCE N89D 38M 34S E THENCE S3D 27M 30S E 330.86' THENCE
 S89D 28M 50S W 210.01' THENCE N0D 08M 02S W 170' THENCE S89D 28M 50S W 466.33' TO THE P.O.B.
 CONTAINING 2.25 ACRES



EXHIBIT B

The Re-Zoning of the subject property to I-1 (Light Industrial) is proposed with the following conditions:

1. The proposed design shall greatly enhance the full perimeter, north, south, east and west with a heavy tree and landscape buffer including no *less* than 79 deciduous trees, 124 evergreen trees, and 248 ornamental shrubs. The Buffer will be a minimum 20' in width.
2. The proposed design shall remove existing parking lot lighting and replace them with 15' tall fully shielded LED lights, equipped with a timing system and motion detection. These lights shall be located as to not exceed 1 footcandle at the inside edge of the asphalt paving. Light source shall not be visible from residential areas.
3. The proposed design shall reuse, in entirety, the existing building, thus repurposing the embodied energy of the structure as opposed to consuming the energy required to remove and rebuild an entirely new project.
4. The proposed design shall not encroach to the east or north of the existing building.
5. The proposed design shall reduce the impervious surface areas by over 10,000 sf, creating a positive effect on the UHI (Urban Heat Island).
6. The proposed design shall add a storm water bioswale with natural vegetation and grasses into which rain run-off will be collected from the roof drains and adjacent asphalt paving.
7. The proposed design shall have all new and replacement roofing be specified as white in color creating a positive effect on the UHI (Urban Heat Island).
8. The proposed design shall limit the hours of operation of the rental office to 8AM-8PM.
9. The proposed design shall include interior and exterior security cameras.
10. The proposed design shall include access controls throughout, with access granted only to customers and staff, and only during business hours.

11. The proposed design shall include repairs to the existing masonry screen wall which runs the entire property line along the residential use.
12. The proposed design shall screen exterior doors from the ROW and the residential district using the enhance perimeter tree and shrub buffer.
13. The proposed design shall include the repainting of the masonry building walls facing the residents to an inconspicuous neutral grey-taupe color.
14. The proposed business operation shall prohibit waste removal vehicles between the hours of 5PM and 9AM.



MCKENNA

February 24, 2026

Planning Commission
City of Taylor
23555 Goddard Road
Taylor, Michigan 48180

Attention: **Ms. Lora Fell, Planning Director**

Subject: **Request for Amendment to the Official Zoning Map: Rezoning from B-3, General Business District to I-1, Light Industrial – Conditional.** *(Site is approximately 2.23 acres in area.)*

Location: **8715 Telegraph Road – Tax ID #60-027-99-0004-004.** *(Located on the east side of Telegraph, between Wohlfeill St and Mary St.)*

Dear Commissioners,

At the City’s request, we have conducted a review of the proposed rezoning for a site located at 8715 Telegraph Road from B-3, General Business District to I-1, Light Industrial. The applicant is proposing the construction of an indoor climate controlled self-storage facility, with the condition that this would be the only use that would be permitted on the site. We have reviewed the proposed rezoning for consistency with the 2014 City of Taylor Master Plan and sound planning and zoning principals. We offer the following comments for your consideration.

DESCRIPTION OF SITE AND SURROUNDING AREA

Figure 1 depicts the boundary of the area requesting to be rezoned. The site is comprised of one parcel totaling approximately 2.23 acres in area. The site currently holds a vacant building and parking lot. The site is currently the Coliseum Racquet Club, though the unique size and dimensions of the building and parcel limit the possibilities of redevelopment options.

The existing land use, zoning, and future land use designations for the site and surrounding parcels are summarized in the following table and map excerpts.



HEADQUARTERS
235 East Main Street
Suite 105
Northville, Michigan 48167

○ 248.596.0920
F 248.596.0930
MCKA.COM

Communities for real life.



Table 1: Zoning, Existing and Future Land Use of Site and Adjacent Property

	Existing Land Use	Current Zoning	Future Land Use Designation
Site	Vacant building	B-3	Community Commercial
North	Restaurant / Single-family Residences	B-3 & R-1C	Community Commercial & Low-Intensity Residential
South	Restaurant / Grass Field	B-3 & R-1C	Community Commercial & Low-Intensity Residential
East	Single-Family Residences	R-1C	Low-Intensity Residential
West	Various Businesses and Offices	B-3	Medium-Intensity Residential

Figure 2: Zoning Map of Area

Zoning Districts

- R-1C Single-Family Residential District
- RM-1 Multiple-Family Residential District
- B-2 Regional Business District
- B-3 General Business District
- P-1 Vehicular Parking

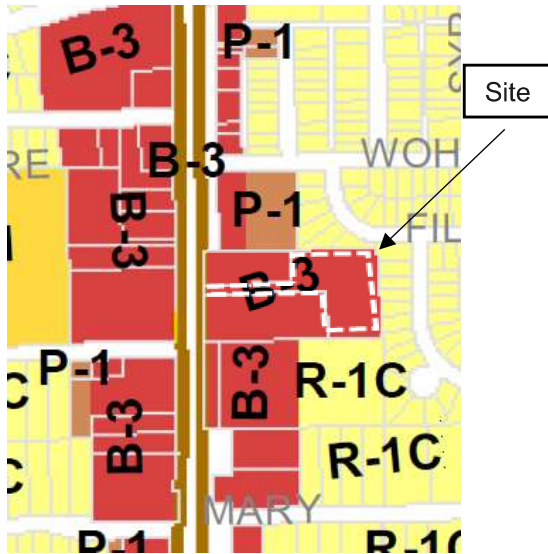
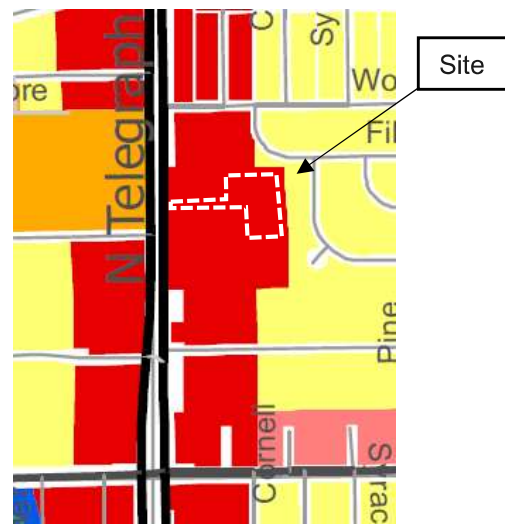


Figure 3: Future Land Use Map of Area

Future Land Use Categories

- Low-Intensity Residential
- Medium-Intensity Residential
- Neighborhood Commercial
- Community Commercial
- Public





REVIEW CRITERIA FOR REZONING

Per Section 27.04 of the City Zoning Ordinance, there are seven criteria that the City must consider when making its findings regarding a proposed amendment to the zoning map. We offer the following comments regarding those criteria below.

1) Consistency with City Master Plan and Subarea Plans.

The proposed use of the site is a self-storage facility. The 97,139 Sq. Ft. (2.23 acre) property is currently occupied by a 2-story building with a first-floor area of 20,000 Sq. Ft. A large parking lot also is present on site.

The site falls under the Telegraph Road Corridor in the Subarea Map of the Master Plan. This road is the most heavily traveled corridor; it is expected to serve as a high-trafficked commercial corridor.

A standard I-1 Light Industrial designation would not be consistent with the Master Plan's Future Land Use designation. A standard I-1 district would allow uses that are not compatible with being adjacent to single family homes. Industrial use characteristics such as excessive noise levels and odor generation would be incompatible with adjacent residences. However, the proposed conditional I-1 rezoning would restrict the permitted uses to only an interior self-storage facility. This type of facility would not generate any significant noise and would not conduct any production or manufacturing activities. The proposed use would enhance compatibility with adjacent uses by enhancing the landscape buffer for the existing residences.

Given that the proposed conditional I-1 rezoning would redevelop a vacant building and the proposed self-storage use will conduct all activities within the building, the requested rezoning could be consistent with the intent of the City's Master Plan.

2) Compatibility of the site's physical characteristics with the uses of the proposed district.

The site is currently comprised of pavement and building footprint. In fact, the applicant proposes to reduce the size of the parking lot and replacing it with landscaping in the southeast corner of the site. The surrounding residential properties can be screened with opaque plantings of trees and shrubs, which will further increase the landscaping on site.

The proposed conditional rezoning would be compatible with the site's physical features because it would result in improving the existing landscape plantings and features.

3) Development trends relating to requested zoning district or current district.

There are currently five self-storage facilities within the City boundaries, though there are more in proximity to the City. These sites are located on busy, main roads, including Telegraph Rd and Allen Rd. Development of self-storage facilities have seen a sharp increase in recent years throughout the Metro-Detroit area, though the demand for them in the City of Taylor is unknown.

4) Compatibility of proposed zoning district uses with surrounding uses.

As noted above, the proposed conditional I-1 zoning would only permit an indoor self-storage facility and prevent the establishment of any manufacturing use or outdoor storage yard. We do not anticipate that the use would be incompatible with the adjacent commercial properties, which are currently restaurants. Just southeast of the site is a U-Haul moving and storage facility. The proximity to this business may actually generate customers for the proposed development, therefore increasing the economic vitality of this business.



Typical industrial uses are a higher intensity of use than single-family residential developments. To the north and east of the site are existing single-family developments. The conditional I-1 rezoning of the subject site would limit the extent of the development. Since the site has an existing building which will be repurposed with an addition, the majority of the site would remain similar in character as it is today. The site would contain landscaping to act as a buffer for the surrounding uses. The proposed redevelopment of this site must not include the encroachment of the necessary setbacks.

5) Capacity of City infrastructure to accommodate proposed zoning district uses.

The site currently holds an existing building, one which is already serviced by public utilities. The proposed addition would have little to no impact on the City's infrastructure. In fact, it is estimated that the change of use of the building would in turn present a low-usage facility in terms of electricity, water, and sewer. Visitors are not expected to be in the facility for an extended period, meaning that customer usage of the bathroom and electricity is low. The applicant has proposed improvements that would decrease utility usage, such as motion sensor lighting.

6) Demand for uses in the proposed zoning district.

The Master Plan has dedicated subareas for industrial zones. The City currently has many parcels with industrial uses. The demand for self-storage facilities by Taylor residents is unknown. While there is land currently planned and/or zoned in the City that could be developed to meet this land use, development of this site will repurpose a long-time vacant site and building that many developers have not attempted to reutilize.

7) Whether this application meets the submittal requirement of at least one (1) year between requests for rezoning.

City staff have indicated that there has not been a request to rezone this site within the past year, with the last being in 2024. The application for rezoning meets this zoning ordinance standard.

8) In addition to the seven criteria above, a determination shall be made that the requested zoning district is more appropriate than another district or amending the list of permitted or special land uses within a district.

Amending the list of permitted uses in the B-3 district to permit self-storage might be inconsistent with the general separation of uses and maintaining compatible uses within a single zoning district because the intent of B-3 is to serve as the locations that provide for retail commercial services. While B-3 is the most intensive of the three business districts, the self-storage land use is most compatible in the I-1 district.

CONCLUSION AND RECOMMENDATION

The proposed conditional rezoning would redevelop a long-term vacant building and improve the visual appearance of the building. A buffer would exist for the residences to the north and east. We advise the Planning Commission to recommend approval of the rezoning to the City Council based on the following findings of fact:

1. The proposed development would repurpose an existing, vacant building, rather than impact natural features through demolition and construction.
2. Capacity of the city infrastructure to accommodate the proposed use and limit the need for any utilities in the future.
3. The proposed use is low intensity, combined with dense buffering to the adjacent residential properties, this use is projected to have little impact on the residents.

We look forward to reviewing these findings with you. Please feel free to contact us with any questions.



Respectfully submitted,

McKENNA

Mario A. Ortega, AICP
Senior Principal Planner

Sommer Nafal, NCI
Assistant Planner

TAYLOR PLANNING COMMISSION MEETING MINUTES
WEDNESDAY, MARCH 4, 2026

The Taylor Planning Commission meeting was held in the Taylor Municipal Building, 23555 Goddard Road, on Wednesday, March 4, 2026 at 7:00 P.M., Chairman Kenneth Stewart presiding.

Pledge of Allegiance

Present: Messrs. Mayor Woolley, Stewart, Fisher, Siemion, Johnson, Ghallozi and Styles

Excused: Mr. Welton

Also Present: Mayor Tim Woolley, Ms. Lora Fell, Planning Director.

Motion by Johnson Supported by Siemion

Resolved: To approve the agenda as received.

Unanimously carried.

Resolution Number 3.18-26

Motion by Johnson Supported by Siemion

Resolved: To approve the minutes of the February 18, 2026, meeting and dispense with the oral reading.

Unanimously carried.

Resolution Number 3.19-26

LEGISLATIVE ACTION

Case #26-03(ZC) – 8715 Telegraph – conditional rezoning
Tax I.D.#60-027-99-0004-004

Parcel #60-027-99-0004-004 of the SW ¼ Section 9, T.3S., R.10E., City of Taylor, Wayne County, Michigan be conditionally rezoned from B-3 (General Business) to I-1 (Light Industrial). The property is located on the east side of Telegraph Road between Wohifeil and Mary Streets. This is a conditional (contract) rezoning.

Ms. Fell gave a brief summary of the history of this development and prior approvals. This request is the same as the request that the Planning Commission recommended approval on in 5/19/21 and 11/20/24.

Mr. Hunter Galbraith, project manager gave a brief explanation of the proposed contract rezoning and the proposed future development. He there were no changes to the conditions from the last submittal.

Mayor Woolley spoke in support of this contract rezoning based on the its location and setback and the conditional items offered.

Discussion included: Past review of the rezoning and Planning Consultant's recommendation.

TAYLOR PLANNING COMMISSION MEETING MINUTES

WEDNESDAY, MARCH 4, 2026

Page 2

Motion by Ghallozi

Supported by Styles

Resolved: To recommend to City Council that Case #26-03 (ZC) be approved, based upon the Planning Consultant's recommendation and the criteria of Section 27.05(c)(2) items "a" - "f", with the 10 conditions offered up by the applicant and the 4 items agreed to by the applicant from the Planning Consultant.

Unanimously carried.

Resolution Number 3.20-26

There was no other business.

Motion by Fisher

Supported by Siemion

Resolved: That the meeting be adjourned.

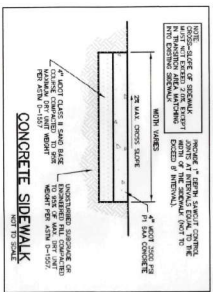
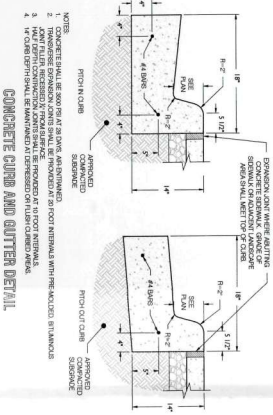
Unanimously carried.

Resolution Number 3.21-26

The meeting adjourned at 7:10 p.m.

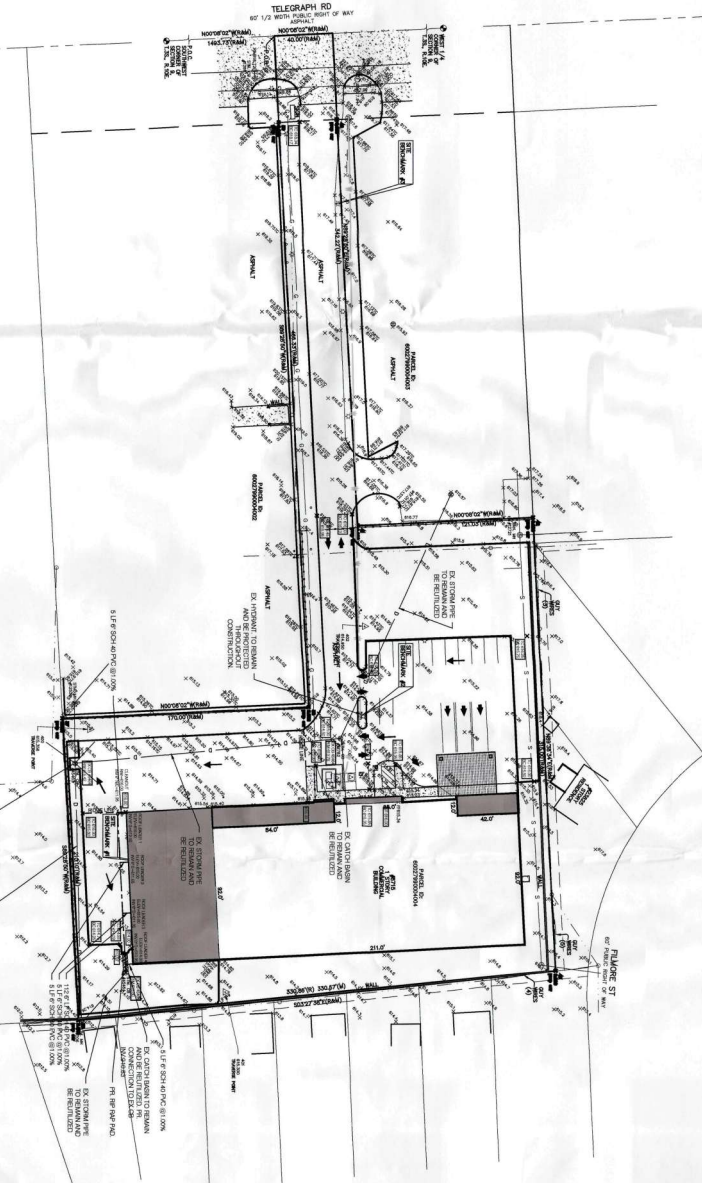
Kenneth Stewart, Chairman

Cynthia A. Bower, City Clerk



MANHOLE SCHEDULE

#	TYPE	RM	SIZE	DIRECTION	INVERT
1239	STORM MANHOLE	63.504	12	E	610.79
1247	CATCH BASIN	63.830	12	NW	613.54
1271	STORM MANHOLE	63.935	10	W	613.45
1272	STORM MANHOLE	63.935	10	E	609.08
1273	CATCH BASIN	63.935	10	N	609.39
1274	STORM MANHOLE	63.935	10	NW	609.28
1275	STORM MANHOLE	63.935	10	E	609.08
1276	CATCH BASIN	63.935	10	W	608.72
1277	CATCH BASIN	63.935	12	W	613.87
1278	CATCH BASIN	63.935	12	E	607.80
1279	CATCH BASIN	63.935	12	NW	607.32
1280	CATCH BASIN	63.935	12	SW	609.32
1281	CATCH BASIN	63.935	6	NE	609.32
1282	CATCH BASIN	63.935	6	SE	609.32



- GRADING NOTES**
1. ALL WORKMANSHIP AND MATERIAL SHALL BE IN ACCORDANCE WITH THE CURRENT STANDARDS AND SPECIFICATIONS OF CITY OF TAYLOR AND WAYNE COUNTY.
 2. IN AREAS WHERE NEW PAVEMENTS ARE BEING CONSTRUCTED, THE TOPSOIL AND SOIL CONTAINING ORGANIC MATTER SHALL BE REMOVED PRIOR TO PAVEMENT CONSTRUCTION.
 3. ON SITE FILL CAN BE USED IF THE SPECIFIED COMPACTION REQUIREMENTS CAN BE ACHIEVED. IF ON SITE SOIL IS USED, IT SHOULD BE CLEAN AND FREE OF FROZEN SOIL, ROCKS, OR OTHER DEBRIS MATERIALS.
 4. THE FINAL SUBGRADE/EXISTING AGGREGATE BASE SHOULD BE PROTECTED FROM OVERSIGHT AND DAMAGE. THE OBSERVATION OF A GEOTECHNICAL/PAVEMENT ENGINEER, LOOSE OR YIELDING AREAS THAT CANNOT BE MECHANICALLY STABILIZED SHOULD BE REINFORCED USING GEOTEXTILES OR REMOVED AND REPLACED WITH ENGINEERED FILL OR AS DIRECTED BY FIELD CONDITIONS.
 5. SUBGRADE UNDERLAYING, INCLUDING BACKFILLING, SHALL BE PERFORMED TO REPLACE MATERIALS SUSCEPTIBLE TO FROST BELOW THE TOPSOIL. IN FILL SECTIONS OR BELOW SUBGRADE IN CUT SECTIONS, WILL BE CLASSIFIED AS SUBGRADE UNDERLAYING.
 6. SUBGRADE UNDERLAYING, INCLUDING BACKFILLING, SHALL BE PERFORMED WHERE NECESSARY AND THE EXCAVATED MATERIAL SHALL BE RECYCLED OR REUSED. THE PROPERTY OF THE CONTRACTOR ANY SUBGRADE UNDERLAYING SHALL BE BACKFILLED WITH SAND OR OTHER SIMILAR MATERIAL UNLESS OTHERWISE SPECIFIED.
 7. BACKFILL UNDERLAYING SHALL BE AS SPECIFIED ON DETAILS.
 8. ANY SIDEWALK WATERING REQUIRED TO ACHIEVE REQUIRED DENSITY SHALL BE CONSIDERED INCIDENTAL TO THE JOB.
 9. FINAL PAVEMENT ELEVATIONS SHOULD BE SO DESIGNED TO PROVIDE POSITIVE SURFACE DRAINAGE. A MINIMUM SURFACE SLOPE OF 1/8" PER FOOT IS REQUIRED UNLESS OTHERWISE SPECIFIED.
 10. CONSTRUCTION METHODS SHOULD BE HANDED ON THE NEW PAVEMENT. THE CONSTRUCTION THEREOF IS ANTICIPATED ON THE EXISTING SITEWORK. THE FINAL LEFT THICKNESS COULD BE INCREASED AND PLACEMENT OF THE CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED. THIS ACTION WILL ALLOW REMOVAL OF EXISTING PAVEMENT. FINISH DOGS OCCUR AS WELL AS REDUCE LOAD BOUNDS ON THE PAVEMENT SYSTEM.
 11. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH CITY OF TAYLOR AND SPECIFICATIONS.
 12. SPOT ELEVATIONS INDICATE TOP-OF-PAVEMENT UNLESS OTHERWISE INDICATED.
TC = TOP-OF CURB
BC = BOTTOM-OF CURB
 13. REFER TO LANDSCAPE PLAN FOR LANDSCAPE TREATMENT AND PLACEMENT.

GENERAL UTILITY NOTES

1. EXISTING UTILITIES ARE TO BE RELOCATED AND PROTECTED DURING CONSTRUCTION.

GRADING LEGEND

X 000.00	EXISTING GRADES
X 100.00	PROPOSED GRADES
---	PROPERTY LINE
---	PROPOSED HOUL LINE

1 Grading & Utility Plan
Scale: 1" = 40'

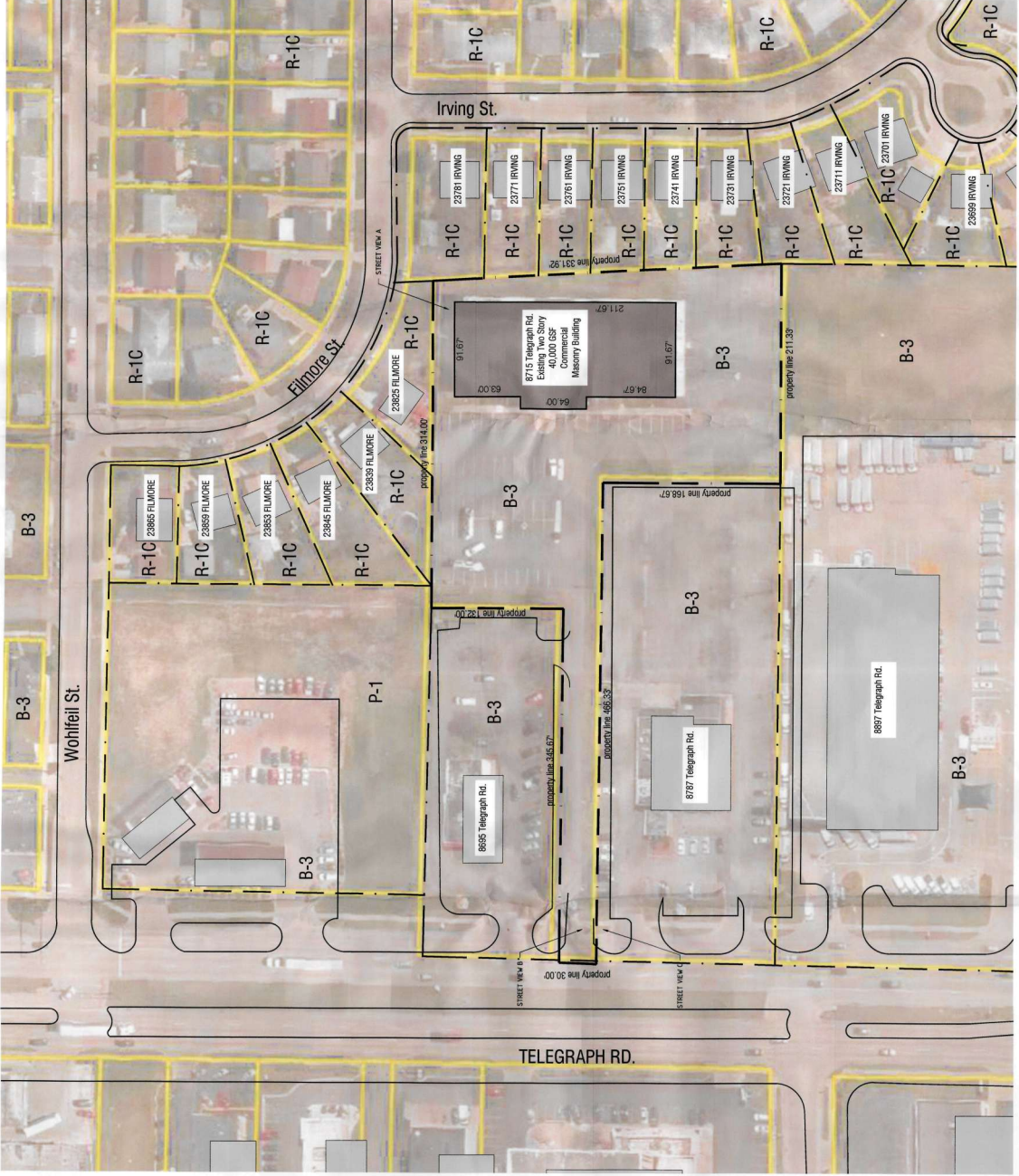
DRAINAGE REQUIREMENTS

DEVELOPMENT	IMPERVIOUS AREA	PERVIOUS AREA	TOTAL AREA
POST-DEVELOPMENT	14,261 SF (1.44 AC)	37,262 SF (0.85 AC)	51,523 SF (1.29 AC)
POST - PRE	12,623 SF (0.28 AC)	12,053 SF (0.28 AC)	

<p>S.M. ENGINEERS CIVIL ENGINEER/SURVEY/ LANDSCAPE ARCHITECT 1939 Lincolnpine Rochester Hills, MI 48065 T: 248.835.3553</p>	<p>2 SITE PLAN REVIEW 04.06.2022</p>
	<p>1 SITE PLAN REVIEW 02.14.2022</p>
<p>TAYLOR STORAGE 6715 Telegraph Road Taylor, MI 48065</p>	<p>NO. ISSUE DATE</p>
<p>GRADING PLAN</p>	<p>C4.0</p>
<p>01300</p>	<p>01300</p>

1 Existing Conditions Plan

SCALE: 1" = 40'



STREET VIEW A



STREET VIEW B



STREET VIEW C

020263

S100

Existing Conditions Plan
Storage Plus
8715 Telegraph Road
Taylor, Michigan 48180



Revision/Issue	Date
SPA Review	8.3.21
ZDA Submittal	24.03.22
50% CD	06.04.22
SPA Revision #1	05.25.22

3300 AUBURN RD, SUITE 300
AUBURN HILLS, MI 48326
T:248.601.4422 F:248.453.5854
WFO@DESIGNHAUS.COM
INFO@DESIGNHAUS.COM

DESIGNHAUS
ARCHITECTURE
1998 EST



ZONING SCHEDULE OF REGULATIONS

Regulation	Required	Provided
Setbacks	(B3 Zoning) Front - 40' Sides - 10' Rear - 20'	EXISTING BUILDING Front: 472.00' Sides: 20.00' / 99.58' Rear: 20.00'
Building Height	3 Stories / 95' (Per B-3 Zoning)	32' - 0"
Minimum Lot Area	N/A	70,769 Sq. Ft.
Maximum Lot Coverage	Building - 30% Impervious - 80%	Existing Building Coverage - 20.432 Sq. Ft. (29.45%) Proposed Building Coverage - 20,432 Sq. Ft. (29.45%) Change: Increase of 7.79%
		Existing OpenSpace: 2,194 Sq. Ft. (21.96%) Proposed OpenSpace: 2,194 Sq. Ft. (21.96%) Change: Increase of 11.23%
		Existing Impervious: 57,614 Sq. Ft. (67.57%) Proposed Impervious: 57,614 Sq. Ft. (67.57%) Change: Decrease of 19.14%
		Overall Increase of +/- 11,000 Sq. Ft. OpenSpace

PARKING SUMMARY

Regulation	Required	Provided
Self Storage Facilities	4 Spaces + 1 / 200 Units	200-300 Anticipated Units = 3 Spaces Office Spaces Provided Covered Customer Loading Provided
	8 Spaces Required 16 Spaces Provided Including 1 Accessible	

SITE DATA

Regulation	Information
Parcel I.D.	027-29-0304-004
Address	8715 Telegraph Rd, Taylor, MI 48180
Zoning	Previously Zoned: B-3 Current Conditional Zoning: I-1
Lot Area	99,893.00 Sq. Ft. (2.29 Acres)

BUILDING NOTES

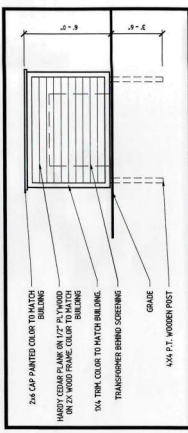
Existing Use - Vacant Request Club
 Proposed Use - Self Storage Conversion
 w/ (16) Exterior Drive-Up Units
 Building Building:
 20,432 SF
 Proposed Additional Second
 Floor Building:
 20,432 SF
 Proposed Address:
 8,288 SF / Floor
 16,666 GSF Total
 57,430 GSF Total Building Area

NO EXISTING TREES ON SITE
NO TREES ON SITE - NO REMOVAL PERMIT REQUIRED

APPROPRIATE VARIANCE SCHEDULE

1	ADDITION TO A NONCONFORMING STRUCTURE	PROPOSED SETBACK: 50'	REQUESTED RELIEF: 30'
2	NORTH SIDE SETBACK	PROPOSED SETBACK: 38'-6"	REQUESTED RELIEF: 9'-4"
3	SOUTH SIDE SETBACK	EXISTING / PROPOSED SETBACK: 20'	REQUESTED RELIEF: 30'
4	REAR BUILDING SETBACK (EXISTING BUILDING)	PROPOSED SETBACK: 31'-7"	REQUESTED RELIEF: 18'-5"
5	REAR BUILDING SETBACK (PROPOSED ADDITION)		

Existing Drive Entry to Remain
 Drive-Thru canopy proposed for covered loading
 All lighting shall be updated, shielded and directed
 away from adjacent properties of the local zoning ordinance.
 See Proboscetic Plan for additional information.
 All Landscaping will be irrigated



UTILIZATION MATRIX:

TOTAL PROPOSED STORAGE AREA	34,362.7 SF
TOTAL BUILDING AREA	36,526.9 SF
UTILIZATION RATIO	34,362.7 SF / 36,526.9 SF = 94.1%

UNIT MATRIX:

SYMBOL	A	B	C	D	E	F	G	H	I	J	K	L	M	N
EMERGENCY	305	5075	5075	5086	5090	5091	5095	5095	5095	5095	5095	5095	5095	5095
AREA	25	215	375	45	56	75	102	300	504	200	176.4	256	64.5	64.5
COUNT	39	77	1	1	1	1	1	1	1	1	1	1	1	1
TOTAL SQFT.	875	2,480.25	315	505	2,600	6,000	1,075	7,500	3,050	3,600	176.6	1,750	64.5	64.5
TOTAL	34,362.7 SF													



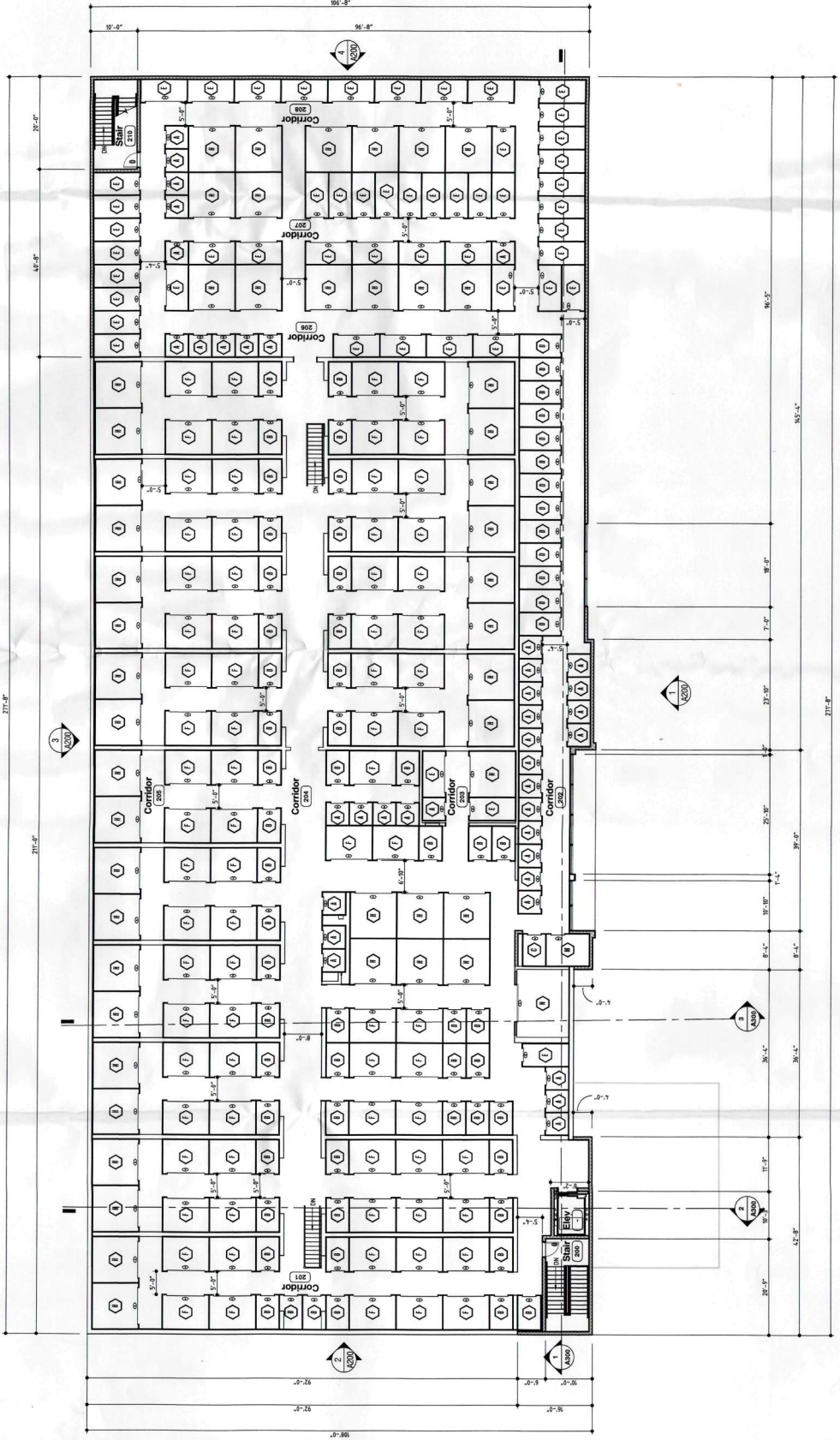
Second Floor Plan

Storage Plus
8715 Telegraph Road
Taylor, Michigan 48180



3300 Auburn Rd., Suite 300
Auburn Hills, MI 48003
T: 248.601.4422 F: 248.453.5854
WWW.DESIGNHAUS.COM
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ARCHITECTURE



UTILIZATION RATIO:

TOTAL PROPOSED STORAGE AREA	34,862.57 SF
TOTAL BUILDING AREA	34,862.57 SF
UTILIZATION RATIO	100.00%

UNIT MATRIX

SYMBOL	(A)	(B)	(C)	(D)	(E)	(F)	(G)	(H)	(I)	(J)	(K)	(L)	(M)	(N)	TOTAL
DIMENSIONS	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	5'00	34,862.57
AREA	25	31.5	31.5	45	59	75	102.5	100	165	200	176.6	270	44.75	164.32	
COUNT	39	77	1	13	53	102	54	75	21	17	1	1	1	1	
TOTAL SQFT.	975	2,817.50	73.50	585	2,655	9,100	15,735	7,500	3,400	3,400	176.6	1,750	44.75	164.32	
TOTAL															34,862.57

A102

02/03/23

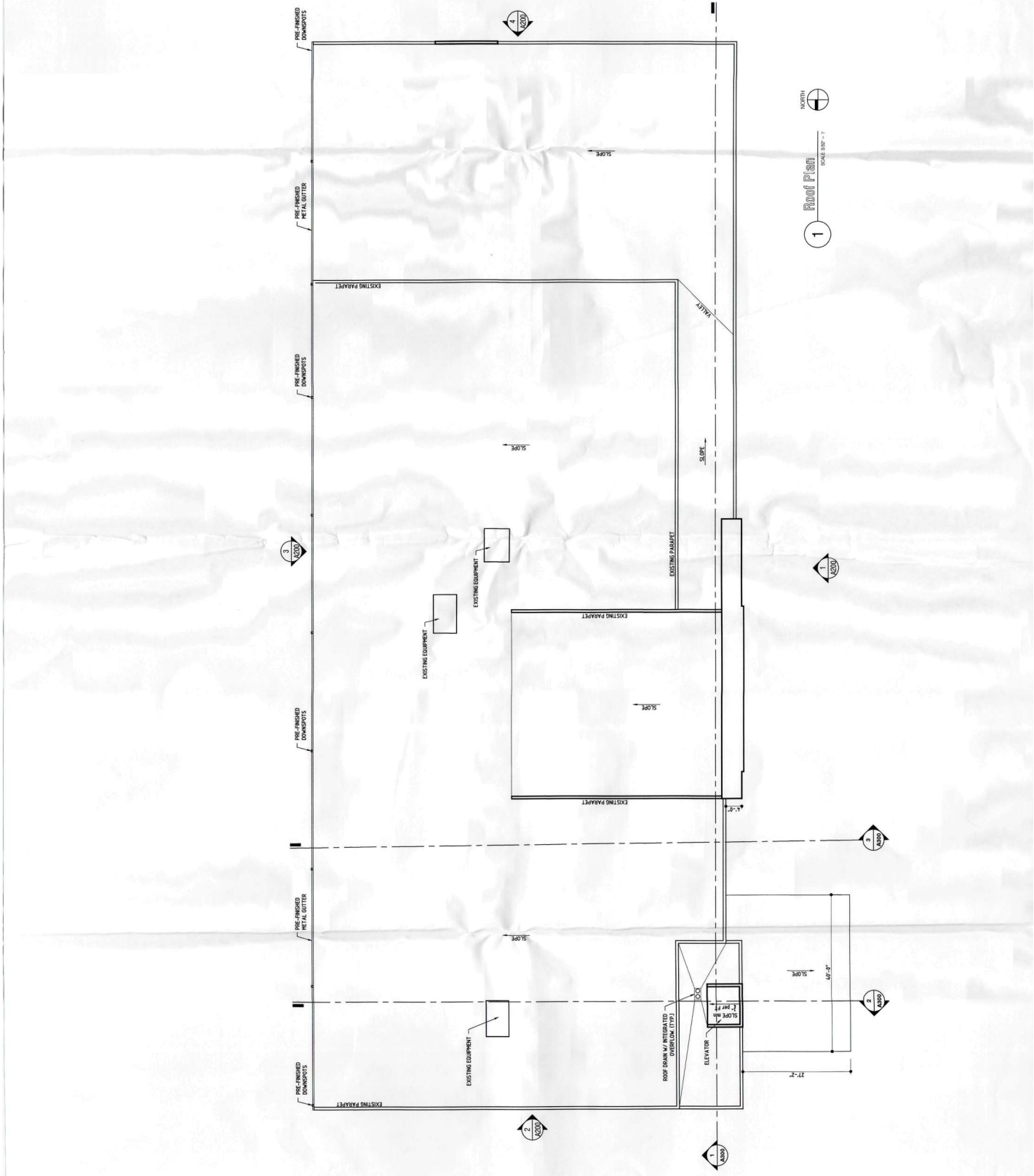
Roof Plan

Storage Plus
Taylor, Michigan 48180

3300 AUBURN RD, SUITE 300
AUBURN HILLS, MI 48326
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SPA Revision #1	05.25.22
SPA CD	04.04.22
ZBA Submittal	04.08.22
SPA Reviewer	0.0.21
Responsible/Status	Client



02/2023
A103

Enlarged Floor Plan

Storage Plus
8715 Telegraph Road
Taylor, Michigan 48180

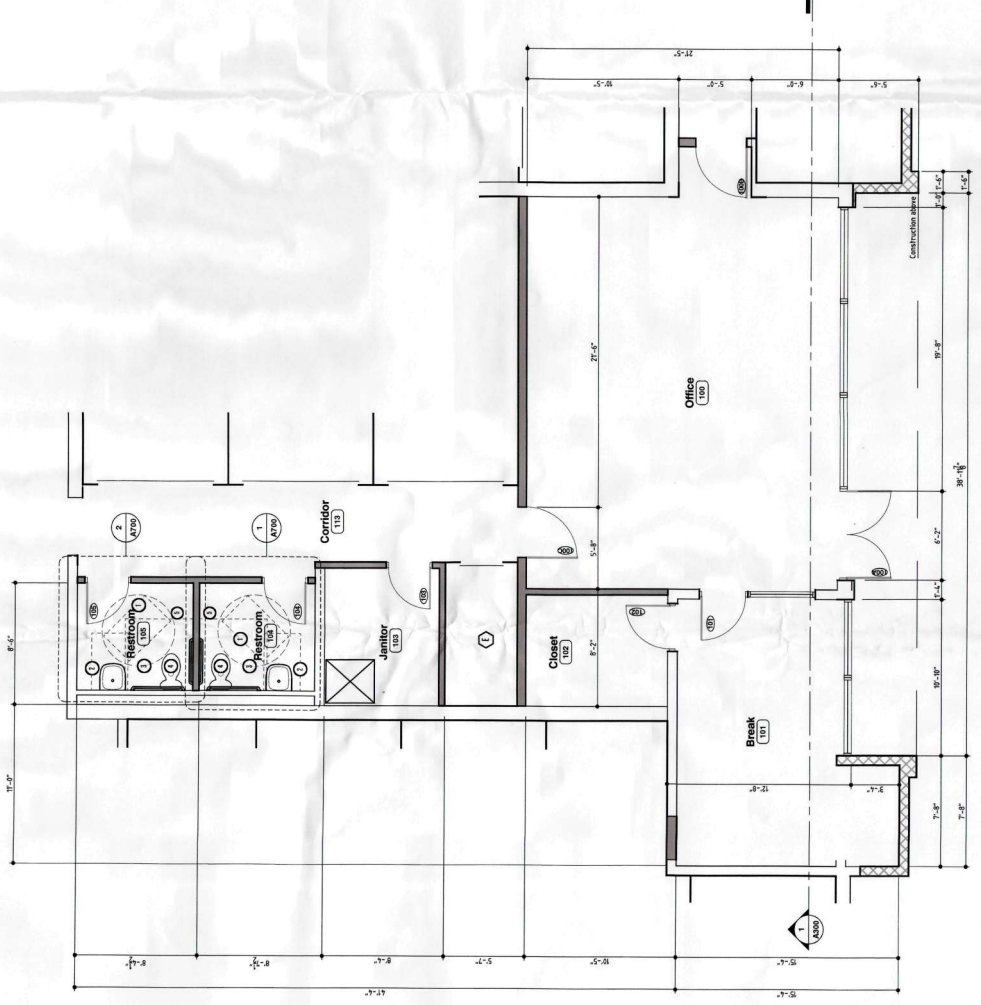


Revision/Issue	Date
SPA Review	3/2/21
SPA Submittal	3/22/22
90% CD	05/03/22
SPA Revision #1	05/23/22

3300 AUBURN RD, SUITE 300
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LEGEND	
NEW WALL	
EXISTING TO REMAIN	
RESTROOMS/ACCESSIBILITY	
♿	NEW LAVATORY CLEAR FLOOR AREA
♿	NEW LAVATORY CLEAR FLOOR AREA CENTER ON LAVATORY
♿	30X66 CLEAR FLOOR AREA
♿	5FT DIA. CLEAR FLOOR AREA (TOWEL RACK)
♿	NEW TOILET
♿	30X66 CLEAR FLOOR AREA



1 Enlarged Plans
SCALE: 1/8" = 1'-0"
NORTH

A200

000293

Elevations

Storage Plus
0715 Telegraph Road
Taylor, Michigan 48180



SPR Revision 01	05.23.22
80% CD	05.23.22
ZBA Submittal	04.29.22
SPR Review	03.21.22
Preparation Date	02.22.22

3300 AUBURN RD. SUITE 300
AUBURN HILLS, MI 48326
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W.W.DESIGNHAUS.COM
INFO@DESIGNHAUS.COM

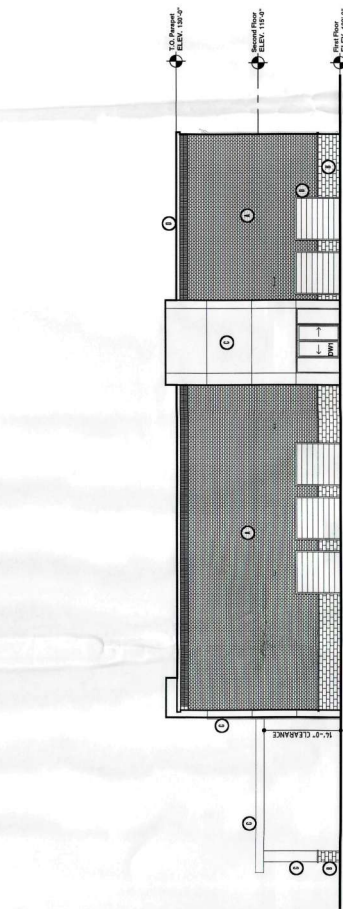
DESIGNHAUS EST 1998
ARCHITECTURE

MATERIAL LEGENDS

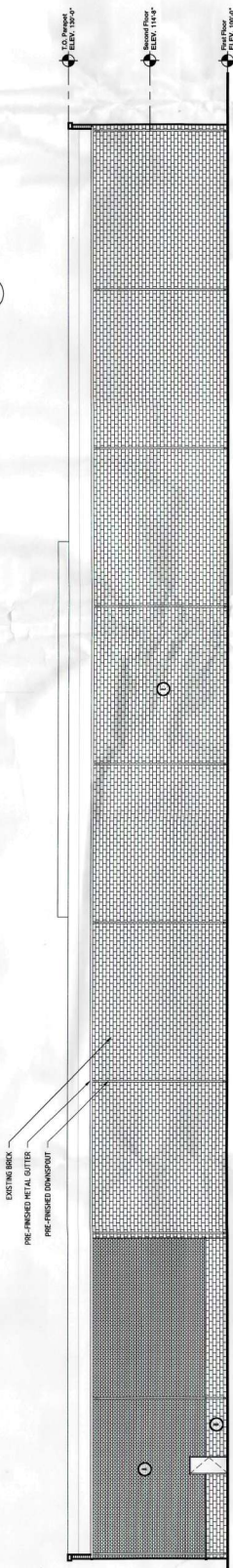
SYMBOL	DESCRIPTION
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○	BRICK
○	10% FINISHED METAL CLIFF FILL
○	EPHEMERAL PARTITION LINE COLOR
○	10% FINISHED METAL CLIFF FILL
○	10% FINISHED METAL CLIFF FILL
○	10% FINISHED METAL CLIFF FILL
○	10% FINISHED METAL CLIFF FILL
○	10% FINISHED METAL CLIFF FILL
○	10% FINISHED METAL CLIFF FILL

GLAZING LEGEND

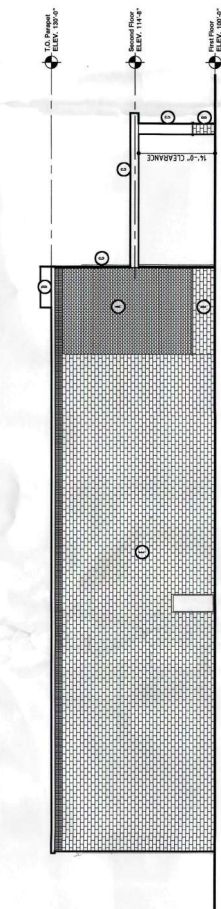
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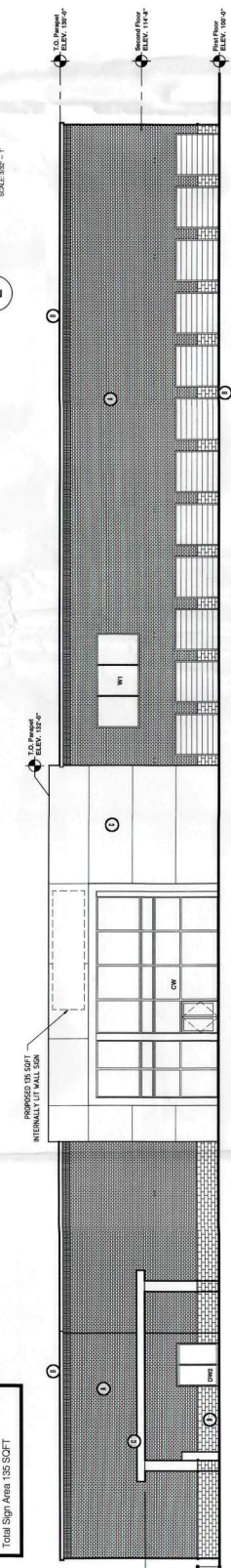
4 South Elevation



3 East Elevation



2 North Elevation



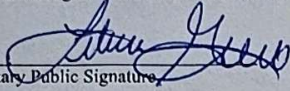
1 West Elevation

WALL SIGN AREA

Allowable Area	PER SECTION 18.06.A
10% Wall Face (270')	100 SQFT Maximum
Allowable Increase	PER SECTION 18.06.B
Distance from F.O.W. 401'-500'	Increase of 35% (35 SQFT)
Total Sign Area	135 SQFT

STATE OF MICHIGAN
COUNTY OF WAYNE

Acknowledged before me in WAYNE County, Michigan, on this 2nd day of January, 2025 by Michelle L. Rimes

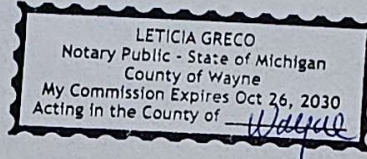

Notary Public Signature

Notary name

Notary public, State of Michigan, COUNTY OF WAYNE

My Commission Expires: _____

Drafted by:
Michelle L. Rimes
6910 Centralia St.
Dearborn Heights, MI 48127

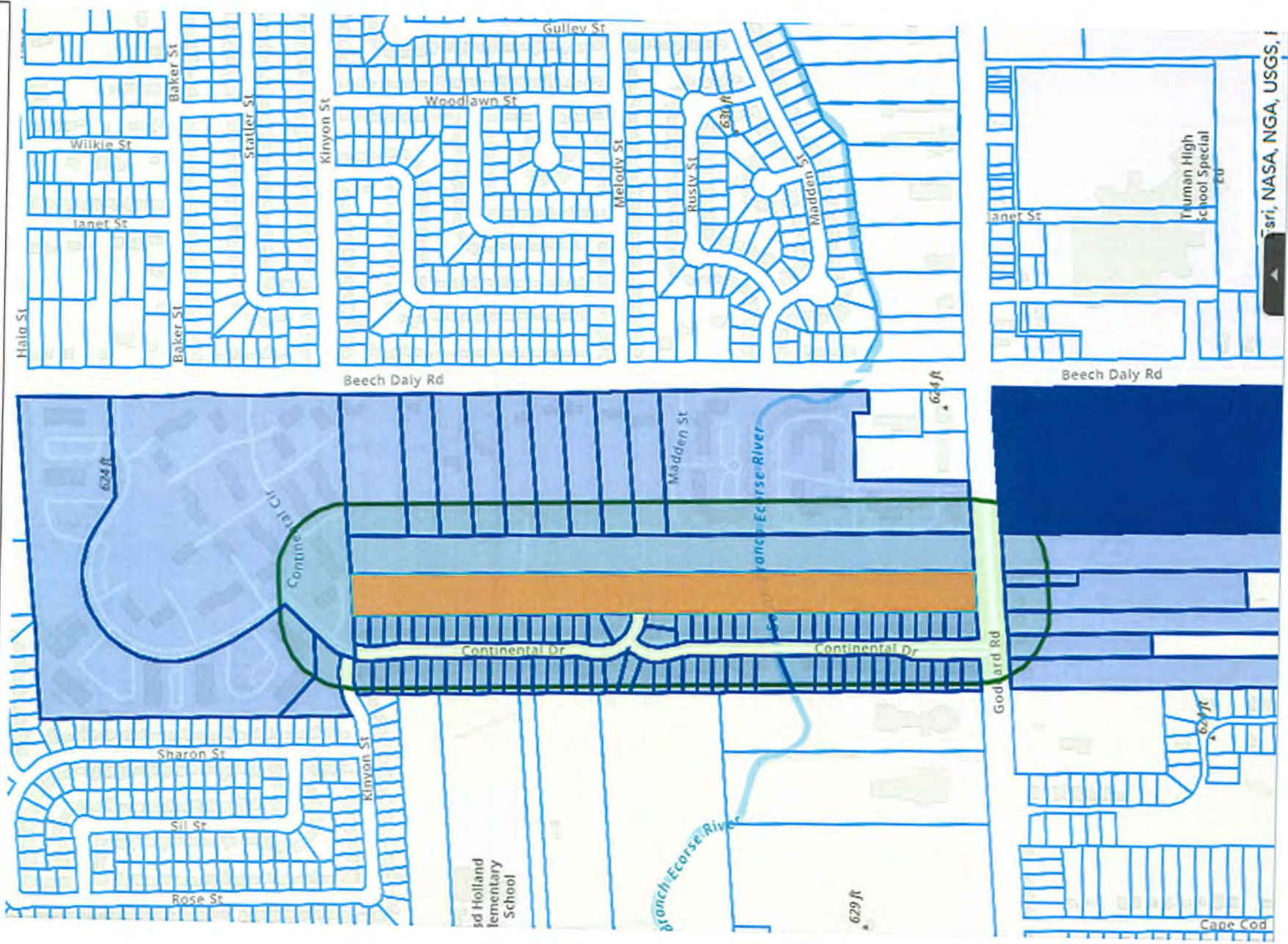


When recorded, return to:
Goddard 25940, LLC
6910 Centralia Street
Dearborn Heights, MI 48127

Revenue stamps: State: 0.00

County: 0.00

300 foot mailing radius for 26050 Goddard - Conditional Rezoning - 144 addresses.



Current occupant	10400 BEECH DALY	Taylor, MI 48180
Current occupant	10216 CONTINENTAL	Taylor, MI 48180
Current occupant	10019 CONTINENTAL	Taylor, MI 48180
Current occupant	10020 CONTINENTAL	Taylor, MI 48180
Current occupant	10043 CONTINENTAL	Taylor, MI 48180
Current occupant	10044 CONTINENTAL	Taylor, MI 48180
Current occupant	GODDARD	Taylor, MI 48180
Current occupant	26050 GODDARD	Taylor, MI 48180
Current occupant	10833 CONTINENTAL	Taylor, MI 48180
Current occupant	10834 CONTINENTAL	Taylor, MI 48180
Current occupant	10822 CONTINENTAL	Taylor, MI 48180
Current occupant	10697 CONTINENTAL	Taylor, MI 48180
Current occupant	10698 CONTINENTAL	Taylor, MI 48180
Current occupant	10068 CONTINENTAL	Taylor, MI 48180
Current occupant	10204 CONTINENTAL	Taylor, MI 48180
Current occupant	10215 CONTINENTAL	Taylor, MI 48180
Current occupant	10810 CONTINENTAL	Taylor, MI 48180
Current occupant	10821 CONTINENTAL	Taylor, MI 48180
Current occupant	25834 CONTINENTAL CIR	Taylor, MI 48180
Current occupant	10040 BEECH DALY	Taylor, MI 48180
Current occupant	10079 CONTINENTAL	Taylor, MI 48180
Current occupant	10080 CONTINENTAL	Taylor, MI 48180
Current occupant	10091 CONTINENTAL	Taylor, MI 48180
Current occupant	10200 BEECH DALY	Taylor, MI 48180
Current occupant	10092 CONTINENTAL	Taylor, MI 48180
Current occupant	10203 CONTINENTAL	Taylor, MI 48180
Current occupant	10007 CONTINENTAL	Taylor, MI 48180
Current occupant	10055 CONTINENTAL	Taylor, MI 48180
Current occupant	10056 CONTINENTAL	Taylor, MI 48180
Current occupant	10100 BEECH DALY	Taylor, MI 48180
Current occupant	10067 CONTINENTAL	Taylor, MI 48180
Current occupant	10430 BEECH DALY	Taylor, MI 48180
Current occupant	10275 CONTINENTAL	Taylor, MI 48180
Current occupant	10031 CONTINENTAL	Taylor, MI 48180
Current occupant	10032 CONTINENTAL	Taylor, MI 48180
Current occupant	10070 BEECH DALY	Taylor, MI 48180
Current occupant	10227 CONTINENTAL	Taylor, MI 48180
Current occupant	10228 CONTINENTAL	Taylor, MI 48180
Current occupant	10239 CONTINENTAL	Taylor, MI 48180
Current occupant	10240 CONTINENTAL	Taylor, MI 48180
Current occupant	10412 BEECH DALY	Taylor, MI 48180
Current occupant	10518 CONTINENTAL	Taylor, MI 48180
Current occupant	25995 MADDEN	Taylor, MI 48180
Current occupant	25983 MADDEN	Taylor, MI 48180

Current occupant	10700 MT VERNON	Taylor, MI 48180
Current occupant	10530 CONTINENTAL	Taylor, MI 48180
Current occupant	10251 CONTINENTAL	Taylor, MI 48180
Current occupant	10252 CONTINENTAL	Taylor, MI 48180
Current occupant	10263 CONTINENTAL	Taylor, MI 48180
Current occupant	10264 CONTINENTAL	Taylor, MI 48180
Current occupant	10276 CONTINENTAL	Taylor, MI 48180
Current occupant	10287 CONTINENTAL	Taylor, MI 48180
Current occupant	10288 CONTINENTAL	Taylor, MI 48180
Current occupant	10444 BEECH DALY	Taylor, MI 48180
Current occupant	10401 CONTINENTAL	Taylor, MI 48180
Current occupant	10400 CONTINENTAL	Taylor, MI 48180
Current occupant	25996 MADDEN	Taylor, MI 48180
Current occupant	10416 CONTINENTAL	Taylor, MI 48180
Current occupant	10470 BEECH DALY	Taylor, MI 48180
Current occupant	10506 CONTINENTAL	Taylor, MI 48180
Current occupant	10542 CONTINENTAL	Taylor, MI 48180
Current occupant	10549 CONTINENTAL	Taylor, MI 48180
Current occupant	10554 CONTINENTAL	Taylor, MI 48180
Current occupant	10561 CONTINENTAL	Taylor, MI 48180
Current occupant	10566 CONTINENTAL	Taylor, MI 48180
Current occupant	10573 CONTINENTAL	Taylor, MI 48180
Current occupant	10578 CONTINENTAL	Taylor, MI 48180
Current occupant	10585 CONTINENTAL	Taylor, MI 48180
Current occupant	10590 CONTINENTAL	Taylor, MI 48180
Current occupant	10597 CONTINENTAL	Taylor, MI 48180
Current occupant	10602 CONTINENTAL	Taylor, MI 48180
Current occupant	10609 CONTINENTAL	Taylor, MI 48180
Current occupant	10614 CONTINENTAL	Taylor, MI 48180
Current occupant	10637 CONTINENTAL	Taylor, MI 48180
Current occupant	10650 CONTINENTAL	Taylor, MI 48180
Current occupant	10649 CONTINENTAL	Taylor, MI 48180
Current occupant	10661 CONTINENTAL	Taylor, MI 48180
Current occupant	10662 CONTINENTAL	Taylor, MI 48180
Current occupant	10673 CONTINENTAL	Taylor, MI 48180
Current occupant	10674 CONTINENTAL	Taylor, MI 48180
Current occupant	10685 CONTINENTAL	Taylor, MI 48180
Current occupant	10686 CONTINENTAL	Taylor, MI 48180
Current occupant	10809 CONTINENTAL	Taylor, MI 48180
Current occupant	10845 CONTINENTAL	Taylor, MI 48180
Current occupant	10846 CONTINENTAL	Taylor, MI 48180
Current occupant	10857 CONTINENTAL	Taylor, MI 48180
Current occupant	10858 CONTINENTAL	Taylor, MI 48180
Current occupant	10869 CONTINENTAL	Taylor, MI 48180

Current occupant	10870 CONTINENTAL	Taylor, MI 48180
Current occupant	10883 CONTINENTAL	Taylor, MI 48180
Current occupant	10884 CONTINENTAL	Taylor, MI 48180
Current occupant	25945 GODDARD	Taylor, MI 48180
Current occupant	25965 GODDARD	Taylor, MI 48180
Current occupant	26053 GODDARD	Taylor, MI 48180
Current occupant	26103 GODDARD	Taylor, MI 48180
Current occupant	26111 GODDARD	Taylor, MI 48180
Current occupant	26131 GODDARD	Taylor, MI 48180
Current occupant	25813 COVE CREEK DR	Taylor, MI 48180
Current occupant	25823 COVE CREEK DR	Taylor, MI 48180
Current occupant	25833 COVE CREEK DR	Taylor, MI 48180
Current occupant	25843 COVE CREEK DR	Taylor, MI 48180
Current occupant	25853 COVE CREEK DR	Taylor, MI 48180
Current occupant	25863 COVE CREEK DR	Taylor, MI 48180
Current occupant	25873 COVE CREEK DR	Taylor, MI 48180
Current occupant	11440 COVE CREEK CT	Taylor, MI 48180
Current occupant	11430 COVE CREEK CT	Taylor, MI 48180
Current occupant	11420 COVE CREEK CT	Taylor, MI 48180
Current occupant	11410 COVE CREEK CT	Taylor, MI 48180
Current occupant	11400 COVE CREEK CT	Taylor, MI 48180
Current occupant	11380 COVE CREEK CT	Taylor, MI 48180
Current occupant	11370 COVE CREEK CT	Taylor, MI 48180
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Current occupant	11350 COVE CREEK CT	Taylor, MI 48180
Current occupant	11381 COVE CREEK CT	Taylor, MI 48180
Current occupant	11391 COVE CREEK CT	Taylor, MI 48180
Current occupant	11401 COVE CREEK CT	Taylor, MI 48180
Current occupant	25852 COVE CREEK DR	Taylor, MI 48180
Current occupant	25842 COVE CREEK DR	Taylor, MI 48180
Current occupant	25832 COVE CREEK DR	Taylor, MI 48180
Current occupant	25822 COVE CREEK DR	Taylor, MI 48180
Current occupant	25812 COVE CREEK DR	Taylor, MI 48180
Current occupant	11011 COVENTRY COURT	Taylor, MI 48180
Current occupant	11021 COVENTRY COURT	Taylor, MI 48180
Current occupant	11041 COVENTRY COURT	Taylor, MI 48180
Current occupant	11051 COVENTRY COURT	Taylor, MI 48180
Current occupant	11061 COVENTRY COURT	Taylor, MI 48180
Current occupant	11071 COVENTRY COURT	Taylor, MI 48180
Current occupant	11081 COVENTRY COURT	Taylor, MI 48180
Current occupant	11111 COVENTRY COURT	Taylor, MI 48180
Current occupant	11161 COVENTRY COURT	Taylor, MI 48180
Current occupant	11151 COVENTRY COURT	Taylor, MI 48180
Current occupant	11141 COVENTRY COURT	Taylor, MI 48180

Current occupant	11131 COVENTRY COURT	Taylor, MI 48180
Current occupant	11110 COVENTRY COURT	Taylor, MI 48180
Current occupant	11100 COVENTRY COURT	Taylor, MI 48180
Current occupant	11080 COVENTRY COURT	Taylor, MI 48180
Current occupant	11070 COVENTRY COURT	Taylor, MI 48180
Current occupant	11050 COVENTRY COURT	Taylor, MI 48180
Current occupant	11040 COVENTRY COURT	Taylor, MI 48180
Current occupant	11020 COVENTRY COURT	Taylor, MI 48180
Current occupant	11010 COVENTRY COURT	Taylor, MI 48180
Current occupant	KINYON	Taylor, MI 48180
Current occupant	25900 CHELSEA PARK DR	Taylor, MI 48180
Current occupant	10008 CONTINENTAL	Taylor, MI 48180

CITY OF TAYLOR

Timothy Woolley
MAYOR

Cynthia A. Bower
CLERK

Nicone Dragone Sr.
TREASURER



23555 Goddard Road
Taylor, MI 48180
(734) 287 6550
www.cityoftaylor.com

COUNCIL

Charley Johnson
CHAIRMAN

Ron Thiede
CHAIR PRO-TEM

Christian Armstrong
Chris Clark
William Patts
Gerald P. Thomas
Dan Wallace

Dear Resident:

Notice is hereby given that there will be a public hearing for the purpose of hearing comments to a proposed rezoning on the following described property:

That the Zoning Map in connection with the Zoning Ordinance of the City shall be amended so that the zoning on the following described property; to wit:

The southern approximately 881 feet of parcel #60-048-99-0013-000 and #60-048-99-0014-000 of the SE ¼ Section 18, T.3S., R.10E., City of Taylor, Wayne County, Michigan to be conditionally rezoned from O-1/R-1C (Office/Single Family Residential) to I-1 (Light Industrial). The property is located on the north side of Goddard Road between Beech Daly and Continental Drive. This is a conditional (contract) rezoning.



The public hearing will be held on **Wednesday, February 18, 2026 at 7:00 P.M.** in the Taylor Municipal Building, 23555 Goddard Road, Taylor, Michigan. You are invited to attend the public hearing or to send your written comments regarding the request for rezoning to the City of Taylor Planning Commission, 23555 Goddard Road, Taylor, Michigan, 48180.

Respectfully submitted,

Lora Fell
Director of Planning
City of Taylor lfell@ci.taylor.mi.us

TO ALL INTERESTED PERSONS:

Notice is hereby given that there will be a Public Hearing for the purpose of hearing comments to a proposed change in Appendix A of the Taylor, Michigan, Code of Ordinances, being the Zoning Ordinance of the City of Taylor.

ORDINANCE NO. _____

AN ORDINANCE TO AMEND APPENDIX A OF THE
TAYLOR, MICHIGAN, CODE OF ORDINANCES
BEING THE ZONING ORDINANCE OF THE CITY OF TAYLOR
BY AMENDING THE ZONING MAP IN CONNECTION THEREWITH

THE CITY OF TAYLOR ORDAINS:

That the Zoning Map in connection with the Zoning Ordinance of the City shall be amended so that the zoning on the following described property; to wit:

The southern approximately 881 feet of parcel #60-048-99-0013-000 and #60-048-99-0014-000 of the SE ¼ Section 18, T.3S., R.10E., City of Taylor, Wayne County, Michigan to be conditionally rezoned from O-1/R-1C (Office/Single Family Residential) to I-1 (Light Industrial). The property is located on the north side of Goddard Road between Beech Daly and Continental Drive. This is a conditional (contract) rezoning.

INSERT MAP HERE

Maps, plats and the description of the amendment are available for public inspection at the Planning Department located in the Taylor Municipal Building, 23555 Goddard Road, Taylor, Wayne County, Michigan, during regular business hours. Written comments may be sent to the same office.

Written comments concerning this proposal will be received by the Planning Commission, 23555 Goddard Road, Taylor, Michigan, until February 17, 2026.

You are further notified that the Taylor Planning Commission will hear any comments to the proposed changes at the Taylor Municipal Building located at 23555 Goddard Road, Taylor, Michigan, on the 18th day of February, 2026, at 7:00 P.M.

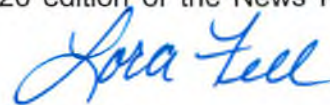
CYTHIA A. BOWER, CITY CLERK
CITY OF TAYLOR

TPH #26-02 (Zc)
END OF NOTICE

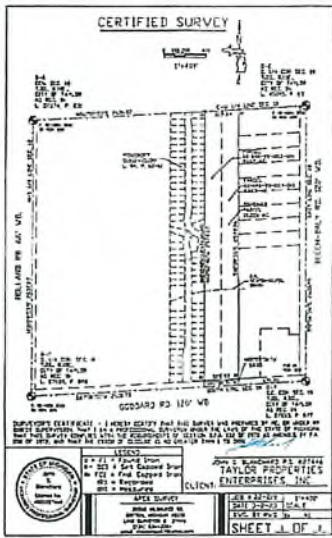
TO: NEWS HERALD NEWSPAPER

January 15, 2026

The above notice is approved for publication in the February 1st, 2026 edition of the News Herald Newspaper.



Lora Fell
Planning Director
City of Taylor



25940 Goddard Rd
Taylor, MI 48180

Zoning:
Current Zoning: R-1C / O-1

Conditional Zoning Request:
I-1 Light Industrial

Parcel Area:
+/- 19.325 Acres
(841,797 SF)

Maximum Height:
40'

Principal Building Setback
Front: 50' (10' Parking)
Sides: 50' (5' Parking)
Rear: 50' (5' Parking)

Maximum Coverage:
All Buildings - 35%
All Impermeable Surfaces - 80%
Minimum Open Space - 20%

Minimum Landscape Buffer:
50' All Sides

LEGAL DESCRIPTION

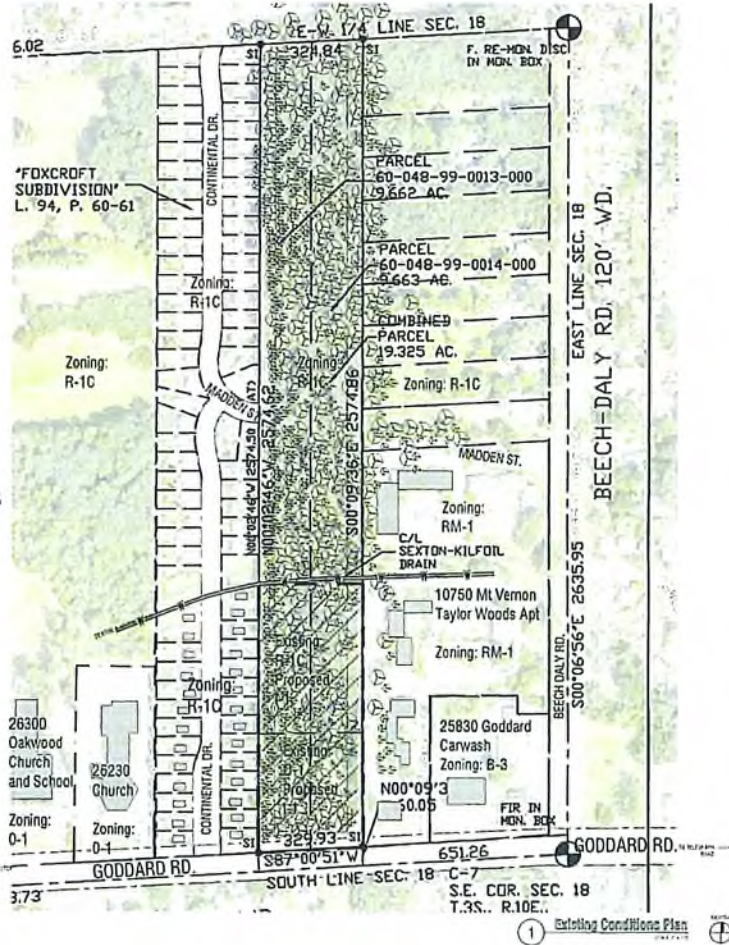
PARCEL 60-048-99-1013-003 WAS PROVIDED
A PARCEL OF LAND IN THE SE 1/4 OF SECTION 18, T.25, R.10E., CITY OF TAYLOR, WAYNE COUNTY, MICHIGAN, DESCRIBED AS:
THE WEST 1/2 OF THE EAST 1/2 OF THE WEST 1/2 OF THE EAST 1/2 OF THE SOUTHWEST 1/4 OF SECTION 18, EXCEPT THE SOUTH 400 FEET THEREOF,
CONTAINING 958 ACRES MORE OR LESS.

PARCEL 60-048-99-1014-000 WAS PROVIDED
A PARCEL OF LAND IN THE SE 1/4 OF SECTION 18, T.25, R.10E., CITY OF TAYLOR, WAYNE COUNTY, MICHIGAN, DESCRIBED AS:
THE EAST 1/2 OF THE EAST 1/2 OF THE WEST 1/2 OF THE EAST 1/2 OF THE SOUTHWEST 1/4 OF SECTION 18, EXCEPT THE SOUTH 400 FEET THEREOF,
CONTAINING 958 ACRES MORE OR LESS.

COMBINED PARCEL
A PARCEL OF LAND IN THE SE 1/4 OF SECTION 18, T.25, R.10E., CITY OF TAYLOR, WAYNE COUNTY, MICHIGAN, DESCRIBED AS:
COMMENCING AT THE SE CORNER OF SAID SECTION 18, THENCE S89°51'51"W, ALONG THE SOUTH LINE OF SAID SECTION 18 AND THE CENTERLINE OF GODDARD ROAD, 103 FEET WEST, 4318 FEET, THENCE N09°35'41"W, 620 FEET TO THE POINT OF BEGINNING; THENCE S89°51'51"W, 23390 FEET TO THE SOUTHWEST CORNER OF FOXCROFT SUBDIVISION AS DESCRIBED IN LESSEES 84 OF PLATS, PAGES 60 AND 61, WAYNE COUNTY RECORDS; THENCE N09°35'41"W, ALONG THE EASTERN LINE OF SAID FOXCROFT SUBDIVISION, 18748 FEET TO THE NORTHEAST CORNER OF SAID FOXCROFT SUBDIVISION AND THE EAST-WEST 1/4 LINE OF SAID SECTION 18; THENCE N84°25'22"E, ALONG SAID EAST-WEST 1/4 LINE, 32484 FEET, THENCE S89°51'51"W, 23748 FEET TO THE POINT OF BEGINNING,
CONTAINING 19.325 ACRES MORE OR LESS.
SUBJECT TO ANY EASEMENTS OR RESTRICTIONS OF RECORD.



Site



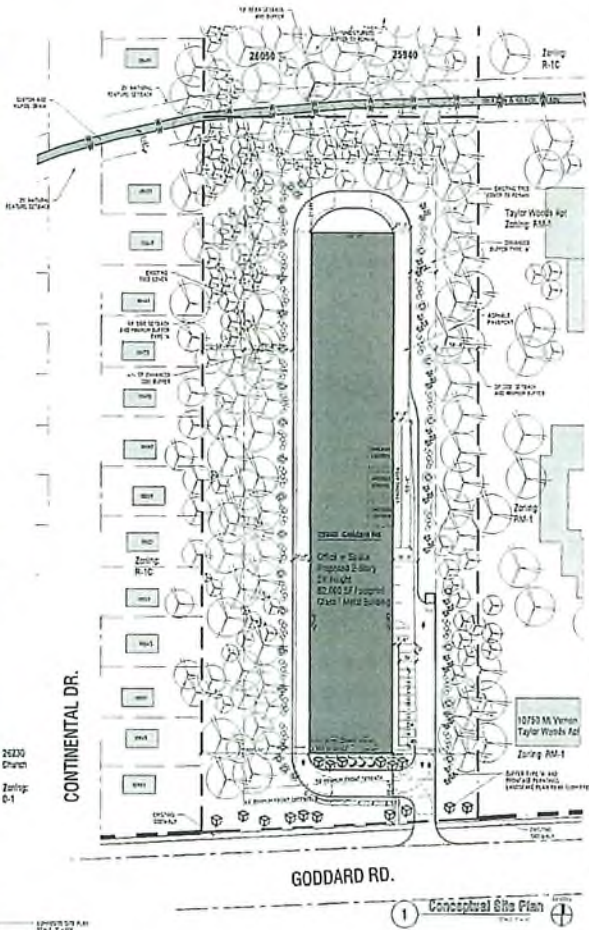
3208 AUBURN RD., SUITE 208
40000 HILLS, MI 48226
TEL: 313.487.4444
WWW.DESIGNHAUSARCHITECTURE.COM

DESIGNHAUS
ARCHITECTURE

25940 Goddard Rd.
Conditional Zoning Request
City of Taylor, Wayne County, Michigan 48180

02025

AS.1.1



25940 Goddard Rd
 Taylor, MI 48180

Zoning:
 Current Zoning: R-1C / O-1
 Conditional Zoning Request:
 I-1 Light Industrial

Parcel Area:
 19.325 Gross Acres
 (841,797 SF)
 As Reasoned:
 6.32 Net Acres
 (275,243 SF)

Maximum Height:
 40' (O-1)
 Proposed:
 24' Tall / 2 Stories

Principal Building Setback (Minimum)
 Front: 50' (10' Parking)
 Sides: 50' (5' Parking)
 Rear: 50' (5' Parking)

Parking:
 Office:
 1 Space / 250 SF
 2,000 / 250 = 8 Spaces
 Storage:
 4 Spaces Office +
 1 Space / 200 Units
 1200 Units / 200 = 6 Spaces
 14 Spaces Required
 17 Spaces Provided

Maximum Coverage:
 All Buildings - 35%
 All Impervious Surfaces - 60%

Proposed Building 62,000 SF (22.53%)
 Proposed Pavement: 48,838 SF

Total Impervious: 110,838 SF (60%)

Minimum Open Space: 20%
 Proposed Open Space: 40%

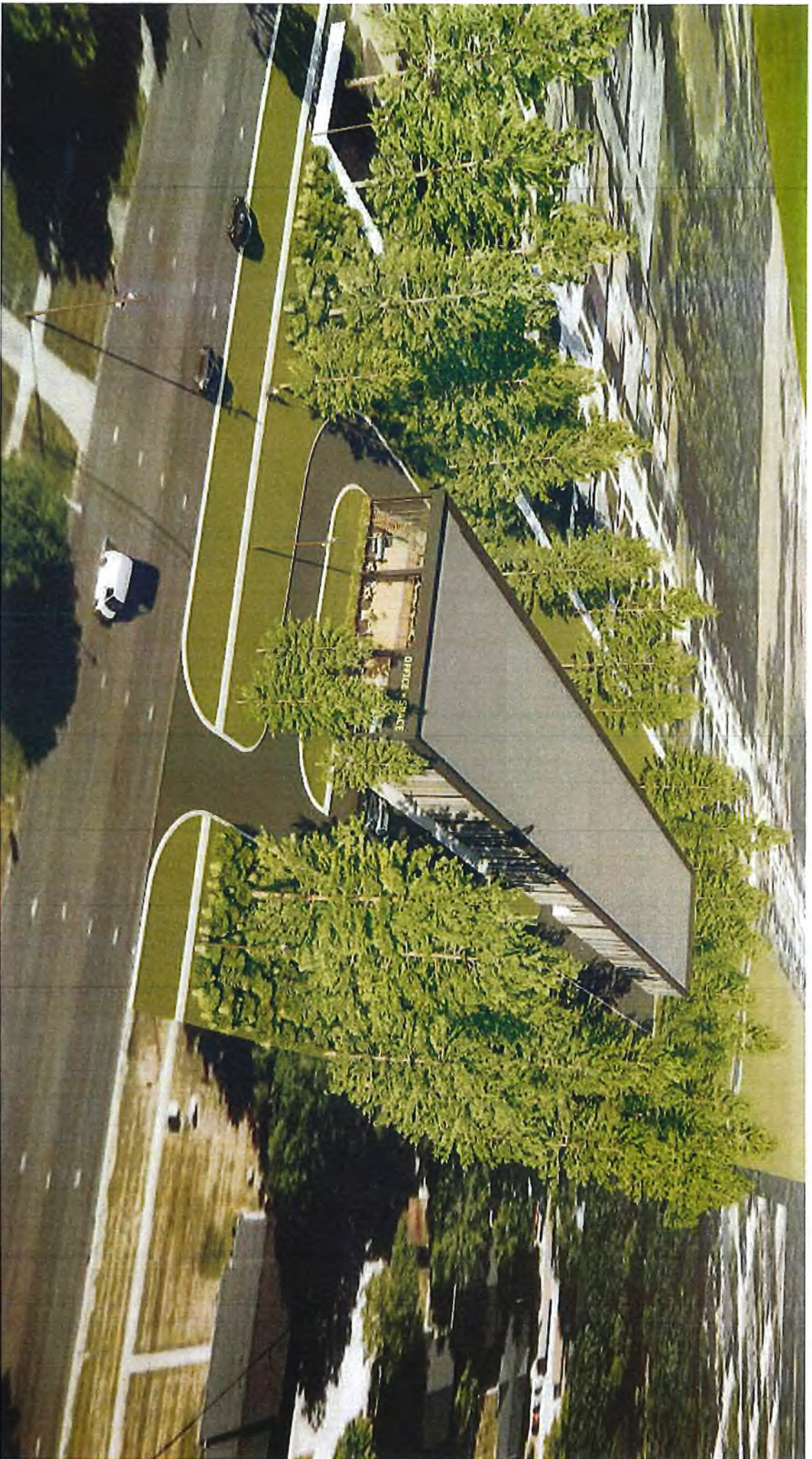
Storm Water Detention:
 Impervious Surface to be Retained:
 110,838 SF (Roof + Pavement)
 Volume: 9,200 CF
 Basin Size Required:
 Top Area: 4,365 SF
 Bottom Area: 1,750 SF

DESIGNHAUS
 ARCHITECTURE

25940 Goddard Rd.
 Conditional Zoning Request
 City of Taylor, Wayne County, Michigan 48180
 Conceptual Site Plan

AS1.2







MCKENNA

January 30, 2026

Planning Commission
City of Taylor
23555 Goddard Road
Taylor, Michigan 48180

Attention: **Ms. Lora Fell, Planning Director**

Subject: **Request for Amendment to the Official Zoning Map: Rezoning from O-1, Office and R-1C, Single Family Residential to I-1, Light Industrial - Conditional.** (Site is approximately 19.33 acres in area.)

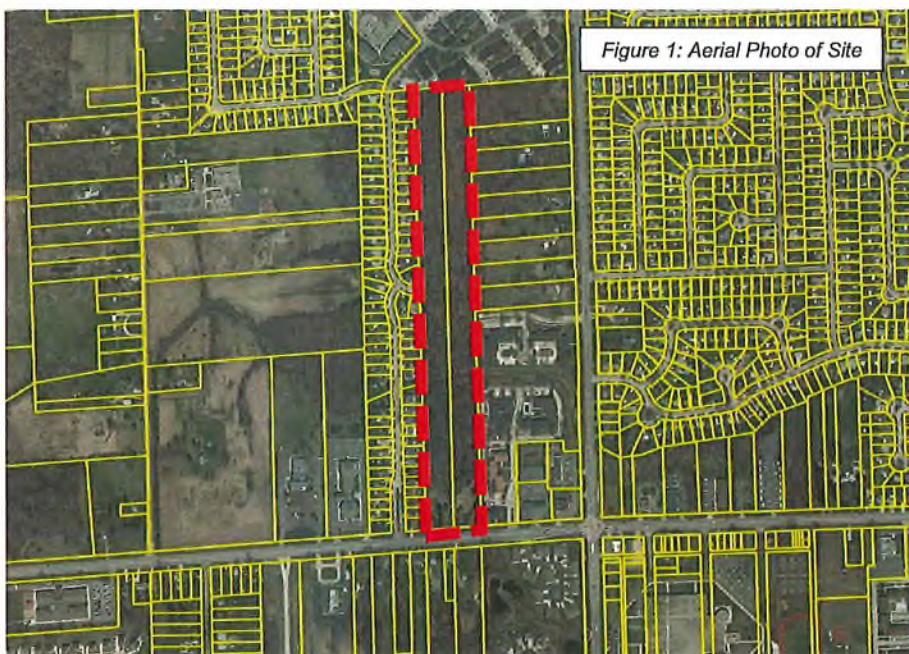
Location: **26050 Goddard Road - Tax ID #60-048-99-0013-000 and 60-048-99-0014-000.** (Located on the north side of Goddard Road, west of Beech Daly Road.)

Dear Commissioners,

At the City's request, we have conducted a review of the proposed rezoning for a site located at 26050 Goddard Road from O-1, Office and R-1C, Single Family Residential to Conditional I-1, Light Industrial. The applicant is proposing the construction of a two-story climate-controlled self-storage facility and office space on the southern 880 foot portion of the site. This would be the only use that would be permitted on the site. We have reviewed the proposed rezoning for consistency with the 2014 City of Taylor Master Plan and sound planning and zoning principals. We offer the following comments for your consideration.

DESCRIPTION OF SITE AND SURROUNDING AREA

Figure 1 depicts the boundary of the area requesting to be rezoned. The site is comprised of two tax parcels totaling approximately 19.33 acres in area. The entire site is currently vacant with the Sexton & Kilfoil drain bisecting the site approximately 900 feet north of Goddard Road. The majority of the site is wooded. Based on information available, it is not known how much of the site is upland and how much of the site may be wetland, due to proximity of the drain. The existing land use, zoning and future land use designations for the site and surrounding parcels are summarized in the following table and map excerpts.



HEADQUARTERS
235 East Main Street
Suite 105
Northville, Michigan 48167

☎ 248.596.0920
☎ 248.596.0930
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Communities for real life.



Table 1: Zoning, Existing and Future Land Use of Site and Adjacent Property

	Existing Land Use	Current Zoning	Future Land Use Designation
Site	Vacant	O-1 and R-1C	Low-Intensity Residential
North	Multi-Family Townhouses	RM-1	Medium Intensity Residential
South	Single Family Residences	R-1C	Low-Intensity Residential
East	Single Family Residences/ Apartments	R-1C and RM-1	Low-Intensity Residential – Medium Intensity Residential
West	Single Family Residences	R-1C	Low-Intensity Residential

Figure 2: Zoning Map of Area

Zoning Districts

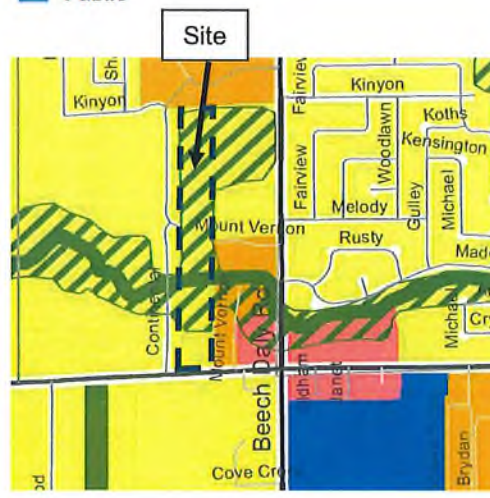
- R-1C Single-Family Residential District
- RM-1 Multiple-Family Residential District
- B-2 Regional Business District
- B-3 General Business District
- P-1 Vehicular Parking
- AE
- 0.2 PCT ANNUAL CHANCE FLOOD HAZARD



Figure 3: Future Land Use Map of Area

Future Land Use Categories

- Greenway/Conservation
- Low-Intensity Residential
- Medium-Intensity Residential
- Neighborhood Commercial
- Community Commercial
- Public



REVIEW CRITERIA FOR REZONING

Per Section 27.04 of the City Zoning Ordinance, there are seven criteria that the City must consider when making its findings regarding a proposed amendment to the zoning map. We offer the following comments regarding those criteria below.



1) Consistency with City Master Plan and Subarea Plans.

An 880 foot deep portion of site from Goddard Road extending to the north and ending on the south of the Sexton-Kilfoil drain is proposed to be rezoned from O-1, Office and R-1C, Single Family Residential to Conditional I-1, Light Industrial. The proposed use of the site is a 123,000 square foot (SF) self-storage facility with a 3,000 SF office space. The applicant is proposing to maintain the existing R-1C zoning and place a deed restriction on the remaining northern portion of the site that would prohibit development and maintain the existing woodlands.

Figure 3 shows an excerpt of the City Future Land Use map for the area under consideration. The subject parcels are not located in a subarea designated on the Master Plan Subareas Map. The Master Plan designates a portion of the site on the south as Low-Intensity Residential. The majority of the site from the south side of the drain all the way to the northern lot line is designated Greenway/Conservation. The effect of the Greenway designation would be to preserve the existing natural features and create a buffer between the uses fronting on Beech Daly Road and the residences on Continental Drive to the west.

A standard I-1 Light Industrial designation would not be consistent with the Master Plan's Future Land Use designation. A standard I-1 district would result in the removal of all existing natural features and allow uses that are not compatible with being adjacent to single family homes.

However, the proposed conditional I-1 rezoning and associated deed restriction would result in the vast majority of the site's existing natural features being protected. The buffer that the Greenway/Conservation intends to create north of the drain would be established by the conditional rezoning. The proposed self-storage development on the southern end of the site would not permit manufacturing, assembly or other intensive production uses. The proposed facility would conduct all activities within the building with no outdoor storage yard or outdoor production activity. Building access would be from the east side of the building, away from the existing single family residences. The building would act as a buffer between the uses along Beech Daly Road and the residences on Continental Dr.

Given that the proposed conditional I-1 rezoning would achieve the buffering goals of the Greenway/Conservation designation and the proposed self-storage use will conduct all activities within the building, also in effect creating a buffer for the existing residences, the requested rezoning would be consistent with the intent of the City's Master Plan.

2) Compatibility of the site's physical characteristics with the uses of the proposed district.

The southern portion of the site along Goddard Road was previously occupied by two single-family homes but is now currently vacant. The adjacent properties have been developed including single family residences to the west, multiple-family townhomes to the north and single-family homes along with apartments to the east.

A Wayne County drain bisects the site and the vast majority of the property is occupied by woodlands. The extent to which these woodlands may also be occupied by wetlands and/or hydric soils is not known. Given the proximity of the drain, the suitability of the soils for building foundations could become problematic. The proposed conditional I-1 rezoning would preserve the vast majority of the existing natural features. The applicant is not proposing to place a road over the existing drain. Given that the adjacent properties adjunct to Goddard Rd have been developed, the portion of the site proposed for improvements should be capable of development. The proposed conditional rezoning would be compatible with the site's physical features because it would result in protecting and preserving the vast majority of the existing natural features.



3) Development trends relating to requested zoning district or current district.

There are currently four self-storage facilities within the City boundaries, though there are more in proximity to the City. These sites are located on busy, main roads, including Telegraph Rd and Allen Rd. Development of self-storage facilities have seen a sharp increase in recent years throughout the Metro-Detroit area, though the demand for them in the City of Taylor is unknown.

4) Compatibility of proposed zoning district uses with surrounding uses.

As noted above, the proposed conditional I-1 zoning would only permit an indoor self-storage facility and prevent the establishment of any manufacturing use or outdoor storage yard. Typical industrial uses are a higher intensity of use than single family residential developments. To the north and east of the site are two existing multiple-family developments. The conditional I-1 rezoning of only the southern portion of the site would limit the extent of development. The vast majority of the site would remain woodlands and act as a buffer for the surrounding uses.

5) Capacity of City infrastructure to accommodate proposed zoning district uses.

The property adjacent to the site has been developed for residential uses. The capacity of the electric, natural gas and communication lines should be able to handle the proposed office and self-storage facility, as these uses typically do not require as much utility usage as the adjacent residential uses. Water and sewer mains exist in the adjacent major road rights-of-way. In terms of whether the size of these mains has the capacity to accommodate the site, we defer to the City engineer as to the capacity of the water and sewer mains to handle increased development.

6) Demand for uses in the proposed zoning district.

As mentioned above, the Master Plan has dedicated subareas for industrial zones. The City currently has many parcels with industrial uses. The demand for self-storage facilities by Taylor residents is unknown. While there is land currently planned and/or zoned in the City that could be developed to meet this land use, the applicant has proposed a configuration that would provide residents with a fully-enclosed self-storage option. Three of the four existing self-storage yards include an outdoor storage yard or have direct access to units from the outdoors.

7) Whether this application meets the submittal requirement of at least one (1) year between requests for rezoning.

City staff have indicated that there has not been a request to rezone this site within the past year, with the last being in 2024. The application for rezoning meets this zoning ordinance standard.

8) In addition to the seven criteria above, a determination shall be made that the requested zoning district is more appropriate than another district or amending the list of permitted or special land uses within a district.

Amending the list of permitted uses in the R-1 to permit self-storage would be inconsistent with the general separation of uses and maintaining compatible uses within a single zoning district.

CONCLUSION AND RECOMMENDATION

The proposed conditional rezoning would result in only a portion of the site being developed and preserve nearly the entirety of the site's existing natural features. An effect buffer would remain for the residences to the west. We advise the Planning Commission to recommend approval of the rezoning to the City Council based on the following findings of fact:



- 1) Requested rezoning is consistent with the intent of the goals of the 2014 City of Taylor Master Plan and the effect of the future land use designation.
- 2) Requested conditional rezoning would result in the protection of existing natural features and be compatible with the site's physical features.
- 3) Capacity of the city infrastructure to accommodate the proposed use and limit the need for any utilities in the future.

We look forward to reviewing these findings with you. Please feel free to contact us with any questions.

Respectfully submitted,

McKENNA

Mario A. Ortega, AICP
Senior Principal Planner

Sommer Nafal, NCI
Assistant Planner

CITY OF TAYLOR

Timothy Woolley
MAYOR

Cynthia A. Bower
CLERK

Nicone Dragone Sr.
TREASURER



23555 Goddard Road
Taylor, MI 48180
(734) 287 6550
www.cityoftaylor.com

COUNCIL

Charley Johnson
CHAIRMAN

Ron Thiede
CHAIR PRO-TEM

Christian Armstrong
Chris Clark
William Patts
Gerald P. Thomas
Dan Wallace

Dear Resident:

Notice is hereby given that there will be a public hearing for the purpose of hearing comments to a proposed rezoning on the following described property:

That the Zoning Map in connection with the Zoning Ordinance of the City shall be amended so that the zoning on the following described property; to wit:

The southern approximately 881 feet of parcel #60-048-99-0013-000 and #60-048-99-0014-000 of the SE ¼ Section 18, T.3S., R.10E., City of Taylor, Wayne County, Michigan to be conditionally rezoned from O-1/R-1C (Office/Single Family Residential) to I-1 (Light Industrial). The property is located on the north side of Goddard Road between Beech Daly and Continental Drive. This is a conditional (contract) rezoning.



The public hearing will be held on **Wednesday, February 18, 2026 at 7:00 P.M.** in the Taylor Municipal Building, 23555 Goddard Road, Taylor, Michigan. You are invited to attend the public hearing or to send your written comments regarding the request for rezoning to the City of Taylor Planning Commission, 23555 Goddard Road, Taylor, Michigan, 48180.

Respectfully submitted,

Lora Fell
Director of Planning
City of Taylor lfell@ci.taylor.mi.us

TO ALL INTERESTED PERSONS:

Notice is hereby given that there will be a Public Hearing for the purpose of hearing comments to a proposed change in Appendix A of the Taylor, Michigan, Code of Ordinances, being the Zoning Ordinance of the City of Taylor.

ORDINANCE NO. _____

AN ORDINANCE TO AMEND APPENDIX A OF THE
TAYLOR, MICHIGAN, CODE OF ORDINANCES
BEING THE ZONING ORDINANCE OF THE CITY OF TAYLOR
BY AMENDING THE ZONING MAP IN CONNECTION THEREWITH

THE CITY OF TAYLOR ORDAINS:

That the Zoning Map in connection with the Zoning Ordinance of the City shall be amended so that the zoning on the following described property; to wit:

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INSERT MAP HERE

Maps, plats and the description of the amendment are available for public inspection at the Planning Department located in the Taylor Municipal Building, 23555 Goddard Road, Taylor, Wayne County, Michigan, during regular business hours. Written comments may be sent to the same office.

Written comments concerning this proposal will be received by the Planning Commission, 23555 Goddard Road, Taylor, Michigan, until February 17, 2026.

You are further notified that the Taylor Planning Commission will hear any comments to the proposed changes at the Taylor Municipal Building located at 23555 Goddard Road, Taylor, Michigan, on the 18th day of February, 2026, at 7:00 P.M.

CYTHIA A. BOWER, CITY CLERK
CITY OF TAYLOR

TPH #26-02 (Zc)
END OF NOTICE

TO: NEWS HERALD NEWSPAPER

January 15, 2026

The above notice is approved for publication in the February 1st, 2026 edition of the News Herald Newspaper.

Lora Fell
Planning Director
City of Taylor



313-377-9449
Gkcl@outlook.com

5644 Middlebelt Road
Garden City, MI 48135

MEMO

To: Lora Fell, Planning Director / City of Taylor
From: Ghassan Khalaf, P.E., GK Consulting Inc.
CC: Ali Hamouch, Goddard 25940, LLC
Date: January 12, 2026

Reference: Proposed Self-Storage Building located at 26050 / 25940 Goddard Road, Taylor, Wayne County, Michigan

1.0 Introduction

This memorandum represents the results of Trip Generation Analyses for the proposed self-storage facility located in Taylor, Michigan at 26050/ 25940 Goddard Road. The property is located on the north side of Goddard Road, 800 feet west of Beech Daly and 4400 feet east of Inkster Road. The proposed project is to construct 123,000 s.f. buildings for the purpose of self-storage business. The building includes 120,000 s.f. storage with 3,000 s.f. of accessory office space. The site is currently vacant with traffic access to Goddard Road.



LOCATION MAP



Ms. Lora Fell
January 12, 2026
Page 2

The intent of this memorandum is to compare expected trips generated (Weekday daily, weekday AM Peak hour, and Weekday PM Peak Hour) from the proposed use (Self-Storage) under proposed zoning of I-1 Light Industrial zoning vs permitted use (assumed office) under current zoning O-1 Office District.

The south part of the property is almost 4.8 acres, which is proposed to be developed as self-storage facility. Based on Taylor Zoning Ordinance, the maximum building area percentage is 35% of 4.8 acres (73,000 sq.ft.). As a conservative approach in this comparison process, we assume an area of 10,000 square feet of permitted office use on existing zoning district O-1.

2.0 Trip Generation

The Institute of Transportation Engineers (ITE) Trip Generation Manual procedures were followed in the calculation of trips generated for the development proposal. To estimate the generated trips using Trip Generation Manual, land use and square footage were used.

- ITE code 710: Office use under current zoning O-1 (total area: 10,000 sq.ft.)
- ITE code 151: Mini self-storage under proposed zoning I-1 (total area: 123,000 sq.ft.)

3.0 Results of Analyses:

1- Proposed use (Self-Storage) vs permitted use (office):

- Weekday Daily: increase of 89 trips (proposed 186 trips – existing 97 trips)
- Weekday AM Peak Hour: increase of 10 trips (proposed 25 trips - existing 15 trips), and
- Weekday PM Peak Hour: increase of 11 trips (proposed 25 trips – existing 14 trips)

2- Proposed use (Self-Storage) vs currently vacant land:

- Weekday Daily: increase of 186 trips (proposed 186 trips – existing 00 trips)
- Weekday AM Peak Hour: increase of 25 trips (proposed 25 trips - existing 00 trips), and
- Weekday PM Peak Hour: increase of 25 trips (proposed 25 trips – existing 00 trips)



Ms. Lora Fell
January 12, 2026
Page 3

4.0 Summary:

The comparison between proposed self-storage and current allowed office use / vacant land trip generation results are below the city of Taylor thresholds for Traffic impact Analyses (500 veh/day, 50 vph Peak hour) and Traffic Impact Study (750 veh / day, 100 vph Peak hour).

Therefore, the proposed development of 123,000 s.f. of self-storage is not expected to have significant impact on the existing traffic characteristics.

Please let us know if you have any questions about the memorandum.

Sincerely,
Ghassan Khalaf
Ghassan Khalaf, P.E.
Tel: 313-377-9449
E-mail: gkci@outlook.com
E-mail: gk@gkci.net



Ghassan Khalaf

Trip Generation Summary
CURRENT PERMITTED OFFICE USE TRIP GENERATION REPORT

Alternative: Alternative 2
 Phase:
 Project: 26050/ 25940 Goddard

Open Date: 1/11/2026
 Analysis Date: 1/11/2026

ITE	Land Use	Weekday Average Daily Trips			Weekday AM Peak Hour of Generator			Weekday PM Peak Hour of Generator					
		*	Enter	Exit	Total	*	Enter	Exit	Total	*	Enter	Exit	Total
710	OFFICEGENERAL 1 10 1000 Sq. Ft. GFA		49	48	97		13	2	15		3	11	14
Unadjusted Volume			49	48	97		13	2	15		3	11	14
Internal Capture Trips			0	0	0		0	0	0		0	0	0
Pass-By Trips			0	0	0		0	0	0		0	0	0
Volume Added to Adjacent Streets			49	48	97		13	2	15		3	11	14

Total Weekday Average Daily Trips Internal Capture = 0 Percent
 Total Weekday AM Peak Hour of Generator Internal Capture = 0 Percent
 Total Weekday PM Peak Hour of Generator Internal Capture = 0 Percent

* - Custom rate used for selected time period

Trip Generation Summary
PROPOSED SELF-STORAGE TRIP GENERATION REPORT

Alternative: Alternative 1
 Phase:
 Project: 26050/ 25940 Goddard

Open Date: 1/11/2026
 Analysis Date: 1/11/2026

ITE	Land Use	Weekday Average Daily Trips			Weekday AM Peak Hour of Generator			Weekday PM Peak Hour of Generator					
		*	Enter	Exit	Total	*	Enter	Exit	Total	*	Enter	Exit	Total
161	MWAREHOUSE 1 123 1000 Sq. FL GFA		93	93	186		13	12	25		13	12	25
Unadjusted Volume			93	93	186		13	12	25		13	12	25
Internal Capture Trips			0	0	0		0	0	0		0	0	0
Pass-By Trips			0	0	0		0	0	0		0	0	0
Volume Added to Adjacent Streets			93	93	186		13	12	25		13	12	25

Total Weekday Average Daily Trips Internal Capture = 0 Percent
 Total Weekday AM Peak Hour of Generator Internal Capture = 0 Percent
 Total Weekday PM Peak Hour of Generator Internal Capture = 0 Percent

* - Custom rate used for selected time period.



January 13, 2026

City of Taylor Planning Commission
Development Services Department
23555 Goddard Rd.
Taylor, MI 46180

RE: Project Narrative for Goddard Road Conditional Rezoning Request

Dear Ms. Fell,

Designhaus Architecture, on behalf of Goddard 25940, LLC, the legal owner of the properties located at 26050 and 25940 Goddard Road, and Ali Hamouch, the developer, respectfully submits this project narrative in accordance with the City of Taylor Zoning Ordinance.

The request has been properly initiated by the legal property owner, with Designhaus Architecture acting as the authorized agent. All required application materials have been submitted, including the zoning map, legal description, owner authorization, existing and proposed zoning, adjacent zoning classifications, AS1.1 - Zoning Site Analysis, AS1.2 - Conceptual Site Plan, traffic information, and written justification addressing the criteria for rezoning.

The applicant will comply with all site posting requirements, including staking parcel corners and posting a 4' x 8' rezoning sign with all required information at least 15 days prior to the Planning Commission hearing.

The combined 19.33-acre site, consisting of parcels 60-048-99-0013-000 and 60-048-99-0014-000, is located on the north side of Goddard Road between Mt. Vernon Street and Continental Drive. The proposed development consists of a single two-story, 24-foot-tall, 123,000 sq. ft. building constructed of metal panel and glass. It includes 3,000 sq. ft. of office space and 120,000 sq. ft. of interior, climate-controlled self-storage. The ground floor building area is 100' x 620' (62,000 SF).

The following design considerations have been made:

- The site is currently vacant and heavily wooded. All tree preservation, removal, and replacement will comply with all required landscape requirements, including woodland protection and replacement ratios.

- Buffer and natural feature setbacks will be maintained around the Sexton and Kilfoil drain as preservation efforts to be maximized in the vicinity. Land north of the drain to remain untouched.
- All loading, unloading, and tenant access activities will occur indoors, with loading access located on the east side of the building, positioned away from R-1C-zoned residential properties to minimize noise and visibility. No loading, access doors, windows, or site equipment will be located on the west side of the building. Loading will be internal via overhead doors centrally located on the east building side.
- The building will be fully fire-suppressed and monitored 24/7 by third-party security systems.
- All necessary utilities are currently available at the street, and stormwater will be detained and released at a controlled rate by implementing a rain garden and infiltration basins when possible.
- Exterior lighting will include wall mounted shielded LED energy efficient fixtures. A photometric study will be provided.

The surrounding zoning includes RM-1, R-1C, B-3, and O-1, with PUD zoning to the south. The proposed use generates minimal traffic, noise, and operational impact and is compatible with these districts. It represents a logical transition between residential, commercial, and office uses along the Goddard corridor.

The rezoning request meets and exceeds the following criteria for amendment of the zoning map:

- Consistency with the City of Taylor master plan, including any subarea or corridor studies. If conditions have changed since the master plan was adopted, the consistency with recent development trends in the area.
- Capability of the site's physical, geological, hydrological and other environmental features with the host of uses permitted in the proposed zoning district.
- Whether development under current zoning is impractical or less reasonable than the requested zoning district given factors such as development trends and other factors.
- Compatibility of all the potential uses allowed in the proposed zoning district with surrounding uses and zoning in terms of land suitability, impacts on the environment, noise, density, nature of use, traffic impacts, aesthetics, infrastructure, impact on ability to develop adjacent properties under existing zoning, and potential influence on property values.
- Capacity of city infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the health, safety and welfare of the city.
- Demand for the types of uses permitted in the requested zoning district in the city in

relation to the amount of land in the city currently zoned to accommodate the demand.

- The request has not previously been submitted within the past one year, unless conditions have changed or new information has been provided.

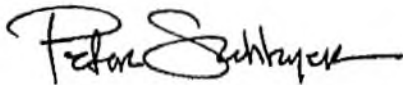
The existing R-1C and O-1 zoning is not practical due to parcel size, drain location, and market conditions. The proposed zoning is more appropriate due to the following:

- The use is low-impact and compatible with surrounding zoning and land uses.
- Infrastructure capacity is adequate, and the project will not burden municipal services.
- There is strong demand for office use with climate-controlled storage, and no similar request has been submitted within the past year.

On behalf of Goddard 25940, LLC and Ali Hamouch, we respectfully request favorable consideration of this rezoning request. The proposed development represents a logical, compatible, and beneficial use of the property that supports the city's planning goals and enhances the Goddard Road corridor.

We appreciate your consideration and welcome any questions you may have.

Regards,



Peter Stuhldreier
Chief Architect



Enclosures:
Application
Check for Application Fee
Agreement Draft
Site Plan
Rezoning Plan
Legal Description



January 14, 2026

City of Taylor Planning Commission
Development Services Department
23555 Goddard Rd.
Taylor, MI 46180

RE: Agreement Draft for Goddard Road Conditional Rezoning Request

Dear Ms. Fell,

STANDARD CONDITIONS

Please see below the standard rezoning agreement details:

1. Property Subject to Agreement

The properties subject to this Agreement are identified as:

- Parcel ID 60-048-99-0013-000 (26050 Goddard Road)
- Parcel ID 60-048-99-0014-000 (25940 Goddard Road)

A full legal description shall be attached as Exhibit A and incorporated herein.

2. Intent and Effect

- An acknowledgement that the conditional rezoning agreement was proposed voluntarily by the applicant.
- A statement that the property shall not be developed or used in any manner that is not consistent with the conditional rezoning agreement.
- A statement that the approval of the rezoning and the conditional rezoning agreement shall be binding upon and inure to the benefit of the property owner and the city, and also their respective heirs, successors, assigns, receivers or transferees. Where the applicant for rezoning is acting on behalf of the landowner through some form of purchase agreement or other mechanism, then the landowner must also consent and sign the agreement.
- A statement that, if a rezoning with a conditional rezoning agreement becomes void in accordance with this section, that no further development shall take place and no permits shall be issued.
- A statement that no part of the conditional rezoning agreement shall permit any activity, use, or condition that would otherwise violate any requirement or standard that is otherwise applicable in the new zoning district.
- A legal description of the land to which the agreement pertains.
- Any other provisions as are agreed upon by the city and applicant.

3. Permitted Use

Rezoning approval shall authorize only the specific project proposed, consisting of:

- A two-story, 24-foot-tall, 123,000 sq. ft. climate-controlled self-storage facility
- Approximately 3,000 sq. ft. of office space
- All loading, unloading, and tenant access occurring indoors
- All loading doors are located exclusively on the east side of the building
- No outdoor storage, hazardous materials, or exterior speakers
- No other uses permitted in the I-1 district shall be allowed unless this Agreement is amended by mutual consent.
- Uses permitted in O-1 office district shall be permitted.

4. Parcel Combination

Prior to issuance of building permits, the two parcels shall be combined and recorded as a single parcel.

5. Building Design and Architectural Standards

- Section 16.01 – Architectural Standards
- Full fire suppression and 24/7 monitored security
- Exterior materials per 16.01.d which require minimum 50% brick, face brick, cast stone, or C-brick on walls facing or visible from a street or adjacent residential district.

6. Site Improvements

- Crosswalks & Sidewalks - Section 16.04
- Exterior Lighting - Section 16.05 (shielded LED fixtures; photometric plan required)
- Refuse Enclosure - Section 16.06
- Mechanical Equipment Screening - Section 16.07
- Stormwater Management - Section 16.08
- No Outdoor Speakers - Section 16.09

Stormwater shall be detained and released at a controlled rate using rain gardens and infiltration basins where feasible.

7. Parking

Parking shall comply with Section 17.00, with a total of 14 spaces provided:

- 8 spaces for office use
- 6 spaces for self-storage use

8. Hours of Operation

- Standard business hours for office operations
- Limited, secured tenant access permitted during off-hours
- No outdoor activity permitted outside standard business hours

9. Construction Standards

- Michigan Building Code (2021)
- National Electrical Code (2023)
- Michigan Mechanical Code (2023)
- Michigan Plumbing Code (2021)
- NFPA Standards
- International Fire Code

10. Exhibits

- Exhibit A: Legal Description
- Exhibit B: AS1.1 Zoning Site Analysis
- Exhibit C: AS1.2 Conceptual Site Plan
- Exhibit D: Landscape and Buffering Plan
- Exhibit E: Building Elevations
- Exhibit F: Traffic Analysis

11. Effect of Agreement

- The Agreement becomes binding upon the property
- Any use inconsistent with this Agreement is prohibited
- Any violation may void the rezoning and halt development

12. Signatures

- City of Taylor – Mayor
- City of Taylor – Planning Director
- City Attorney – Approval as to Form
- Owner – Goddard 25940, LLC
- Developer – Ali Hamouch
- Authorized Agent – Designhaus Architecture (*optional*)

SPECIFIC CONDITIONS

Please see below the specific rezoning agreement details:

- It is the developer's intent to conditionally rezone the area south of the Sexton and

Page 3

Kilfoil Drain – approximately 275,243 sf (6.32 acres).

- Front (South / Goddard Rd.): Required 50' / Proposed 82'
- East Side: Required 50' / Proposed 101'-5"
- West Side: Required 50' / Proposed 127'-10"
- Rear (North): Required 50' / Proposed 137'-7"
- West buffer increased to 107' minimum
- East buffer increased to 80'
- Front buffer maintained at 40'
- Maintain and preserve 25' natural feature setback of Sexton and Kilfoil drains (south branch of Ecourse River)
- Enhanced frontage and site landscaping for naturalistic visual impact
- Rain gardens and bioswale implementation for minimal stormwater impact
- Deed restriction for the property north of the drain to remain as is.
- No loading doors, access doors, windows, or mechanical equipment on the west elevation
- No west-side openings or equipment
- Maximum height: 24', excluding mechanical screening

We appreciate your consideration and welcome any questions you may have.

Regards,



Peter Stuhlreyer
Chief Architect



TAYLOR PLANNING COMMISSION MEETING MINUTES

WEDNESDAY, FEBRUARY 18, 2026

The Taylor Planning Commission meeting was held in the Taylor Municipal Building, 23555 Goddard Road, on Wednesday, February 18, 2026 at 7:00 P.M., Acting Chairman Bill Fisher presiding.

Chairman Fisher informed the Commission that Mr. Bob Szwed, former Chairman with 38 years on the Planning Commission, has fallen seriously ill.

Commissioner Ghallozi led the Commission in prayer for Mr. Szwed and his family.

Pledge of Allegiance

Present: Messrs. Fisher, Siemion, Johnson, Ghallozi, and Welton

Excused: Mr. Stewart

Also Present: Mayor Tim Woolley, Ms. Lora Fell, Planning Director.

Motion by Johnson Supported by Stewart

Resolved: To approve the agenda as received.

Unanimously carried.

Resolution Number 2.14-26

Motion by Johnson Supported by Siemion

Resolved: To approve the minutes of the February 4, 2026, meeting and dispense with the oral reading.

Unanimously carried.

Resolution Number 2.15-26

LEGISLATIVE ACTION

Case #26-02 (ZC) – vacant Goddard (N. side of Goddard between Beech Daly & Continental Dr.)- conditional (contract) rezoning

The southern approximately 881 feet of parcels #60-048-99-0013-000 and #60-048-99-0014-000 of the SE ¼ Section 18, T.3S., R.10E., City of Taylor, Wayne County, Michigan to be conditionally rezoned from O-1/R-1C (Office/Single Family Residential) to I-1 (Light Industrial). The property is located on the north side of Goddard Road between Beech Daly and Continental Drive. This is a conditional (contract) rezoning.

Mr. Peter Sthlreyer, architect gave a brief explanation of the proposed contract rezoning and the proposed future development of the southern portion of the properties. No future development is planned for the parcels north of the drain. He has offered to put the northern portion in a conservation easement. He stated there was a market for residential goods storage, the building would be fully suppressed, no dumpsters on site and traffic would consist of approximately 5-10 trips per day.

Mayor Woolley spoke in support of this particular rezoning due to it being the least impact for the residents and the preservation of the woodlands.

Discussion included: Planning Consultant has recommended approval, 5-10 trips per day, approximately 13 acres would be preserved, operations similar to Clover, all storage inside, property values could decrease if alternative smaller homes are constructed, entire site will be kept up by applicant, all activity on east side of building only, increased building setbacks, increased landscaping, storage hours limited, security 24/7, average storage lease is 9 months, cameras, customers swipe in and out.

Resident inquired if property will be maintained by developer.

Applicant stated yes.

Resident complimented current administration's transparency, noted vacancies in the city and proposed the city do a "property swap".

Resident expressed concern regarding traffic and the proposed development. Resident opposed development.

Resident expressed concern about increased traffic and does not believe a storage facility is needed.

Resident stated he would like to see the property remain "as is".

Motion by Siemion Supported by Ghallozi
Resolved: To recommend to City Council that Case #26-02(ZC) be approved, based upon the Planning Consultant's recommendation.
Unanimously carried.
Resolution Number 2.16-26

Motion by Johnson Supported by Styles
Resolved: That the meeting be adjourned.
Unanimously carried.
Resolution Number 2.17-26

The meeting adjourned at 8:19 p.m.

Bill Fisher, Acting Chairman

Cynthia A. Bower, City Clerk



MCKENNA

February 11, 2026

Ms. Lora Fell
Planning Director
City of Taylor
23555 Goddard Road
Taylor, Michigan 48180

Subject: Fee Schedule Update

Dear Lora:

We proudly consider the City of Taylor as one of our most valued and longest-standing clients. We take pride in the strong working relationship that has developed with the City since we started our partnership in 1996.

We continue our commitment to providing the City with planning services necessary to address ongoing development and investment in the community. We are always eager to work together with the City to ensure the highest level of services are provided to the residents, property owners and businesses of Taylor. We are also proud of the relationship we have with City staff and the Planning Commission, resulting in efficient collaboration and the consistent and fair application of your rules and regulations relating to planning and zoning.


McKenna is conducting a review of our fees in our partner communities. In Taylor we have not increased our hourly rates nor our fees for review services since 2010. However, the cost of competing for Michigan's top planning personnel, health insurance, planning technology, mileage, professional insurance, and other similar expenses has increased significantly. In order to provide the City with the same high level of service, we find it necessary to revise the hourly rate and review fee schedule to reflect the costs of providing professional planning services more accurately in 2026 and beyond.

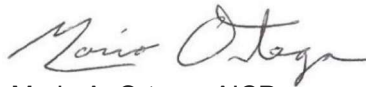
Our new fee schedules, attached below, will take effect March 1, 2026. In conjunction with this adjustment, McKenna will not seek to raise these fees prior to December 31, 2027.

Please do not hesitate to contact us with any questions.

Respectfully submitted,

MCKENNA


John R. Jackson, AICP
President


Mario A. Ortega, AICP
Senior Principal Planner

HEADQUARTERS
235 East Main Street
Suite 105
Northville, Michigan 48167

O 248.596.0920
F 248.596.0930
MCKA.COM

Communities for real life.

DEVELOPMENT REVIEW FEES LUMP SUM

REVIEW TYPE	FEE
Zoning Compliance Permits	
• Lot Split (Non-Plat)	\$200
• Non-Residential Site Improvements	\$300
• Single Family Home	\$200
• All Others (Residential Fences, Accessory Structures, Etc.)	\$150
Site Plan Reviews	\$1,000
Special Use Review	\$600 + Site Plan Review Fee
Rezoning/Amendments	
• Text Amendment (proposed by applicant)	\$750
• Rezoning	\$750
• Conditional Rezoning	\$800 + Site Plan Review Fee + Contract Review Document Fees (if applicable)
• Planned Unit Development	\$1,000 + Site Plan Review Fee + Condominium Review Fees (if applicable)
Street Vacations	\$500
Plats and Condominium	
• Preliminary Plat – Tentative Review	\$1,000
• Preliminary Plat – Final Review	\$800
• Final Plat	\$600
• Master Deed or By-Laws	\$500 per document
Zoning Board of Appeals	
• Variance	\$500
• Interpretation	\$500
• Appeal	\$500 (no report will be issued and no fee charged if the appeal is regarding a McKenna decision)

Reviews of revised submissions will be reviewed at 50% of the original fee. Revisions that are determined by the City and the Consultant to be substantial shall be reviewed at the original fee.

Attendance at meetings shall be billed hourly at the professional fee rate.



Professional Fees

PROFESSIONAL FEE SCHEDULE FOR HOURLY RATED SERVICES

Professional Classification	Rate Per Hour*	
President	\$250	<p>* Rates include the following overhead: Accounting, Advertising and Promotion, Books, Publications and Maps, Business Entertainment, Charitable Contributions, Computers, Furniture and Fixtures, Graphics Supplies and General Insurance, Interest, Legal, Licenses, Meals, Memberships and Subscriptions, Office Equipment, Office Space and Parking, Office Supplies, Postage (Except Overnight), Professional Dues, Software, Taxes and Telephone.</p> <p>These rates do not include photography, outside reproduction, document or materials purchases, which are invoiced additionally. Rates also do not include reimbursable costs for travel, courier, overnight mail, etc. Mileage will be invoiced at the Federal mileage rate.</p> <p>Hourly rates are valid through December 31, 2027. Annually thereafter, they will be adjusted by a percentage equal to any increase in the Consumer Price Index for the prior 12 months per U.S. Department of Labor, Bureau of Labor Statistics.</p>
Executive or Senior Vice President	\$225	
Vice President	\$220	
Director	\$210	
Senior Principal or Manager	\$200	
Principal	\$150	
Senior	\$135	
Associate	\$115	
Assistant	\$105	
Administrative Assistant	\$75	
Consultation, preparation for, and sitting as expert witness in legal matters.	\$220	

City of Taylor
Planning Fees

7/1/2025

Condominiums/Planned Unit Development (PUD)

Concept Review	\$3,500.00
Additional Charge Per Acre	\$100.00
Additional Charge Per Lot	\$50.00
Preliminary Review	\$3,000.00
Final Review	\$1,500.00

Legal Reviews / Document Preparation

Condominium Document Review	\$700.00
Subdivision Association Document Review	\$700.00
Development Agreement Preparation & Review	\$3,000.00
(Legal review fees above are the minimum fee plus any additional, actual legal cost incurred by the City.)	

Re-Occupancy of Existing Building

New Business Review Fee	\$75.00
Site Compliance Inspection	\$150.00
Follow-up Inspection	\$0.00
Additional Inspections	\$150.00

Single Family, Manufactured Home, Commercial or Industrial Subdivision

Initial Review	<i>(Tentative Preliminary)</i> \$2,500.00
Additional Charge Per Lot	<i>(Tentative Preliminary)</i> \$100.00
(Tentative Preliminary Plat: includes informal review prior to submission & review of one (1) plat)	
Stage II Plat (Charged for each revised plat after review of ini <i>(Final Preliminary)</i>)	\$2,000.00
Final Plat	\$1,500.00

Site Plan Review

Base Charge Under 10 Acres	\$2,500.00
Plus Additional Per Acre Charge	\$100.00
Base Charge 10 - 20 Acres	\$3,000.00
Plus Additional Per Acre Charge	\$100.00
Base Charge Over 20 Acres	\$3,500.00
Plus Additional Per Acre Charge	\$100.00
Base Charge Multi-Family	\$3,500.00
Plus Additional Per Acre Charge	\$100.00
Partial acres are rounded off(up). On plans resubmitted for review 1/2 of the current fee applies to each resubmittal. 1/2 of the current fee applies to site plan amendment/re-approval of expired plan.	
Adm. Rev. fee	\$1,000.00
Post submittal mtg. at request of applicant	\$200.00

Special Meetings

Special Meetings (Plus any actual consulting fees incurred by the City.)	\$2,000.00
Special Land Use	\$2,000.00
Regulated Land Use	\$3,000.00
Rezoning	\$3,500.00
Zoning Ordinance Text Amendment	\$3,500.00

Special Ever Review fee	\$50.00
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Zoning

Zoning Board of Appeal: Residential	\$250.00
Commercial - 1st Variance	\$1,500.00
Commercial - Each Additional Variance	\$300.00
Use Variance (Plus any additional actual legal cost incurred b	\$4,000.00

Zoning Review Letter Fees

Commercial Simple (Zoning & Use Only)	\$350.00
Commercial Full Review & Research Requests	\$1,000.00
(If site plan review is needed the fee is 50% of existing site plan review fee. As Built plans must be provided for a full review.)	

Zoning Ordinance Booklet	\$75.00
Zoning Ordinance Digital	\$50.00
Zoning Map 29" X 44"	\$30.00
Zoning Map 11" X 17"	\$15.00
Master Plan Booklet	\$75.00
Master Plan CD	\$50.00
Copy, Print, and Scan Fees	
Copy/Print 8.5 x 11 up to 11 x 17 B&W	<i>1st 2 pages</i> \$3.00
Copy/Print 8.5 x 11 up to 11 x 17 B&W	<i>each additional page</i> \$0.50
Copy/Print 8.5 x 11 up to 11 x 17 Color	<i>per page</i> \$3.00
Copy/Print Oversized B&W	<i>per square foot</i> \$15.00
Copy/Print Oversized Color	<i>per square foot</i> \$3.00
Scan 8.5 x 11 up to 11 x 17 B&W/Color	<i>per page</i> \$2.50
Scan Oversized B&W	<i>per square foot</i> \$5.00
Scan Oversized Color	<i>per square foot</i> \$5.00

City of Taylor
Planning Fees

7/1/2025

Vehicle Dealer License Review

Vehicle Dealer License fee	\$500.00
Re-occupancy inspections and applicable fees apply.	

Traffic Study Review

Traffic Study Review
* Actual hourly rate of consultants completing review.

Trees Removal/Replacement

Removal Per Review	\$400.00
Replacement Fee - \$100 per inch for each inch over 6 inches in DBH (diameter breast height) for each tree removed. Additional charges may apply for removal of "Landmark Trees"	

Production Filming Permit Application Fees

Motion Picture, Television, or Still Photography

Motion Picture, Television, or Still Photography on Public/Private Property	\$150.00
Still Photography Only on Public Property	\$75.00
Expedited Processing Fee (if less than 48 hrs processing time is required, if possible, as determined by the Dept of Dev. Services)	

Public Property Use Fee

Public Property Use Fee	<i>per day</i> 75.00/day
Includes the following from prep time to clean up time: Motion Picture, Television, Video or Still Photography On-Street Base Camp (upon approval from DDS)	
Additional fees will apply for: Use of roads requiring special barricades, noticing, and/or public safety personnel. (Hourly rates for staff time to be calculated and charged separately per day.) Parking space use/rental cost per day. (Charged at current going rates per day.)	

Extended Hours of Permitted Filming Activity

Extended Hours of Permitted Filming Activity	<i>per day</i>
Any film permitted activity beyond the hours of 7 am - 11 pm or driving scenes on major, minor or neighborhood roads requiring special barricades, noticing, and/or public safety personnel.	

(Hourly rates for staff time to be calculated and charged separately per day.)

Staff Costs

Monitoring fee for additional Police, Fire, Ordinance Enforcement, Public Works, Parks & Recreation or other staff as determined by the Mayor or his/her designee.

Fee will be determined based on estimated hours needed and most current overtime rate.

A deposit is required.

Full payment is due within 30 days of commencement of film permit activities.

Planning Marketing Rebate

A marketing rebate of \$200.00 will be issued if the "City of Taylor" is used as product placement. The rebate/refund is equal to the Permit Application Fee amount paid but not to exceed \$200.00.

The name "City of Taylor" must be referenced in the applicant's final featured product as well as in the film credits. The project must use the City's name in good taste and help to promote the community as a safe and clean place in which to live, work and play. The rebate will be issued after the Project Evaluation Survey has been turned in to the City when all film permit activities have ceased.



HEADQUARTERS
235 East Main Street
Suite 105
Northville, Michigan 48167

O 248.596.0920
F 248.596.0930
MCKA.COM

February 23, 2026

Invoice No: 93045 - 30

Ms. Lora Fell
Planning Director
City of Taylor
23555 Goddard Road
Taylor, MI 48180

Project 93045 City of Taylor CS

Professional Services from January 1, 2026 to January 31, 2026

PROFESSIONAL SERVICES

26050 Goddard - O1/R1C to Conditional I1 Rezoning Review

Flat Fee \$400

\$400.00

Invoice Total \$400.00

THANK YOU. McKenna Associates. Please remit to above address and indicate project number on voucher.



QUOTE

ProPump and Controls, Inc.
 30 Stonecrest Ct, Suite 100
 Shelbyville, KY, 40065-8128
 Phone: (502)633-0677 ext

Quote Nbr.: **Q010799**
 Order Date: 11/24/2025
 Valid Until: 12/9/2025
 Sales Person: COREY BERGIN
 Reference:
 Payment Terms: Net 30
 Financing terms available upon request

BILL TO:	SHIP TO:
CASH & CREDIT CARD CUSTOMERS Tax Exempt	CASH & CREDIT CARD CUSTOMERS Tax Exempt
2101 Cantu Ct	2101 Cantu Ct
Sarasota FL 34232-6240	Sarasota FL 34232-6240
United States of America	United States of America

SCOPE OF WORK:

Replacing PM pump 2" galvanized drop pipe.
 Remove pump and motor off of old drop pipe
 Reinstall pump and motor onto new drop pipe
 Reuse pump head & motor leads

**If any other parts are found to need to be replaced during disassembly customer will be notified
 **Pump station has to be accessible for crane

NO.	ITEM	QTY.	UOM	PRICE	AMOUNT
1	SMALL PART: 2" Drop Pipe, Bolts	1.000	EA	1,000.000	1,000.00
2	LABOR: Technician Labor	1.000	HR	2,160.000	2,160.00
3	CRANE: Crane Rental Vendor PO	1.000	EA	0.000	4,000.00

Signature: _____
 Date: _____
 Requested Delivery Date _____

Quote Total: 7,160.00
Less Discount: 0.00
Tax Total: 0.00
Total (USD): 7,160.00
Deposit Due:

Continued...



QUOTE

ProPump and Controls, Inc.
30 Stonecrest Ct, Suite 100
Shelbyville, KY, 40065-8128
Phone: (502)633-0677 ext

Quote Nbr.: Q010799
Order Date: 11/24/2025
Valid Until: 12/9/2025
Sales Person: COREY BERGIN
Reference:
Payment Terms: Net 30
Financing terms available upon request

PAYMENT TERMS : All Purchase Orders are subject to acceptance by ProPump & Controls, Inc. Receipt of production deposit, verification of acceptable credit, and confirmation of order are required before production. All orders subject to 50% production deposit. Balance due 30 days from date of invoice.

If shipment or installation of equipment is delayed by customer request at no fault of ProPump & Controls, Inc., customer agrees to amend the contract or purchase order as follows, unless superseded by other terms noted on the accepted contract or agreement.

- Delayed shipment: Customer agrees to pay any storage fees requested by equipment manufacturer, if applicable.
- Equipment Installation delayed at customer request following shipment: ProPump will furnish equipment and labor to offload equipment and store on site as directed by customer. Equipment and labor costs for the offload shall be additional to the quoted price. ProPump shall invoice customer for 90% of the equipment price, less any previous production deposits, due net 30 days following delivery of product. Balance for equipment and installation labor will be invoiced on completion of work.

Orders placed under Preferred Customer Agreements include all discounts and fee payments.

Please note that all invoices paid by Credit Card are subject to a 2.8% convenience fee without prior agreement.

How to order: Please help us expedite your order by providing the following:

Is this sale taxable? (circle one) Yes No (If the order is non-taxable, a tax exempt certificate for the "ship to" state must be submitted with this order.)

Please return one signed copy of this quotation on acceptance. Merchandise delivered or shipped is due and payable to: ProPump & Controls, Inc. 30 Stonecrest Ct. Suite 100 Shelbyville, KY 40065. Fax number: 502-633-0733 Phone: 800-844-0677.

DELIVERY: Delivery dates are estimates and confirmed shipment cannot be determined until all manufacturing details are known. ProPump & Controls Inc. will make reasonable efforts to establish a delivery schedule after receipt of an executed contract and all approvals. Seller shall not be liable for special or consequential damages caused by delay in delivery. Customer agrees to execute bill and hold contract in the event of order delay.

LATE FEES: Late fee of 1 1/2 % of the unpaid balance will be charged per month on all accounts which are past due, plus any collections or attorney's fees incurred in settlement of past due accounts.

LIEN: Seller retains a security interest in all products sold to buyer until the purchase price and other charges, if any, are paid in full as provided in Article 9 of the Uniform Commercial Code. Seller will file a Mechanics Lien or execute other documents as required to perfect the security interest in the products sold.

TAXES: State, city and local taxes are excluded from the contract price unless otherwise noted. Sales tax will be invoiced on the contract price unless written exemption is provided.

CELL MODEM: Your pump station includes one year of cell modem connection unless otherwise indicated and will automatically renew annually unless you cancel your service. You will be invoiced 30 days prior to renewal with NET 30 terms. Service shall be terminated for non-payment. If you choose not to renew, please notify ProPump & Controls no later than 30 days in advance of your renewal date. If service is terminated for any reason, a reconnection fee of \$375 shall apply plus any unpaid balance.

FACTORY AUTHORIZED WARRANTY: ProPump & Controls, Inc. warrants products manufactured by ProPump and associated component parts and/or labor, for defects in materials and workmanship for a period of one year following date of installation by ProPump, but not later than fifteen months from date of invoice if installed by others.

For products sold by ProPump & Controls, Inc but manufactured wholly by others, ProPump will extend the manufacturer's warranty to the customer, and will assist in handling warranty claims. Standard manufacturer warranties for water pumping systems are one year from date of startup by ProPump, but not later than fifteen months from date of manufacturers invoice. Provided that all installation and operation responsibilities have been properly performed, manufacturer will provide a replacement part or component during the warranty life. Repairs done at manufacturer's expense must be pre-authorized.



ProPump and Controls, Inc.
30 Stonecrest Ct, Suite 100
Shelbyville, KY, 40065-8128
Phone: (502)633-0677 ext

QUOTE

Quote Nbr.:	Q010799
Order Date:	11/24/2025
Valid Until:	12/9/2025
Sales Person:	COREY BERGIN
Reference:	
Payment Terms:	Net 30
Financing terms available upon request	

This proposal may contain equipment which requires costly means to remove and replace for service or repair, due to site conditions. ProPump & Controls will not accept liability for any costs associated with the removal or replacement of equipment in difficult-to-access locations, unless specifically agreed to in writing on the original sales proposal. This includes the use of cranes larger than 15 tons, divers, barges, helicopters, or other unusual means. All such extraordinary costs shall be borne by the customer, regardless of the reason necessitating removal of the product from service.

ProPump & Controls, Inc., or its sub-contractors are not responsible for damage to turf or cart paths, provided that Owner's Representative has designated reasonable routes for access to the site, for vehicles including heavy trucks and cranes, and ProPump & Controls, Inc and subcontractor personnel have followed those routes. For access routes which require extraordinary means to traverse, such as wet ground or thin cart paths which may require placement of boards or steel plates to prevent damage, additional costs may be incurred if conditions are not brought to ProPump's attention prior to submitting a proposal.

Warranty may be voided in the event of any of the following:

- Default of any agreement with supplier or manufacturer.
- The misuse, abuse of the pumping equipment outside is intended and specified use.
- Failure to conduct routine maintenance.
- Handling any liquid other than irrigation water.
- Exposure to electrolysis, erosion, or abrasion.
- Presence of destructive gaseous or chemical solutions.
- Over voltage or unprotected low voltage.
- Unprotected electrical phase loss or phase reversal.
- TDS over 1000mg
- PH levels lower than 6.0
- Calcium hardness less than 50 mg/L
- Alkalinity less than 100mg/L
- Chloride and or sulfate ions greater than 50 mg/L each
- Free chlorine or use of other strong biocides.
- Langelier index from -50 to + 1.5
- Damage occurring when using control panel as service disconnect.

The foregoing constitutes ProPump & Control's sole warranty and has not nor does it make any additional warranty, whether express or implied, with respect to the pumping system or component. ProPump & Controls, Inc. makes no warranty, whether express or implied, with respect to fitness for a particular purpose or merchantability of the pumping system or component. ProPump & Controls, Inc. shall not be liable to purchaser or any other person for any liability, loss, or damage caused or alleged to be caused, directly or indirectly, by the pumping system. In no event shall ProPump & Controls, Inc. be responsible for incidental, consequential, or act of God damages, nor shall manufacturer's liability for damages to purchaser or any other person ever exceed the original factory purchase price.



QUOTE

ProPump and Controls, Inc.
 30 Stonecrest Ct, Suite 100
 Shelbyville, KY, 40065-8128
 Phone: (502)633-0677 ext

Quote Nbr.: **Q011134**
 Order Date: 3/11/2026
 Valid Until: 3/26/2026
 Sales Person: TRAVIS HUDGENS
 Reference:
 Payment Terms: Net 30
 Financing terms available upon request

BILL TO: Lakes of Taylor Golf Club 25505 Northline Rd Taylor MI 48180-4597 United States of America	SHIP TO: Lakes of Taylor Golf Club 25505 Northline Rd Taylor MI 48180-4597 United States of America
--	--

SCOPE OF WORK:
 New ABB 580 VFD
 New 2" Oval electric flush valve

NO.	ITEM	QTY.	UOM	PRICE	AMOUNT
1	80-700-060: VFD, 60HP	1.000	EA	7,058.000	7,058.00
2	18-000-116: VALVE,FLUSH,2",OOVAL,N.C.,120VAC	1.000	EA	508.532	345.80
3	SMALL PART: Small Part: Wire, Mounting, Galvanized Pipe	1.000	EA	150.000	150.00
4	LABOR: Technician Labor	1.000	HR	2,160.000	2,160.00

Signature: _____
 Date: _____
 Requested Delivery Date _____

Quote Total: 9,713.80
Less Discount: 0.00
Tax Total: 453.23
Total (USD): 10,167.03
Deposit Due:



QUOTE

ProPump and Controls, Inc.
30 Stonecrest Ct, Suite 100
Shelbyville, KY, 40065-8128
Phone: (502)633-0677 ext

Quote Nbr.: Q011134
Order Date: 3/11/2026
Valid Until: 3/26/2026
Sales Person: TRAVIS HUDGENS
Reference:
Payment Terms: Net 30
Financing terms available upon request

PAYMENT TERMS : All Purchase Orders are subject to acceptance by ProPump & Controls, Inc. Receipt of production deposit, verification of acceptable credit, and confirmation of order are required before production. All orders subject to 50% production deposit. Balance due 30 days from date of invoice.

If shipment or installation of equipment is delayed by customer request at no fault of ProPump & Controls, Inc., customer agrees to amend the contract or purchase order as follows, unless superseded by other terms noted on the accepted contract or agreement.

- Delayed shipment: Customer agrees to pay any storage fees requested by equipment manufacturer, if applicable.
- Equipment Installation delayed at customer request following shipment: ProPump will furnish equipment and labor to offload equipment and store on site as directed by customer. Equipment and labor costs for the offload shall be additional to the quoted price. ProPump shall invoice customer for 90% of the equipment price, less any previous production deposits, due net 30 days following delivery of product. Balance for equipment and installation labor will be invoiced on completion of work.

Orders placed under Preferred Customer Agreements include all discounts and fee payments.

Please note that all invoices paid by Credit Card are subject to a 2.8% convenience fee without prior agreement.

How to order: Please help us expedite your order by providing the following:

Is this sale taxable? (circle one) Yes No (If the order is non-taxable, a tax exempt certificate for the "ship to" state must be submitted with this order.)

Please return one signed copy of this quotation on acceptance. Merchandise delivered or shipped is due and payable to: ProPump & Controls, Inc. 30 Stonecrest Ct. Suite 100 Shelbyville, KY 40065. Fax number: 502-633-0733 Phone: 800-844-0677.

DELIVERY: Delivery dates are estimates and confirmed shipment cannot be determined until all manufacturing details are known. ProPump & Controls Inc. will make reasonable efforts to establish a delivery schedule after receipt of an executed contract and all approvals. Seller shall not be liable for special or consequential damages caused by delay in delivery. Customer agrees to execute bill and hold contract in the event of order delay.

LATE FEES: Late fee of 1 1/2 % of the unpaid balance will be charged per month on all accounts which are past due, plus any collections or attorney's fees incurred in settlement of past due accounts.

LIEN: Seller retains a security interest in all products sold to buyer until the purchase price and other charges, if any, are paid in full as provided in Article 9 of the Uniform Commercial Code. Seller will file a Mechanics Lien or execute other documents as required to perfect the security interest in the products sold.

TAXES: State, city and local taxes are excluded from the contract price unless otherwise noted. Sales tax will be invoiced on the contract price unless written exemption is provided.

CELL MODEM: Your pump station includes one year of cell modem connection unless otherwise indicated and will automatically renew annually unless you cancel your service. You will be invoiced 30 days prior to renewal with NET 30 terms. Service shall be terminated for non-payment. If you choose not to renew, please notify ProPump & Controls no later than 30 days in advance of your renewal date. If service is terminated for any reason, a reconnection fee of \$375 shall apply plus any unpaid balance.

FACTORY AUTHORIZED WARRANTY: ProPump & Controls, Inc. warrants products manufactured by ProPump and associated component parts and/or labor, for defects in materials and workmanship for a period of one year following date of installation by ProPump, but not later than fifteen months from date of invoice if installed by others.

For products sold by ProPump & Controls, Inc but manufactured wholly by others, ProPump will extend the manufacturer's warranty to the customer, and will assist in handling warranty claims. Standard manufacturer warranties for water pumping systems are one year from date of startup by ProPump, but not later than fifteen months from date of manufacturers invoice. Provided that all installation and operation responsibilities have been properly performed, manufacturer will provide a replacement part or component during the warranty life. Repairs done at manufacturer's expense must be pre-authorized.



ProPump and Controls, Inc.
30 Stonecrest Ct, Suite 100
Shelbyville, KY, 40065-8128
Phone: (502)633-0677 ext

QUOTE

Quote Nbr.:	Q011134
Order Date:	3/11/2026
Valid Until:	3/26/2026
Sales Person:	TRAVIS HUDGENS
Reference:	
Payment Terms:	Net 30
Financing terms available upon request	

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Warranty may be voided in the event of any of the following:

- Default of any agreement with supplier or manufacturer.
- The misuse, abuse of the pumping equipment outside is intended and specified use.
- Failure to conduct routine maintenance.
- Handling any liquid other than irrigation water.
- Exposure to electrolysis, erosion, or abrasion.
- Presence of destructive gaseous or chemical solutions.
- Over voltage or unprotected low voltage.
- Unprotected electrical phase loss or phase reversal.
- TDS over 1000mg
- PH levels lower than 6.0
- Calcium hardness less than 50 mg/L
- Alkalinity less than 100mg/L
- Chloride and or sulfate ions greater than 50 mg/L each
- Free chlorine or use of other strong biocides.
- Langelier index from -50 to + 1.5
- Damage occurring when using control panel as service disconnect.

The foregoing constitutes ProPump & Control's sole warranty and has not nor does it make any additional warranty, whether express or implied, with respect to the pumping system or component. ProPump & Controls, Inc. makes no warranty, whether express or implied, with respect to fitness for a particular purpose or merchantability of the pumping system or component. ProPump & Controls, Inc. shall not be liable to purchaser or any other person for any liability, loss, or damage caused or alleged to be caused, directly or indirectly, by the pumping system. In no event shall ProPump & Controls, Inc. be responsible for incidental, consequential, or act of God damages, nor shall manufacturer's liability for damages to purchaser or any other person ever exceed the original factory purchase price.

Taylor Meadows Pump house work

The Variable frequency drive and down pipe replacement that has to be fixed asap is not work that plumbing companies do. There are only a handful of companies that do this type of work in the golf course industry. The company ProPumps has been the main company that does specialized Pump house work in the northern part of the United States. I also received a quote from Pumpman out of southern Ohio which was significantly more expensive because of how far away the company is. ProPumps is a very reputable company in the golf industry and stands by their work and I have had great experiences with them in the past. ProPumps should be able to do both the pressure maintenance downpipe replacement and VFD replacement within the same day. It is crucial that we get both of these repairs done as soon as possible to avoid the potential of losing turf.

Daniel Heidt
Superintendent
Taylor Meadows Golf Club

CITY OF TAYLOR SOLE SOURCE JUSTIFICATION FORM

INSTRUCTIONS: Complete this entire form, including **BOTH SECTIONS I and II** for all sole source purchases. Send the completed form to the Budget & Finance Department, Central Purchasing. In compliance with Section 4.12 of the City of Taylor Purchasing Policy, this document shall accompany any sole source request to the City Council as part of council package for review and consideration. Failure to provide the requested information and supporting documentation will most likely result in postponement.

If you have any questions, or need additional assistance, contact the Purchasing Office at 734.374.1459 or 734.374.1396. Information is also available at <http://www.cityoftaylor.com/purchasing>.

NOTE:

- 1. Price is never a basis for a sole source request.
- 2. Supporting documentation must accompany this form.

Requisition Title (or) #: _____

Commodity/service being purchased: GOLF COURSE PUMPS Proposed purchase price: \$ 17,328

Proposed / Requested Supplier: PRO PUMPS & CONTROLS, INC.

Requested by: ALEC GRANT

Department: GOLF

Date: 3/23/26

Email/phone #: (734) 287-2100

Sole source justification prepared by: ALEC GRANT

I. SOLE SOURCE JUSTIFICATION

1. A **Sole Source Purchase** is available from only one supplier and meets at least one of the following criteria (please check the appropriate boxes):

- One-of-a-kind** - The commodity or service has no competitive product alternatives on the market.
- Compatibility** - The commodity or service matches existing brand of equipment for compatibility.
- Replacement part** - The commodity is a replacement part for a specific brand of existing equipment.
- Unique design** - The commodity or service meets physical design or quality requirements.
- Professional Service** - The service falls under the Purchasing Policy Professional Service Section 4.10.

If any of the above apply – Are you aware of other sources (other than the one requested) that could offer the exact brand or service, such as a distributor, third party broker, or reseller? If so, please list possible sources.

OR

- Delivery date** - ONLY ONE SUPPLIER can meet necessary delivery date requirement for the required quantity on ____.
- Emergency** - URGENT NEED for the item or service does not permit soliciting competitive bids, as in cases of emergencies, disaster, etc., which was required on ASAP

2. Briefly explain what it is about this product or service that only this vendor can meet: _____

IT IS A SPECIALIZED PRODUCT/SERVICE THAT IS SPECIFIC TO GOLF COURSES. THE COMPANY HAS NO COMPETITORS IN THIS REGION & DOES ALL OF GOLF COURSE PUMP REPAIR/REPLACEMENT IN MICHIGAN.

CITY OF TAYLOR SOLE SOURCE JUSTIFICATION FORM

- 3. Indicate if the product or service has been purchased for this department in the past, the approximate date of purchase, and purchase order number, if known: _____
- 4. List the specific important features or specific performance specifications or parameters that make this product or service unique or proprietary, AND indicate specifically why these unique features are important to your department operations: GOLF SPECIFIC PUMPS. UNIQUE SERVICE W/O COMPETITORS IN THE AREA
- 5. List other suppliers generally believed to offer the same or very similar product or service. Indicate if they were contacted for a description and/or price of their product or service. If they were not contacted, indicate why they were omitted. Indicate for each specifically why their product or service is judged to be unacceptable.

- a) GEROW EQUIPMENT COMPANY (PUMP MAN) - MORE EXPENSIVE QUOTE
 - b) _____
 - c) _____
- } OTHER COMPANIES NOT INTERESTED IN DOING THE WORK/INSTALLATION

II. DOCUMENTATION OF PRICE REASONABLENESS

Check the box(es) that apply and provide information:

- 1. I determined that the price is reasonable for one of the following reasons:
 - (a) I compared the proposed price to prices I previously paid for the same or similar goods and/or services. See PO# (Specify price: \$) _
 - (b) I compared the proposed price to current published catalog, price lists, or market prices as documented in the attachments (attach relevant documentation) and the proposed price is less.
 - (c) I compared the proposal price to similar benchmarks, (e.g., dollars per pound, horsepower, or other units of measure) to identify any gross inconsistencies. Describe comparisons made: _____
 - (d) Based on my knowledge of the market, my experience of prior similar proposals. Describe basis of market knowledge or reference prior proposals: _____
 - (e) The price is set by law or regulations. Provide reference to law or regulation: _____
 - (f) The goods or services are available on the market for the same or similar price. Provide reference to market pricing information: _____
- 2. Sections 1 (a) through (f) do not apply. (Purchasing will contact you to discuss price reasonableness before a requisition can be processed.)

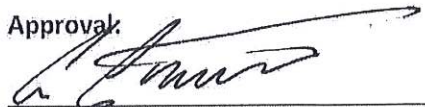
CITY OF TAYLOR SOLE SOURCE JUSTIFICATION FORM

III. CONFLICT OF INTEREST STATEMENT

The Requestor agrees that there is no real or potential Conflict of Interest in recommending this product and/or service as a Sole Source procurement. (NOTE: If you do have a real or potential conflict of interest, please contact the appropriate Purchasing Agent.)

As the authorized department official, I certify that the above justification is accurate and complete to the best of my knowledge and belief.

Approval:



Signature of Department Head

3/23/26
Date

ALEC GRANT
Printed Name



Signature of Purchasing Manager

3-24-26
Date

Printed Name



May 1 - September 27

Weekday / Weekend Holiday

	9 Holes Walking		9 Holes Riding		18 Holes Walking		18 Holes Rid	
	WD	WE	WD	WE	WD	WE	WD	WE
Resident	\$33	x	\$42	x	\$45	\$60	\$60	\$75
Non-Resident	\$36	x	\$45	x	\$50	\$70	\$65	\$85
Adult Midday <small>Valid Anyday after 12:00pm</small>	\$33	\$40	\$42	\$49	\$45	\$60	\$60	\$69
Senior Resident <small>NOT VALID FRI/SAT/SUN</small>	\$25	x	\$31	x	\$28	X	\$38	X
Senior Non-Resident <small>NOT VALID FRI/SAT/SUN</small>	\$29	x	\$35	x	\$32	X	\$42	X
Junior <small>NOT VALID FRI/SAT/SUN</small>	\$25	x	\$31	x	\$28	X	\$38	X

Weekend

Weekday

Early Bird Nine Hole

30 minutes after the first tee time

\$39

x

Early Twilight

after 3pm on weekends

\$59

x

Twilight

after 5pm daily
all you can play until sunset

\$39

\$35

May 1 - September 20

Weekday / Weekend Holiday



	9 Holes Walking		9 Holes Riding		18 Holes Walking		18 Holes Rid	
	WD	WE	WD	WE	WD	WE	WD	WE
Resident	\$ 25	X	\$ 34	X	\$ 29	\$ 39	\$ 44	\$ 54
Non-Resident	\$ 30	X	\$ 39	X	\$ 35	\$ 45	\$ 50	\$ 60
Adult Midday <small>Valid Anyday after 12:00pm</small>	\$ 25	\$ 28	\$ 34	\$ 37	\$ 29	\$ 35	\$ 44	\$ 50
Senior Resident <small>NOT VALID SAT/SUN</small>	\$ 18	X	\$ 24	X	\$ 22	X	\$ 32	X
Senior Non-Resident <small>NOT VALID SAT/SUN</small>	\$ 21	X	\$ 27	X	\$ 24	X	\$ 34	X
Junior <small>NOT VALID SAT/SUN</small>	\$ 18	X	\$ 24	X	\$ 22	X	\$ 32	X
Youth on Course <small>WEEKDAYS AFTER 1PM</small>	\$ 5	X	\$ 11	X	\$ 5	X	\$ 15	X

Weekend

Weekday

Early Bird Nine Hole

30 minutes after the first tee time

\$29

x

Early Twilight

\$44

x



May 1 - September 28

Weekday / Weekend Holiday

9 Holes Walking		9 Holes Riding		18 Holes Walking		18 Holes Rid	
WD	WE	WD	WE	WD	WE	WD	WE

Resident

\$30	x	\$39	x	\$38	\$54	\$53	\$69
------	---	------	---	------	------	------	------

Non-Resident

\$33	x	\$42	x	\$44	\$61	\$59	\$76
------	---	------	---	------	------	------	------

Adult Midday

Valid Anyday after 12:00pm

\$30	\$37	\$39	\$46	\$38	\$54	\$53	\$63
------	------	------	------	------	------	------	------

Senior Resident

NOT VALID FRI/SAT/SUN

\$22	x	\$28	x	\$25	X	\$35	X
------	---	------	---	------	---	------	---

Senior Non-Resident

NOT VALID FRI/SAT/SUN

\$26	x	\$32	x	\$29	X	\$39	X
------	---	------	---	------	---	------	---

Junior

NOT VALID FRI/SAT/SUN

\$22	x	\$28	x	\$25	X	\$35	X
------	---	------	---	------	---	------	---

Weekend

Weekday

Early Bird Nine Hole

30 minutes after the first tee time

\$34

x

Early Twilight

after 3pm on weekends

\$53

x

Twilight

after 5pm daily
all you can play until sunset

\$38

\$32

May 1 - September 21

Weekday / Weekend Holiday

9 Holes Walking		9 Holes Riding		18 Holes Walking		18 Holes Rid	
WD	WE	WD	WE	WD	WE	WD	WE

Resident

\$ 23	X	\$ 32	X	\$ 27	\$ 37	\$ 42	\$ 52
-------	---	-------	---	-------	-------	-------	-------

Non-Resident

\$ 28	X	\$ 37	X	\$ 33	\$ 43	\$ 48	\$ 58
-------	---	-------	---	-------	-------	-------	-------

Adult Midday

Valid Anyday after 12:00pm

\$ 23	\$ 26	\$ 32	\$ 35	\$ 27	\$ 33	\$ 42	\$ 48
-------	-------	-------	-------	-------	-------	-------	-------

Senior Resident

NOT VALID SAT/SUN

\$ 16	X	\$ 22	X	\$ 20	X	\$ 30	X
-------	---	-------	---	-------	---	-------	---

Senior Non-Resident

NOT VALID SAT/SUN

\$ 19	X	\$ 25	X	\$ 22	X	\$ 32	X
-------	---	-------	---	-------	---	-------	---

Junior

NOT VALID SAT/SUN

\$ 16	X	\$ 22	X	\$ 20	X	\$ 32	X
-------	---	-------	---	-------	---	-------	---

Youth on Course

WEEKDAYS AFTER 1PM

\$ 5	X	\$ 11	X	\$ 5	X	\$ 15	X
------	---	-------	---	------	---	-------	---

Weekend

Weekday

Early Bird Nine Hole

30 minutes after the first tee time

\$29

x

Early Twilight

\$42

x

City of Taylor
Blanket Purchase Approval Form

Department: Lakes of Taylor Golf
Fiscal Year: FY26

Supplier Name	Supplier Number (Auto Populated)	Requested Amount Not to Exceed	Goods/Service Description
8 trax	3018	\$ 10,000	Entertainment
A-1 SERVICES	707	\$ 4,000	contractor
Ace's Wild Entertainment	369	\$ 4,000	Entertainment
Acushnet/Titleist Company	488	\$ 45,000	merchandise
Adidas	337	\$ 15,000	merchandise
Ahead	12586	\$ 8,000	merchandise
Amazon Fulfillment Services, Inc.	5836	\$ 5,000	supplies
Bad Birdie LLC	10796	\$ 4,000	merchandise
Bar Stool	1505	\$ 4,000	merchandise
Beautiful Beginnings by Audra	3054	\$ 4,000	event décor
Belfor	6617	\$ 8,000	contractor
Belle Tire	831	\$ 4,000	tires
Biride Delivery	12252	\$ 3,000	merchandise
Black Clover	884	\$ 4,000	merchandise
Blue Tees	6163	\$ 2,000	merchandise
Branded Bills	4243	\$ 4,000	merchandise
Bridgestone Golf, Inc	515	\$ 15,000	merchandise
Cakes by Stephanie	2068	\$ 4,000	food and beverage
Callaway Golf	701	\$ 20,000	merchandise
Catapult Group	11793	\$ 20,000	merchandise
Cintas Fire Protection	6042	\$ 18,000	contractor
Cleveland Golf/Srixon	1647	\$ 6,000	merchandise
Cornerstone Fence	6033	\$ 4,000	contractor
Darran Hutchinson (beer labels)	1316	\$ 5,000	supplies
Deaborn Brand Sausage	1634	\$ 2,000	food and beverage
De-Cal Inc	3356	\$ 40,000	contractor
Delbene Produce	1286	\$ 8,000	food and beverage
Detroit Cutlery	956	\$ 2,000	kitchen supplies
Easy Picker	7227	\$ 6,000	driving range supplies
FairFax Electric	10651	\$ 8,000	contractor
Fire Systems of Michigan	12266	\$ 10,000	contractor
ForeUp	6306	\$ 6,000	POS system
Fortune Fish	6324	\$ 10,000	food and beverage
Frog Holler Inc.	3731	\$ 20,000	food and beverage
Gandol	5569	\$ 2,000	contractor
Gear For Sports Inc.	4838	\$ 4,000	merchandise
General Linen	10724	\$ 10,000	supplies
Gordon Food Service Inc.	429	\$ 200,000	food and beverage
H & F Marketing LLC	7199	\$ 5,000	supplies
Hesco	5937	\$ 3,000	contractor
Hobart	10667	\$ 4,000	contractor
Hotsy Midwest	7127	\$ 2,000	contractor
Hutson, Inc. of Michigan	11585	\$ 4,000	contractor
Instawork	11812	\$ 20,000	temp work agency
J & J Clean & Clear Inc.	609	\$ 4,000	contractor
J & M Golf	609	\$ 8,000	merchandise
Joseph Elliot	7176	\$ 3,000	merchandise
Key Awards	11541	\$ 4,000	trophies
La Grasso Bros produce	10682	\$ 6,000	food and beverage
Leonard's Syrups	1381	\$ 8,000	food and beverage
Mark's Meats	7018	\$ 6,000	food and beverage

City of Taylor
Blanket Purchase Approval Form

MI Custom Signs	2124	\$	7,500	marketing signage
Michigan Bread.com	6525	\$	8,000	food and beverage
Michigan Liquor Control Commission	3741	\$	3,000	contractor
Michigan Tournament Fleet Inc.	2615	\$	3,000	golf cart rental
Midwest Golf & Turf	2753	\$	5,000	golf cart supplies
Mizuno USA, Inc.	5992	\$	8,000	merchandise
Mr. Gas	10104	\$	2,000	contractor
Muck Men	4988	\$	4,000	contractor
Northern Haserot	11395	\$	60,000	food and beverage
NuCentry Textile	6470	\$	8,000	event décor
On the Mark Golf	11106	\$	4,000	merchandise
O'reilly Auto Parts	4455	\$	4,000	auto supplies
Pepsi-Cola	390	\$	40,000	food and beverage
Pest Masters Inc.	466	\$	2,500	contractor
Ping Inc.	940	\$	35,000	merchandise
Proud 90	5242	\$	4,000	merchandise
Range Servant	920	\$	15,000	driving range supplies
Rave Beverage	6782	\$	12,000	food and beverage
Restaurant Depot	11061	\$	2,000	restaurant supplies
Samco	6877	\$	4,000	contractor
Schreiber Roofing	11732	\$	4,000	contractor
Sell's equipment	5228	\$	4,000	parts
SOHN LINEN SERVICE INC	4966	\$	5,000	event décor
SunGlo Carpet Cleaning	1162	\$	5,000	contractor
SYSCO DETROIT LLC	409	\$	20,000	food and beverage
Taylor Made Golf Company	491	\$	30,000	merchandise
Top Shelf Cigars	10124	\$	8,000	cigars
Travis Mathews	2176	\$	5,000	merchandise
Uline	6730	\$	8,000	office supplies
Unspecified Vendor	11594	\$	5,000	misc.
Vantage Apparel	6148	\$	5,000	merchandise
Vice Sports	11514	\$	4,000	merchandise
Volvik USA	10980	\$	5,000	merchandise
Waggle Golf	11563	\$	4,000	merchandise
Water Landscapes	4146	\$	2,500	golf course fountains
Wittek	3570	\$	2,000	driving range supplies

The purpose of this form is to obtain blanket council approval for the procurement of small, recurring goods/services which may exceed \$2,000 total spend for the year. This approval helps streamline the process and create efficiency for departments. Approvals are only for one fiscal year and may not be carried over to a subsequent year.



Water Landscapes, LLC
 PO Box 925
 Union Lake, MI 48387
 2483795351
 WaterLandscapes@gmail.com

Invoice 10108

BILL TO
 Lakes of Taylor Golf Club
 Lakes of Taylor Golf Club
 c/o Bob
 25555 Northline Rd
 Taylor, Mi 48188

DATE 02/16/2026	PLEASE PAY \$3,119.20	DUE DATE 02/17/2026
--------------------	--	------------------------

DATE	DESCRIPTION	QTY	RATE	AMOUNT
02/16/2026	New Control Panel for Hole #14 Fountain/Labor/Installation	1	3,200.00	3,200.00

PLEASE NOTE: All payments will be applied to oldest invoices first.

A 2% monthly late charge will be assessed to all invoices not paid within 30 days.

If you would like to pay by credit card, please contact us and we can resend the invoice with a credit card payment option. There is a 3% credit card processing fee.

SUBTOTAL	3,200.00
TAX	0.00
TOTAL	3,200.00
PAYMENT	80.80

TOTAL DUE \$3,119.20

THANK YOU.

Thank you for your business and have a great day!

Motion By: Jill Brandana **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve the Proclamation honoring former Mayor Richard Marshall.

Unanimously carried
CCR #: 9.421-25

Motion By: Angie Winton **Supported By:** Jill Brandana
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To receive & file the General Employee Retirement System Performance Review June 30, 2025.

Unanimously carried
CCR #: 9.422-25

Motion By: Angie Winton **Supported By:** Jill Brandana
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To receive & file the August 2025 Paid Invoice Report.

Unanimously carried
CCR #: 9.423-25

Motion By: Angie Winton **Supported By:** Jill Brandana
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To receive & file the August 2025 Budget Performance Report.

Unanimously carried
CCR #: 9.424-25

Motion By: Angie Winton **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To receive & file Presentation from Third Judicial Circuit of Michigan.

Unanimously carried
CCR #: 9.425-25

Motion By: Lindsey Rose **Supported By:** Tina Daniels
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve ESO Solutions, current vendor, for annual medical and fire service reporting maintenance in the amount not to exceed \$30,455. Funded through General Fund-Fire Department-Contractual Service Account.

Unanimously carried
CCR #: 9.431-25

Motion By: Angie Winton **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve authorizing the Mayor to sign the Purchase Agreement on behalf of the City of Taylor for the sale of Parcel ID numbers 60-004-01-0323-000 and 60-004- 01-0324-000 for an amount of \$6,500, plus closing costs. Proceeds go to the General Fund.

Unanimously carried
CCR #: 9.432-25

Motion By: Charley Johnson **Supported By:** Angie Winton
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve Stryker Pro care Service for annual preventative maintenance of Lucas and LP15 heart monitoring devices in the amount not to exceed \$9,891. Funded through General Fund- Fire Department Contractual account.

Unanimously carried
CCR #: 9.433-25

Motion By: Angie Winton **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Tina Daniels, Lindsey Rose
Nays: Douglas Geiss
Resolved: To receive & file the first reading of Appendix A, City of Taylor, Michigan Code of Ordinances, regulating the development and use of land to rezone 26655 Northline, part of parcel#60-078-99- 0001-706 of the NW ¼ Section 30 T.3S., R.10E., City of Taylor, Wayne County, Michigan from O-1 (Office) to I-1 (Light Industrial). The property is located on the southwest corner of Northline Road between Inkster and Lange Roads.

Roll Call Vote

5 – 1 Motion carried
CCR #: 9.434-25

Motion By: Angie Winton **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels
Nays: None
Resolved: To amend CCR #: 8.352-25 Engineering services, per the attached agreement, for the approved city vendors for engineering services for the 2024/2025 and 2025/2026 Fiscal Years, funded through TBA Capital Outlay.

Unanimously carried
CCR #: 9.435-25

Motion By: Lindsey Rose **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve JLR Group, low qualified bid, for interior and exterior painting services at the Petting Farm in an amount of \$52,920 with an additional contingency amount of \$2,000, for a total amount not to exceed \$54,920. Funded through TBA Capital Outlay.

Unanimously carried
CCR #: 9.436-25

Motion By: Charley Johnson **Supported By:** Tina Daniels
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve amending the 2021-2023 Park millage allocation, per the attached documents, this amendment would reallocate \$75,334 for improvements to Heritage Park, specifically for the installation of Pickleball courts.

Unanimously carried
CCR #: 9.437-25

Motion By: Angie Winton **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Tina Daniels, Lindsey Rose
Nays: Jill Brandana, Douglas Geiss
Resolved: To approve Tesla, customer direct pricing, for the purchase of a Tesla Cybertruck Electric Vehicle for an amount not to exceed \$84,750, funded through the Community Energy Management Grant.

Interested Parties: R. Pratt
Roll Call Vote

4 – 2 Motion carried
CCR #: 9.438-25

Motion By: Jill Brandana **Supported By:** Lindsey Rose
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve the purchase of Brinc Indoor Tactical Drone and Brinc Ball Tactical Phone from Brinc Drones, Inc., sole source, for an amount not to exceed \$26,100, funded through the 2023 JAG Grant.

Unanimously carried
CCR #: 9.439-25

Motion By: Jill Brandana **Supported By:** Charley Johnson
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve opening the Public Hearing to present the program year (PY) 2024 Consolidated Annual Performance Evaluation Report (CAPER) for the Community Development Block Grant.

Unanimously carried
CCR #: 9.440-25

Motion By: Charley Johnson **Supported By:** Jill Brandana
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve closing the Public Hearing that presented the program year (PY) 2024 Consolidated Annual Performance Evaluation Report (CAPER) for the Community Development Block Grant.

Unanimously carried
CCR #: 9.441-25

Motion By: Charley Johnson **Supported By:** Angie Winton
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve three (3) Planning Department employees to attend the Planning Michigan 2025 Conference in Kalamazoo, MI, for an amount of \$3,109, plus out of pocket expenses, funded through Planning Department - Training and Transportation.

Unanimously carried
CCR #: 9.442-25

Motion By: Jill Brandana **Supported By:** Lindsey Rose
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve Dominion Voting, sole source, for the programming of the 2025 Local Primary and General elections in the amount not to exceed \$6,000 funded through General Fund, Clerk's Office, Election Supplies.

Unanimously carried
CCR #: 9.443-25

Motion By: Charley Johnson **Supported By:** Tina Daniels
Ayes: Charley Johnson, Angie Winton, Jill Brandana, Douglas Geiss, Tina Daniels, Lindsey Rose
Nays: None
Resolved: To approve the issuance of a Carnival License to St. Alfred's to hold a carnival on September 19th through September 21st, 2025.

Unanimously carried
CCR #: 9.444-25



Knight Watch Inc
3005 Business One Dr.
Kalamazoo, MI
49048, US

Customer
City of Taylor
23555 Goddard Rd.
Taylor, MI
48180, US

Prepared By:
Lee Spillman
(616) 235-2100
lspillman@knightwatch.net

Guido Ulin
(734) 374-1365
gulin@ci.taylor.mi.us

Project: City of Taylor - Fire Alarm Projects

Scope of Work

Provide (1) CAT6 cable from desired IT Rack to Fire Alarm Panel for communication for each building

Cat 6 Connection locations for each site per 2/26/26 email:

- City Hall – City Hall MDF (Upstairs Server room)
- Police Department – Downstairs PD IDF (Downstairs where the Clemis switch is. Ask Cpl. McDermott or Lt. Regan for access)
- Fire Station 1 - Switching closet on the top floor by the training room.
- Media Center – (IDF in the locked maintenance room)
- Parks Maintenance Building (IDF off the conference room by the copier.)
- Library (Only one IDF. Julie P. knows the location and has the key.)
- 23rd District Court (MDF by Deanna’s office.)
- Heritage Hall (Only one IDF.)
- Recreation Center (Only one IDF.)
- Petting Farm (IDF in the main office building across from Alison’s office.)
- Animal Shelter (IDF by the laundry machines in the new part of the building.)
- TSX (Downstairs IDF next to the conference room in the main office.)
- Lakes of Taylor (IDF in the furnace room, next to the Pro Shop)
- DPW (The Telecom closet in the front of the building. Kasey has the key.)
- Senior Center (Only one IDF.)
- Fire Station 2 (IDF on the right as you enter the back door to the Garage.)
- Fire Station 3 (IDF on the far side from the dorms by the workshop.)
- Taylor Meadows Golf Course (IDF left of the ATM across from the management offices)

JAG will provide (1) 120v Circuit with breaker lock and Fire Alarm identification from nearest available panel to the FACP or NAC panel that requires power.

*Price is based on an average of all buildings as some are short, others are 200 feet away.

Subcontractor	Quantity
Subcontractor Cost - Cat6	1 ea
Subcontractor Costs - 120V	1 ea
Misc. Materials	1 ea

Included (+)

1. Open Air Cabling above Drop Ceiling &/or At Deck Level
 2. 120V Panel Power
-

Excluded (-)

1. Ceiling and/or wall removal, patching and painting

Notes

Summary

Subtotal	\$65,217.24
Taxes	\$0.00

\$65,217.24

Accepted By

Date

.....

.....

Quote Tabulation

Splash Pad

Name of Vendor	Address	Telephone	Price	Comments & Recommendations
Cornerstone Fence	24355 Telegraph Rd. Brownstown, MI 48134	734-318-4290	\$8,343	
Brady's Fence	11093 Armstrong Rd., South Rockwood, MI 48179	734-789-9879	\$10,100	
Upright Fence	37915 Ford Rd. Westland, MI 48185	734-729-5172	\$11,598	

Purchasing Agent Signature: _____ Date: _____ Time: _____

Cornerstone Fence

24355 Telegraph Road
Brownstown Charter Township, Michigan 48134

ESTIMATE #8452

SENT ON:
Mar 24, 2026

RECIPIENT:

Taylor Building & Maintenance

12111 Pardee Road
Taylor, Michigan 48180
Phone: 313 330 7511
HOA: NO



Phone: 734-318-4290
Email: sales@cornerstonefenceco.com
Website: https://CornerstoneFenceCo.com/

Product/Service	Description	Qty.	Total
Black CSF-20 Aluminum Fence - 6' High (with Circles) Commercial	Includes - Material and installation of 4' high aluminum fence.	76	\$8,968.00
Black Aluminum Walk Gate - 4' Wide	Includes - Material and installation of aluminum walk gate.	1	\$975.00
Black Aluminum Double Drive Gate - 6'x 6' Wide	Includes - Material and installation of 6'x 6' wide aluminum double drive gate.	1	\$1,850.00

A deposit of \$5,896.50 will be required to begin.

Total	\$11,793.00
--------------	--------------------

PLEASE READ CONTRACT IN FULL

ALL ESTIMATES ARE GOOD FOR 7 DAYS.

CORNERSTONE FENCE - INSTALLATION TERMS & CONDITIONS

1. Deposits & Payment Terms

All customers are required to pay a deposit prior to scheduling installation. The deposit amount is 50% of the total contract price, unless the customer is a contractor, in which case payment in full is required upfront.

The remaining balance is due in full within five (5) business days after completion of the project.

Any balance not paid within this time frame may be subject to a \$25 per week charge until paid in full.

2. Written Agreement Only

All agreements between Cornerstone Fence and the customer must be in writing. No verbal agreements, representations, or promises made by any Cornerstone Fence employee or installer shall be binding unless documented in writing.

Any changes to the scope of work must be approved in writing and may result in additional charges and/or schedule adjustments.

3. Grade, Slopes & Ground Conditions

Cornerstone Fence is not responsible for existing grade conditions, including dips, slopes, uneven terrain, or resulting gaps

Cornerstone Fence

24355 Telegraph Road
Brownstown Charter Township, Michigan 48134

ESTIMATE #8321

SENT ON:
Mar 03, 2026

RECIPIENT:

Taylor Building & Maintenance

12111 Pardee Road
Taylor, Michigan 48180
Phone: 313 330 7511
HOA: NO



Phone: 734-318-4290
Email: sales@cornerstonefenceco.com
Website: https://CornerstoneFenceCo.com/

Product/Service	Description	Qty.	Total
Black CSF-20 Aluminum Fence - 4-1/2' High with Circles	Includes - Material and installation of 4-1/2' high aluminum fence.	52	\$3,068.00
Black Aluminum Double Drive Gate - 4-1/2'h x12' Wide with Circles	Includes - Material and installation of 4-1/2'h x12' wide aluminum double drive gate.	1	\$1,800.00
Black Aluminum Double Drive Gate - 8' Wide with Circles	Includes - Material and installation of aluminum walk gate.	1	\$1,450.00
Black CSF-20 Aluminum Fence - 4-1/2' High with Circles (Reuse existing Material)	Includes - Taking Down and installation of existing 4-1/2' high aluminum fence. we will have to order some post and panel.	64	\$1,600.00
Black Aluminum Double Drive Gate - 4-1/2'x 8-10' Wide (Reuse Existing Gate) NEW POST	Includes - Material and installation of 4'x10' wide aluminum double drive gate.	1	\$275.00
Black Aluminum Walk Gate - 5' Wide (Reuse Existing Gate) NEW POST	Includes - Material and installation of aluminum walk gate.	1	\$150.00

A deposit of \$4,171.50 will be required to begin.

Total	\$8,343.00
--------------	-------------------

Quote Tabulation

Pavilion C

Name of Vendor	Address	Telephone	Price	Comments & Recommendations
Cornerstone Fence	24355 Telegraph Rd. Brownstown, MI 48134	734-318-4290	\$11,793	
Brady's Fence	11093 Armstrong Rd., South Rockwood, MI 48179	734-789-9879	\$13,300	
Upright Fence	37915 Ford Rd. Westland, MI 48185	734-729-5172	\$13,748	

Purchasing Agent Signature: _____

Date: _____

Time: _____



Wayne Lawn & Garden

39915 Michigan Ave
Canton, MI 48188
(734) 721-5220

Quote 19918

VALID UNTIL 03/26/2026
SALESPERSON Bryan Lubaway
bryan@waynelawn.com

SHIP TO
City Of Taylor DPW
25605 Northline Rd
Taylor, MI 48180
(734) 287-6550

BILL TO (#1921237) (☑️ AUTO-SEND)
City Of Taylor DPW
25605 Northline Rd
Taylor, MI 48180
(734) 287-6550
Tax ID: 38-6006926

CUST PO CONTACT pybarra@cityoftaylor.mi.gov (734) 287-6550

SHIP OUT Will Call **DROP SHIP** No

#	PART	DESCRIPTION	QTY	RATE	DISC	NET	EXT
1	HARDWARE	(MIS) tweels 26x12x12 rear tire	1.00	4,200.00		4,200.00	4,200.00
2	STTII72V-40BV-EFI	(SCA) STTII72V-40BV-EFI Scag Turf Tiger II Series- 40hp Briggs EFI Engine, 72" Velocity Deck	1.00	19,999.00	3,599.82(18.00%)	16,399.18	16,399.18
3	STTII72V-40BV-EFI	(SCA) STTII72V-40BV-EFI Scag Turf Tiger II Series- 40hp Briggs EFI Engine, 72" Velocity Deck	1.00	19,999.00	3,599.82(18.00%)	16,399.18	16,399.18

PAYMENT	DETAILS	DATE	AMT

COMMENTS

SUBTOTAL	44,198.00
DISCOUNTS	-7,199.64
TOTAL	36,998.36
PAYMENTS	0.00
BALANCE DUE	36,998.36

ACCEPTANCE: _____ **DATE:** _____

Items returned within 30 days of purchase with original invoice are subject to a 30% restocking fee. Special order items are only returnable with factory "ok", plus a 30% restocking fee and return freight. Special order items not picked up after 30 days will be placed back in inventory, a 30% restocking fee and inbound shipping are charged. No returns on electrical parts. Signers warrant authority to execute this sales order on behalf of customer.

Current version we want to go with this BID

Dixon Outdoor Equipment

Sales • Parts • Service

37980 Ann Arbor Rd, Livonia, MI 48150

(734) 453 - 6250

Estimate

Date	Estimate #
3/18/2026	12230

Customer Info

City of Taylor DPW
 Brad Snider
 25605 N Line Rd
 Taylor, MI 48180

Item	Description	Qty	Total
STTII-72v-31D...	Scag Turf Tiger 72" Vel. plus, 31 HP Kawasaki DFI Liq-Cooled		21,369.00
DISCOUNT	DISCOUNT - Sourcewell		-4,366.00
			17,003.00
STTII-72v-31D...	Scag Turf Tiger 72" Vel. plus, 31 HP Kawasaki DFI Liq-Cooled		21,369.00
DISCOUNT	DISCOUNT		-4,366.00
			17,003.00
MISC	Tweel Tires 26x12-12	4	3,996.00
DISCOUNT	DISCOUNT		-176.00
	Estimate is for 2 Scag Turf Tiger 72" Deck Mowers with Kawasaki Liquid Cooled Motors - Sourcewell Contract # 031121		
	Some popular accessories include Grass Flaps (\$475 installed each), Light Kits (\$385 installed each)		
	Delivery is included free of charge if needed.		

Subtotal \$37,826.00

Sales Tax (6.0%) \$0.00

Total \$37,826.00



Ground Maintenance, CE, Utility Tractors, Mowers, RTV's - 112624-KBA Ag Tractors with Related Attachments - 082923-KBA NJPA Arkansas 4600041718 Nebraska 14777 (OC) Mississippi 8200067336

ZD1611LF-72 WEB QUOTE #2943597 Date: 2/18/2026 1:52:50 PM -- Customer Information -- HALL, LANNY 25952 CITY OF TAYLOR LHALL@CITYOFTAYLORMI.GOVE 3136438478

Quote Provided By WOLVERINE RENTAL & SUPPLY, INC. James Ousnamer 5475 s state road Ann Arbor, MI 48108 email: JAMES@WOLVERINERENTAL.COM phone: 7346653223

-- Standard Features --

-- Custom Options --



ZD1600 Series ZD1611LF-72 *** EQUIPMENT IN STANDARD MACHINE ***

DIESEL ENGINE

Model: Kubota V1505-CR-TE5-ZD1 Type: Diesel, liquid (4cyl.) with DPF HP 30.8 Gross @ 2500 rpm Displacement: 91.4 cu. in. Battery 12v CCA 670A, RC115 min Alternator Capacity: 75 Amps / 12 Volts

OPERATING FEATURES

Zero Turn Radius Selectable Front Axle: Rigid/Oscillating Premium Air Ride Suspension Seat Advanced Engine Monitor Hands-free Hydraulic Deck Lift Hands-free Parking Brake Toolless Front Maintenance Lift 12 V Power Outlet Front Axle Tie Down Features Dual Element Air Filter Cup Holder

TRANSMISSION

Hydrostatic Drive (2) HST w/Gear Reduction Brake - Wet Multi Disks Forward Speeds 0 - 10.6 mph Reverse Speeds 0 - 5.3 mph

SAFETY EQUIPMENT

Seat Safety Switch Control Lever Safety Switch Parking Brake Safety Switch Foldable ROPS Electronic Key Shut Off

STEERING / MOTION CONTROL

(2) Hand Levers, Adjustable Hydraulically Damped, Adjustable

SIDE DISCHARGE MOWER

72" Kubota PRO Deck w/ACS 8 Gauge, 6" Deep Deck 1-5" Cut Height, Adjustable 1/4" Increments Flexible Discharge Cover 3 Blades

FLUID CAPACITY

Fuel Tank 13.1 gal Engine Coolant 3.96 qts w/ Recovery Tank Crankcase w/ Filter 4.1 qts Transmission Case and Axle Gear 12.8 qts and Axle Gear

POWER TAKE OFF

Hydraulic Independent PTO Shaft Drive Mower Deck Wet Disk Clutch

ENGINE/MACHINE MONITORING

DPF Regeneration Status Fuel Level Water Temperature Hour Meter RPM Oil Pressure Service Reminder

REAR DISCHARGE MOWER

72" Kubota PRO Deck 5.5" Deep Deck 1-5" Cut Height, Adjustable 1/4" Increments 3 Blades Bolt-on Skid Bars

DIMENSIONS

Height (rops up): 78.7" Height (rops down): 64.6" Length: 105.7" Width Overall (w/o Mower) 60.6" Wheelbase: 61.4" Weight 2094 lbs.

* Manufacturer's estimate.

TIRES AND WHEELS

Front 15 x 6.5 - 8 Flat-free Front 26 x 12.0 Turf, Low Profile

Table with pricing: ZD1611LF-72 Base Price: \$28,799.00, Configured Price: \$28,799.00, Sourcewell Discounts, Kubota Items: (\$6,335.78), Total Discount: (\$6,335.78), SUBTOTAL: \$22,463.22, Dealer Assembly: \$0.00, Freight Cost: \$262.50, PDI: \$400.00

Total Unit Price: \$23,125.72 Quantity Ordered: 2

Final Sales Price: \$46,251.44

Purchase Order Must Reflect Final Sales Price.

To order, place your Purchase Order directly with the quoting dealer

*All equipment specifications are as complete as possible as of the date on the quote. Additional attachments, options, or accessories may be added (or deleted) at the discounted price. All specifications and prices are subject to change. Taxes are not included. The PDI fees and freight for attachments and accessories quoted may have additional charges added by the delivering dealer. These charges will be billed separately. Prices for product quoted are good for 60 days from the date shown on the quote. All equipment as quoted is subject to availability.

Animal Shelter Generator – Settlement Summary, Funding Breakdown, and Council Approval Needed

From Guido Ulin <gulin@cityoftaylor.mi.gov>

Date Thu 3/19/2026 8:36 AM

To Ralph Richard <rrichard2@cityoftaylor.mi.gov>

Cc Dan Bzura <dbzura@cityoftaylor.mi.gov>; Kasey Sandifur <ksandifur@cityoftaylor.mi.gov>

 2 attachments (124 KB)

2026-01-29 City of Taylor Animal Shelter Generator Rental Estimate.pdf; 2026-03-06 SETTLEMENT CHECK 367993094 IN THE AMOUNT OF \$68,209.21.pdf;



Good morning Ralph,

I am sending this email regarding the next steps required for the generator at the Animal Shelter, including the costs associated with the rental of the temporary unit, the purchase of the replacement unit, and the need for you to take these items forward for Council approval at the 4/7/2026 meeting.

Below is a summary of the steps that have been taken to date, as well as those still required.

We have received the settlement check from SKS Electric's insurance provider in the amount of \$68,209.21. The breakdown of the settlement is as follows:

Rental Unit Cost / Payment Details

- Delivery and first month: \$6,296.00 (paid by SKS/Insurance)
- Pick-up fee: \$1,150.00 (paid by SKS/Insurance)
- Lead time for new unit: 20–25 weeks
- Payment for 5 months @ \$3,243 per month = \$16,215.00 (paid to the City by SKS/Insurance)
- Total rental payments by SKS/Insurance: \$23,661.00

We understand there is a multiplier if the unit runs more than eight hours in a month. Under this proposal, the City will assume any additional cost if that occurs.

New Unit Cost

- SKS/Insurance pays: \$44,548.21 (estimated repair cost)
- City of Taylor pays: \$21,922.39 (difference between repair vs. replacement cost)
- Total cost for the new unit: \$66,470.60

Total amount received by the City: \$68,209.21 (final capped settlement amount).

The City of Taylor acknowledges that if the temporary unit runs more than eight hours in any month, we will assume the additional charges. Additionally, if the lead time for the new generator exceeds five months, the City will be responsible for any rental costs beyond the initial five-month period.

We will need to seek Council approval to make the rental payments shown in the attachment included with this email, as well as approval for the purchase of the new unit that has already been ordered. An updated attachment for the new generator will be forwarded to you once received, as I requested an updated copy this morning. Ben Clayton was aware of and approved the emergency purchase of the replacement generator as outlined in the cost breakdown included in this email.

The motion will need to include the cost for both the rental unit and the purchase of the new generator. If the lead time extends beyond the estimate, we will need to return to Council for additional funding at that time. Hopefully, this will not be necessary and the unit will arrive ahead of schedule.

Please note per Dan that the remaining cost difference of \$21,922.39 will be funded utilizing Act 179 funds. You may also consider requesting approval for \$25,166, as this would cover one additional month of rental costs should the lead time extend, ensuring appropriate approval is already in place.

Please note that all approvals requested will be for American Generators Sales & Service LLC.

Please respond to all and confirm that you have received this email and will add the required motion(s) to the 4/7/2026 Council agenda.

Thank you!

Respectfully,
Guido Ulin
Director, Parks & Recreation & Building & Grounds
City of Taylor
gulin@cityoftaylormi.gov | 734-374-3908



Please note that we have recently changed email domains from @ci.taylor.mi.us to @cityoftaylormi.gov.

Confidential: This message and all contents contain information from the City of Taylor which may be privileged, confidential or otherwise protected from disclosure under applicable law. The information is intended to be for the addressee only. If you are not the addressee, or if you have received this

message in error, any disclosure, copying, distribution or use of the contents of this message is prohibited. If you have received this message in error, please notify us immediately by reply email or telephone 734-287-6550 and destroy the original message and all attachments without retaining any copies.

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Date Thu 3/19/2026 8:36 AM

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We will need to seek Council approval to make the rental payments shown in the attachment included with this email, as well as approval for the purchase of the new unit that has already been ordered. An updated attachment for the new generator will be forwarded to you once received, as I requested an updated copy this morning. Ben Clayton was aware of and approved the emergency purchase of the replacement generator as outlined in the cost breakdown included in this email.

The motion will need to include the cost for both the rental unit and the purchase of the new generator. If the lead time extends beyond the estimate, we will need to return to Council for additional funding at that time. Hopefully, this will not be necessary and the unit will arrive ahead of schedule.

Please note per Dan that the remaining cost difference of \$21,922.39 will be funded utilizing Act 179 funds. You may also consider requesting approval for \$25,166, as this would cover one additional month of rental costs should the lead time extend, ensuring appropriate approval is already in place.

Please note that all approvals requested will be for American Generators Sales & Service LLC.

Please respond to all and confirm that you have received this email and will add the required motion(s) to the 4/7/2026 Council agenda.

Thank you!

Respectfully,
Guido Ulin
Director, Parks & Recreation & Building & Grounds
City of Taylor
gulin@cityoftaylormi.gov | 734-374-3908



Please note that we have recently changed email domains from @ci.taylor.mi.us to @cityoftaylormi.gov.

Confidential: This message and all contents contain information from the City of Taylor which may be privileged, confidential or otherwise protected from disclosure under applicable law. The information is intended to be for the addressee only. If you are not the addressee, or if you have received this

message in error, any disclosure, copying, distribution or use of the contents of this message is prohibited. If you have received this message in error, please notify us immediately by reply email or telephone 734-287-6550 and destroy the original message and all attachments without retaining any copies.

Animal Shelter Generator – Settlement Summary, Funding Breakdown, and Council Approval Needed

From Guido Ulin <gulin@cityoftaylor.mi.gov>

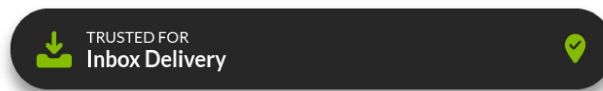
Date Thu 3/19/2026 8:36 AM

To Ralph Richard <rrichard2@cityoftaylor.mi.gov>

Cc Dan Bzura <dbzura@cityoftaylor.mi.gov>; Kasey Sandifur <ksandifur@cityoftaylor.mi.gov>

 2 attachments (124 KB)

2026-01-29 City of Taylor Animal Shelter Generator Rental Estimate.pdf; 2026-03-06 SETTLEMENT CHECK 367993094 IN THE AMOUNT OF \$68,209.21.pdf;



Good morning Ralph,

I am sending this email regarding the next steps required for the generator at the Animal Shelter, including the costs associated with the rental of the temporary unit, the purchase of the replacement unit, and the need for you to take these items forward for Council approval at the 4/7/2026 meeting.

Below is a summary of the steps that have been taken to date, as well as those still required.

We have received the settlement check from SKS Electric's insurance provider in the amount of \$68,209.21. The breakdown of the settlement is as follows:

Rental Unit Cost / Payment Details

- Delivery and first month: \$6,296.00 (paid by SKS/Insurance)
- Pick-up fee: \$1,150.00 (paid by SKS/Insurance)
- Lead time for new unit: 20–25 weeks
- Payment for 5 months @ \$3,243 per month = \$16,215.00 (paid to the City by SKS/Insurance)
- Total rental payments by SKS/Insurance: \$23,661.00

We understand there is a multiplier if the unit runs more than eight hours in a month. Under this proposal, the City will assume any additional cost if that occurs.

New Unit Cost

- SKS/Insurance pays: \$44,548.21 (estimated repair cost)
- City of Taylor pays: \$21,922.39 (difference between repair vs. replacement cost)
- Total cost for the new unit: \$66,470.60

Total amount received by the City: \$68,209.21 (final capped settlement amount).

The City of Taylor acknowledges that if the temporary unit runs more than eight hours in any month, we will assume the additional charges. Additionally, if the lead time for the new generator exceeds five months, the City will be responsible for any rental costs beyond the initial five-month period.

We will need to seek Council approval to make the rental payments shown in the attachment included with this email, as well as approval for the purchase of the new unit that has already been ordered. An updated attachment for the new generator will be forwarded to you once received, as I requested an updated copy this morning. Ben Clayton was aware of and approved the emergency purchase of the replacement generator as outlined in the cost breakdown included in this email.

The motion will need to include the cost for both the rental unit and the purchase of the new generator. If the lead time extends beyond the estimate, we will need to return to Council for additional funding at that time. Hopefully, this will not be necessary and the unit will arrive ahead of schedule.

Please note per Dan that the remaining cost difference of \$21,922.39 will be funded utilizing Act 179 funds. You may also consider requesting approval for \$25,166, as this would cover one additional month of rental costs should the lead time extend, ensuring appropriate approval is already in place.

Please note that all approvals requested will be for American Generators Sales & Service LLC.

Please respond to all and confirm that you have received this email and will add the required motion(s) to the 4/7/2026 Council agenda.

Thank you!

Respectfully,
Guido Ulin
Director, Parks & Recreation & Building & Grounds
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GRETCHEN WHITMER
GOVERNOR

STATE OF MICHIGAN
DEPARTMENT OF TRANSPORTATION
LANSING

BRADLEY C. WIEFERICH, P.E.
DIRECTOR

January 6, 2026

Dear Street Administrator:

Enclosed is the annual Act 51 maps, instruction booklet and forms for Act 51 Mileage Certification period (July 1, 2026, to June 30, 2027) representing the calendar year of 2025.

Please carefully review the streets represented on the certified Act 51 map and ensure they continue to meet all Act 51 street funding requirements.

Please identify any street/s on the map that is no longer open to public traffic, any requested reclassification changes to local and/or major streets and any requested mileage certification/decertification. All resolution requirements, instructions and sample resolutions are in the Mileage Certification for Cities and Villages booklet (enclosed).

Please sign the red stamped "State Copy" map in the designated area and return, along with any requested mileage changes by April 20, 2026. See page 6 of the instruction booklet for alternate ways to return signed map. The second map is for your records.

Failure to return the signed map will result in your agency being placed in noncompliance, which would result in the withholding of Act 51 distributions.

Michigan Department of Transportation
Data Inventory and Integration Division
Attn: Kelly Bolt B340
425 W. Ottawa
P.O. Box 30050
Lansing, MI 48909

All financial based inquiries should be directed to the Financial Operations Division at 517-643-2754. If you have any questions or require assistance with the street certification process, please contact me at boltk1@michigan.gov or 517-256-9372

Sincerely,

Kelly Bolt
Act 51 Certification Specialist
Road Network Unit
Geospatial Management and Reporting Section
Michigan Department of Transportation

cc: Wendi Burton

MICHIGAN DEPARTMENT OF TRANSPORTATION
Bureau of Transportation Planning

Instructions and Forms for Preparing:

Annual Road

MILEAGE CERTIFICATION FOR CITIES AND VILLAGES

In accordance with Public Act 51 of 1951, as amended

STATE TRANSPORTATION COMMISSION

Michael D. Hayes - Chair
Heath E. Salisbury - Vice Chair
Rita Brown
Suzanne Schultz
Richard Turner
Rhonda Welburn

Bradley Wiefelich Director

Please discard previous editions

PREFACE

Public Act 51 of 1951, as amended, requires that each incorporated city and village sign and submit the annual map and requested changes to the Data Inventory and Integration Division, Michigan Department of Transportation. This submittal covers the period of January 1 through December 31 and must show mileage certified as of December 31, 2025.

Section 13(7) of Public Act 51, of 1951 requires the governing body of each municipality to appoint a street administrator “who shall be responsible for and shall represent the municipality in transactions with the state transportation department pursuant to this act.” It is the duty of the street administrator to prepare this report or to have it prepared by those responsible and ensure that the reports are submitted by **April 20, 2026** to:

Michigan Department of Transportation
Data Inventory & Integration Division
Attn: Kelly Bolt: B340
425 W. Ottawa
P.O. Box 30050
Lansing, MI 48909

All reports are subject to review by the Michigan Department of Transportation and correction by the submitting agency before final approval by the Michigan Department of Transportation.

FAILURE TO RETURN THE REQUIRED MATERIAL IS CONSIDERED CAUSE FOR NON-COMPLIANCE AND WITHHOLDING OF MICHIGAN TRANSPORTATION FUNDS.

If you have any problems or questions in the preparation of your report, please contact Kelly Bolt at 517-256-9372 or at boltk1@michigan.gov.

As part of MDOT’s efforts to increase transparency with our partners and the public and to increase general availability of Act 51 mileage certification maps, a public-facing website will contain the most recent Act 51 mileage certification maps and other information pertaining to Act 51 is available at: <http://michigan.gov/act51maps>

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FORM CHECK LIST

If no changes to the city/village road mileages are being requested:

RETURN:

- Act 51 Street System Map reviewed, signed and dated by Street Administrator

If requesting changes to the city/village mileages:

RETURN:

- Act 51 Street System Map reviewed, signed and dated by Street Administrator
- Form 2008B: Additions and Deletions to City or Village Local Street System
- Form 2008A: Request for Change in the Major Street System (only if requesting a reclassification)
- Resolution submitted by the governing body
- Center line description of requested street(s), plat, or construction plans

STREET SYSTEM MAP

It is required to return the Act 51 Street System map signed and dated by the street administrator by April 20, 2026. Edit the map to show the following:

A. Certification/Addition of Streets

Streets may be certified/added to a city/village certified street system only when a street is: under the jurisdiction of the municipality, accepted into the city/village street system for public street purposes and open 24/7 to automobile traffic on or before December 31, 2025, and accompanied by an on the ground centerline measurement of said street(s). All resolution requirements are listed on page 5. Any new street, local or major, must be accompanied with a resolution signed by the governing body. **All streets added must be contained within or along corporate limits.** Alleys, private roads and any form of restricted road, are not permitted to be certified. List requested mileage changes on form 2008B: Additions and Deletions to City or Village Local Street System, page 13. For the required resolution documentation refer to page 5, for a sample resolution refer to page 9. Local streets to be added need to be drawn on the map and highlighted in yellow. Major street to be added or requested for reclassification to major, should be highlighted in green. For major street requirements refer to page 6. The current certified mileage totals can be found in the block marked APPROVED on the Act 51 map. Any disagreement of the current certified mileage total, differences must be supported by a street inventory.

B. Decertification/Vacation of Streets

Decertification/Vacation must be made of any streets which are no longer open to automobile traffic, have been abandoned, privatized, vacated, or not under the legal jurisdiction of the municipality. Show all mileage requests for local streets on form 2008B: Additions and Deletions to City or Village Local Street System, page 13, or for major streets on form 2008A: Request for Change in the Major Street System, page 15. For the required documentation refer to page 5. Please use red to cross out streets to be decertified. Decertification of local or major streets must be accompanied by a resolution signed by the governing body; see sample resolution for decertification/vacation on page 10.

C. Corporate Limits

All annexations must be filed with the Secretary of State. See the Michigan Department of State Information Sheet on pages 11-12 for the appropriate documentation that must be submitted. Show any changes in your corporate limits directly on your map and accompanied by a copy of the approval letter from the Office of the Great Seal acknowledging the annexation or relinquishment of territory.

D. Street Names

Please review the map and note any street name changes on it. Any significant street name changes must be supported by a resolution. The source of the street name change must be the local road naming authority.

There may be more than one recognized name for a street. If the United States Postal Service (USPS) is using a name that is different from the official name, you may add it to the map with "(USPS)" in parenthesis after the name. Postmasters are directed by the USPS to work with local naming authorities to resolve road name differences.

F. Signature

The red certification stamp on the map **must be signed and dated** by the Street Administrator and returned to the address located on the first page of the instruction booklet by **April 20, 2026**.

**REQUIREMENTS FOR
ADDITIONS AND DELETIONS
TO CITY OR VILLAGE LOCAL STREET SYSTEM**

All certifications (additions) and/or decertification (deletions) to the local street system, must be shown on the map in the appropriate color, must be listed on form 2008B and must also have a resolution from the governing body with the requirements listed below. A sample resolution for a street certification and decertification is contained in the Appendix on pages 9 and 10. A final plat approval is not a substitute for a resolution of acceptance. A resolution is not allowed to have any contingencies or it will not be accepted. Alleys and private roads are NOT permitted to be certified under Act 51.

The following is **required** information to certify a street addition:

- A. Form 2008B, Additions and Deletions to City or Village Local Street System. This form should include the name of the street(s), the Termini ("From" and "To") and the length given in feet. Electronic forms can be found at: <http://mdotjboss.state.mi.us/webforms/WebFormsHome.htm>
- B. A centerline description that is tied to the section corner, copy of construction plans, formal survey drawings as an attachment, would be preferred.

Centerline descriptions are required to **measure directly down the center of said street**. A boundary description of the property or a right of way description is NOT equivalent to a centerline description and will not be accepted. Ariel measurements are also not accepted for Act 51.

Centerline measurements are measured one of the following ways:

- beginning from the center of the termini street to the center of intersecting termini street.
 - streets with a cul-de-sac or round-about are measured straight through to the end of pavement and NOT around the circumference of the cul-de-sac or round-about.
- C. Addition resolution requirements: A resolution approved by the governing body with the following statements are required for all street certifications. Example on page 9.
 - Acceptance of centerline description or equivalent (see B.)
 - Acknowledgment that said street is located within city/village right-of-way and is under municipal control.
 - Acknowledgement that the street is a public street and is for public street purposes.
 - Acknowledgment that the street has been accepted into the municipal street system and is open to the public for public use on (specific date) or before December 31, 2025.

The following is **required** information for street decertification:

- A. Form 2008B, Additions and Deletions to City or Village Local Street System, page 13. This form should include the name of the street(s), the Termini ("From" and "To") and the length in feet. Electronic forms can be found at: <http://mdotjboss.state.mi.us/webforms/WebFormsHome.htm>
- B. Resolution of Decertification from governing body, example on page 10.

Please use red pen to XXX out street(s) designated for decertification.

JURISDICTIONAL TRANSFERS

The following documents must be received to the Michigan Department of Transportation to accept a Jurisdiction transfer as a result of a street transfer:

- A resolution from the receiving governing body accepting jurisdiction.
- A resolution from the relinquishing governing body resigning jurisdiction.

The street must be contained within or along corporate limits and both resolutions must be submitted in order to complete transfer. Jurisdictional transfers can be processed monthly.

OPTIONAL ELECTRONIC SIGNING AND SUBMITTAL OF MAP

The Michigan Department of Transportation is now allowing electronic signing of the Act 51 Mileage Certification maps. The State of Michigan is now requiring electronic signatures through OneSpan (DocuSign/Cosign is no longer allowed). No account/signing up/password is necessary with the new OneSpan software.

Signing of the annual Act 51 map is required for Act 51 mileage certification, now both the paper signed version and electronic signed version is acceptable. Only one form of the signed map needs to be returned by the April 20, 2026 due date.

If the city/village is interested in signing the Act 51 map electronically, please email Kelly Bolt at Boltk1@michigan.gov. An email will be sent to the authorized Street Administrator, in it will contain an electronic link to OneSpan, the map will open with a signature box to electronically sign and resubmit for final certification.

If the city/village is interested in sending street mileage change documents electronically, this process utilizes MDOT's MILogin/e-Prop and would require setting up an account and password. Please email Kelly Bolt at Boltk1@michigan.gov for those specific instructions.

REQUEST FOR CHANGE IN THE MAJOR STREET SYSTEM

If certain roads in the local street system have are/become of sufficient importance to be reclassified as major streets, a request must be submitted using form 2008A, page 15, electronic forms can be found at: <http://mdotboss.state.mi.us/webforms/WebFormsHome.htm> This form must be filled completely including the justification specified for each requested reclassification of a major street.

Major streets are those city and village streets of the greatest general importance to the municipality. Streets requested for reclassification must be in conformance with the *Uniform Criteria for Major Streets*. In the interest of facilitating current traffic demands, cities and villages may request a revision to the classification of their major street system. Each request is reviewed by staff from the department's Data Inventory and Integration Division. The approval for the addition of a major street will be based upon the unique characteristics of each city/village with respect to relative traffic volumes, area service, and conformance with the criteria contained in the *Uniform Criteria for Major Streets*.

The *Uniform Criteria for Major Streets* publication contains basic standards for a street to be classified as a Major Street, as agreed upon by MDOT and the Michigan Municipal League (MML) and approved by the State Transportation Commission.

This office will make any adjustments to the major/local street mileage as a result of classification changes. The guidelines for the selection of major streets are located on page 1 of the *Uniform Criteria for Major Streets* handbook.

New streets may enter your street system as major streets, but first must meet all street certification and resolution requirements before they can be classified as major, page 5.

A copy of the *Uniform Criteria for Major Streets* booklet is available upon request by contacting our unit staff at 517-256-9372 or by e-mail at boltkl@michigan.gov.

REQUEST FOR CHANGE IN STREET ADMINISTRATOR

Any changes of the Street Administrator should be made in the ADARS system: www.michigan.gov/act51.

APPENDIX

**SAMPLE RESOLUTION
FOR
STREET ADD
CERTIFICATION**

At a regular meeting of the City/Village Council of _____, Michigan, held at the City/Village Hall on _____,

the following resolution was offered by member _____, and supported by member _____.

Whereas the City/Village of _____ did on _____ Date _____ acquire title to _____ Street.

And whereas it is necessary to furnish certain information to the State of Michigan to place this street within the City/Village Street System for the purpose of obtaining funds under Act 51, P.A. 1951 as amended.

NOW THEREFORE IT IS RESOLVED:

1. That the center line of said street is described as:

SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE

Commencing at the North 1/8th post of the Northeast Quarter of Section 14, Town 1 South, Range 2 West, thence along the North section line East 563 feet, thence South 468.74 feet to the Point of Beginning of said street, thence North 88°-32'-00" East 303.31 feet along the center line to the easterly end of said street.

SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE SAMPLE

2. That said street is located within a City/Village right-of-way and is under the control of the City/Village of _____.

3. That said street is a public street and is for public street purposes.

4. That said street is accepted into the municipal street system and is open to the public for public use on (specific date) or before December 31, 2025

Resolution duly adopted. (Date)

(City/Village Clerk)

Certified to be a true copy, _____.

(Date)

(City/Village Clerk)

***SAMPLE RESOLUTION FOR
DECERTIFICATION/VACATION***

NOW THEREFORE IT IS RESOLVED:

At a regular meeting of the City/Village Council of _____, Michigan, held at the city/village hall on _____, the following resolution was offered by member _____, and supported by member _____.

Whereas the City/Village of _____ does wish to decertify/vacate a portion of _____ Street. This decertification/vacation of _____ Street is located between _____ Street and _____ Street for a total decertification/vacation length of _____ feet.

Resolution duly adopted.

(Date) _____

(City/Village Clerk) _____

Certified to be a true copy, _____

(Date) _____

(City, Village Clerk) _____

MICHIGAN DEPARTMENT OF STATE INFORMATION SHEET

FINANCIAL OUTREACH – ACT 51

Laura Loomis: 517-643-2754
LoomisL2@michigan.gov
Michigan Department of Transportation
Financial Outreach
425 W. Ottawa
P.O. Box 30050
Lansing MI 48909

MICHIGAN DEPARTMENT OF STATE

Secretary of State
430 W. Allegan
Lansing, MI 48918

ANNEXATION DOCUMENTS TO BE FILED WITH THE SECRETARY OF STATE

OFFICE OF THE GREAT SEAL

110 W. Michigan Avenue
Ste. 100
Lansing, MI 48918-1750
517-373-2531

GENERAL LAW VILLAGES - Act 3, PA 1895, as amended, MCL 61.1 - 74.22

1. Copy of Property Owner Petition containing property description;
2. Copy of Village Petition and/or Resolution to County containing property description;
3. Copy of County Board of Commissioners Order containing property description.

HOME RULE VILLAGES - Act 278, PA 1909, as amended, MCL 78.1 - 78.28

ANNEXATION BY ELECTION

1. Copy of Petition containing property description;
2. Copy of Every Resolution, Affidavit or Certificate required;
3. Board of Canvassers Certificate.

ANNEXATION BY MUTUAL CONSENT

1. Copy of Petition containing property description;
2. Copy of Village Resolution containing property description;
3. Copy of Resolution of Adjoining City, Village or Township containing property description.

CITIES - Act 279, PA 1909, as amended, MCL 117.1 - 117.38

ANNEXATION BY MUTUAL CONSENT

1. Copy of Property Owner Petition containing property description;
2. Copy of City Resolution containing property description;
3. Copy of Resolution of Adjoining City, Village or Township containing property description.

ANNEXATION OF ADJACENT TERRITORY, OWNED BY THE CITY ANNEXING THE PROPERTY OR PROPERTY WHICH CONSISTS OF FRACTIONAL PLATTED SUBDIVISION LOTS LOCATED IN AN ADJOINING CITY, VILLAGE OR TOWNSHIP

1. Copy of City Resolution containing property description;
2. Copy of Resolution of Adjoining City, Village or Township containing property description.

ANNEXATION OF ADJACENT PARK OR VACANT PROPERTY IN A TOWNSHIP, OWNED BY THE CITY ANNEXING THE PROPERTY, AND THERE IS NO ONE RESIDING THEREON

Copy of City Resolution containing property description.

STATE BOUNDARY COMMISSION - Act 191, PA 1968, as amended, MCL 123.1001-123.1020

Documents are filed by the State Boundary Commission.

THE LAND TRANSFER ACT, P.A. 425 OF 1984

Copy of Agreement between Township and City or Village containing property description

FREQUENTLY ASKED QUESTIONS

When should the City/Village Act 51 signed map be submitted?

The current certified map is distributed at the beginning of January and due back to MDOT, signed and with any requested mileage changes, by April 20, 2026.

Who needs to sign and date the map?

Maps need to be signed and dated by the appointed Street Administrator of the City or Village.

Is there anything else that needs to be completed/checked before signing and submitting the map?

It is the responsibility of the Street Administrator to carefully review the streets represented on the certified Act 51 map. Streets need to be open to public traffic and continue to meet all Act 51 street funding requirements.

Is signing the map required every year if there are no mileage changes?

Yes, the Act 51 road mileage total is certified every year even when no mileage changes are requested.

When is a resolution needed?

A resolution is required for all road mileage certification (addition) and decertification (deletions) to the certified road mileage total.

When do forms 2008A or 2008B need to be submitted?

Only when mileage changes are requested. Complete and submit form 2008A for major roads or 2008B for local roads.

Is listing the name of the road enough information for a resolution?

No, see page 5 for specific language required for resolution. A sample resolution for mileage certification is located on page 9. Sample resolution for mileage decertification is located on page 10.

Is a resolution needed to request a classification change?

No, a resolution is not required when making a request for reclassification. Reclassification requests should be highlighted on the map in green and form 2008A and/or 2008B should be completed, paying special attention to the Justification section.

**RESOLUTION FOR
CHANGING A LOCAL STREET NAME
IN THE CITY OF TAYLOR'S ACT 51 LOCAL STREET SYSTEM**

AT A REGULAR MEETING OF THE CITY COUNCIL OF TAYLOR, MICHIGAN, HELD AT CITY HALL ON APRIL 7, 2026, THE FOLLOWING RESOLUTION WAS OFFERED BY CITY COUNCIL MEMBER _____, AND SUPPORTED BY CITY COUNCIL MEMBER _____.

WHEREAS IT IS NECESSARY TO FURNISH CERTAIN INFORMATION TO THE STATE OF MICHIGAN WHEN LOCAL STREET NAMES ARE CHANGED WITHIN THE CITY STREET SYSTEM FOR THE PURPOSE OF OBTAINING FUNDS UNDER ACT 51, P.A. 1951 AS AMENDED.

NOW THEREFORE THE FOLLOWING BE RESOLVED BY ALTERING/DELETED FROM THE CITY STREET SYSTEM:

1. GREENSWAY DR FROM RACHO RD EAST 984 ' TO DEAD END.
2. KATHERINE AVE FROM PENNSYLVANIA SERVICE DR NORTH 220' TO DEAD END.
3. DUDLEY AVE FROM PENNSYLVANIA SERVICE DRIVE NORTH 250' TO DEAD END.
4. WEST POINT AVE FROM EUREKA RD NORTH 153' TO DEAD END.
5. PINE AVE FROM EUREKA RD NORTH 308' TO DEAD END.
6. PINE AVE FROM EUREKA RD SOUTH 510' TO DEAD END.
7. BURR AVE FROM VIRGINIA AVE NORTH 270' TO DEAD END.
8. COOPER FROM VIRGINIA AVE SOUTH 190' TO DEAD END.
9. ORCHARD AVE FROM OLDHAM AVE EAST 40' TO DEAD END.
10. LEO ST FROM WEDDEL AVE WEST 130' TO DEAD END.
11. HASKELL AVE FROM MORTENVIEW DR WEST 190' TO DEAD END.
12. BUTZEL ST FROM BANNER AVE EAST 500' TO DEAD END.
13. BUCK AVE FROM ECORSE RD NORTH 150' TO DEAD.
14. WESTLAKE ST FROM CHARLES AVE NORTH 150' TO DEAD END.
15. CYPRESS AVE FROM TROY AVE WEST 95' TO DEAD END.
16. NEWCASTLE AVE FROM WEST POINT AVE WEST 135' TO DEAD END.

I HEREBY CERTIFY that the foregoing is a true and correct copy of a resolution adopted by the City Council of the City of Taylor, County of Wayne, Michigan, on April 8, 2026.

Cynthia A. Bower, City Clerk
City of Taylor
Wayne County, Michigan

Vendor	Amount 2025-2026							
Gas and Oil	Council Approved Amount	Increase to PA Amount	Fund	Department	Account	Sub Account	Program	Company
Eastern Oil Company	\$ 20,000.00		101	530	751	0.00	0.00	0.00
Linde Gas & Equipment Inc	\$ 10,000.00		101	530	751	0.00	0.00	0.00
RKA Petroleum	\$ 200,000.00		101	530	751	0.00	0.00	0.00
		\$ -	\$430,000.00	Budget Approved				
Operating Supplies								
1st AYD	\$ 2,500.00		101	530	761	0.00	0.00	0.00
Grainger	\$ 3,000.00		101	530	761	0.00	0.00	0.00
Home Depot Credit Services	\$ 2,500.00		101	530	761	0.00	0.00	0.00
Hutson	\$ 2,500.00		101	530	761	0.00	0.00	0.00
Lawson Products	\$ 7,000.00		101	530	761	0.00	0.00	0.00
Macqueen	\$ 7,000.00		101	530	761	0.00	0.00	0.00
Safety Kleen	\$ 2,500.00		101	530	761	0.00	0.00	0.00
Security ACE Hardware	\$ 2,500.00		101	530	761	0.00	0.00	0.00
Zep	\$ 3,000.00		101	530	761	0.00	0.00	0.00
Zips	\$ 2,500.00		101	530	761	0.00	0.00	0.00
		\$ -	\$30,000.00	Budget Approved				
Uniforms								
Unifirst Corp	\$ 10,000.00		101	530	767	0.00	0.00	0.00
		\$ -	\$6,000.00	Budget Approved				
Repair & Maintenance								
Allied Inc	\$ 6,000.00	\$ 15,000.00	101	530	931	0.00	0.00	0.00
Auto Value	\$ 30,000.00		101	530	931	0.00	0.00	0.00
Belle Tire	\$ 40,000.00		101	530	931	0.00	0.00	0.00
Harmon Glass	\$ 4,000.00		101	530	931	0.00	0.00	0.00
MacQueen	\$ 2,000.00		101	530	931	0.00	0.00	0.00
Metro Airport Truck	\$ 15,000.00	\$ 25,000.00	101	530	931	0.00	0.00	0.00
Michigan CAT	\$ 5,000.00		101	530	931	0.00	0.00	0.00
MTech Company	\$ 20,000.00		101	530	931	0.00	0.00	0.00
NAPA Genuine Auto Parts	\$ 15,000.00	\$ 25,000.00	101	530	931	0.00	0.00	0.00
Pace Transportation Service	\$ 5,000.00		101	530	931	0.00	0.00	0.00
Pomp's Tire Service Inc	\$ 10,000.00		101	530	931	0.00	0.00	0.00
Smiley Tire & Retreading Co	\$ 25,000.00		101	530	931	0.00	0.00	0.00
Southgate Ford	\$ 10,000.00		101	530	931	0.00	0.00	0.00
Southpointe Radiator	\$ 5,000.00		101	530	931	0.00	0.00	0.00
Sparks Tire	\$ 4,000.00		101	530	931	0.00	0.00	0.00
Taylor Chevrolet Inc	\$ 35,000.00		101	530	931	0.00	0.00	0.00
Truck & Trailer	\$ 10,000.00		101	530	931	0.00	0.00	0.00
Van Buren Steel	\$ 2,500.00		101	530	931	0.00	0.00	0.00
Wolverine Truck Sales	\$ 14,000.00		101	530	931	0.00	0.00	0.00
			\$180,000.00	Budget Approved				



City of Taylor 2024 Transportation

ASSET MANAGEMENT PLAN

Prepared by:
Brian Frisk
Wade Trim Associates, Inc.
25251 Northline Road
Taylor, MI 48180
734-947-9700

January 13, 2025



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EXECUTIVE SUMMARY

As conduits for commerce and connections to vital services, roads and bridges are some of the most important assets in any community, and other assets like culverts, traffic signs, traffic signals, and utilities support and affect roads and bridges. The City of Taylor's (Taylor) roads, bridges, and support systems are also some of the most valuable and extensive public assets, all of which are paid for with taxes collected from ordinary citizens and businesses. The cost of building and maintaining these assets, their importance to society, and the investment made by taxpayers all place a high level of responsibility on local agencies to plan, build, and maintain roads, bridges, and support assets in an efficient and effective manner. This asset management plan is intended to report on how Taylor is meeting its obligations to maintain the public assets for which it is responsible.

This plan identifies Taylor's assets and condition and how Taylor maintains and plans to improve the overall condition of those assets. An Asset Management Plan (AMP) is required by Michigan Public Act 325 of 2018, and this document represents fulfillment of some of Taylor's obligations towards meeting these requirements. However, this plan and its supporting documents are intended to be much more than a fulfillment of required reporting. This asset management plan helps to demonstrate Taylor's responsible use of public funds by providing elected and appointed officials as well as the general public with the inventory and condition information of Taylor's assets, and it gives taxpayers the information they need to make informed decisions about investing in Taylor's essential transportation infrastructure.

INTRODUCTION

Asset management is defined by Public Act 325 of 2018 as “an ongoing process of maintaining, preserving, upgrading, and operating physical assets cost effectively, based on a continuous physical inventory and condition assessment and investment to achieve established performance goals”. In other words, asset management is a process that uses data to manage and track assets, like roads and bridges, in a cost-effective manner using a combination of engineering and business principles. This process is endorsed by leaders in municipal planning and transportation infrastructure, including the Michigan Municipal League, County Road Association of Michigan, the Michigan Department of Transportation (MDOT), and the Federal Highway Administration (FHWA). The City of Taylor is supported in its use of asset management principles and processes by the Michigan Transportation Asset Management Council (TAMC), formed by the State of Michigan.

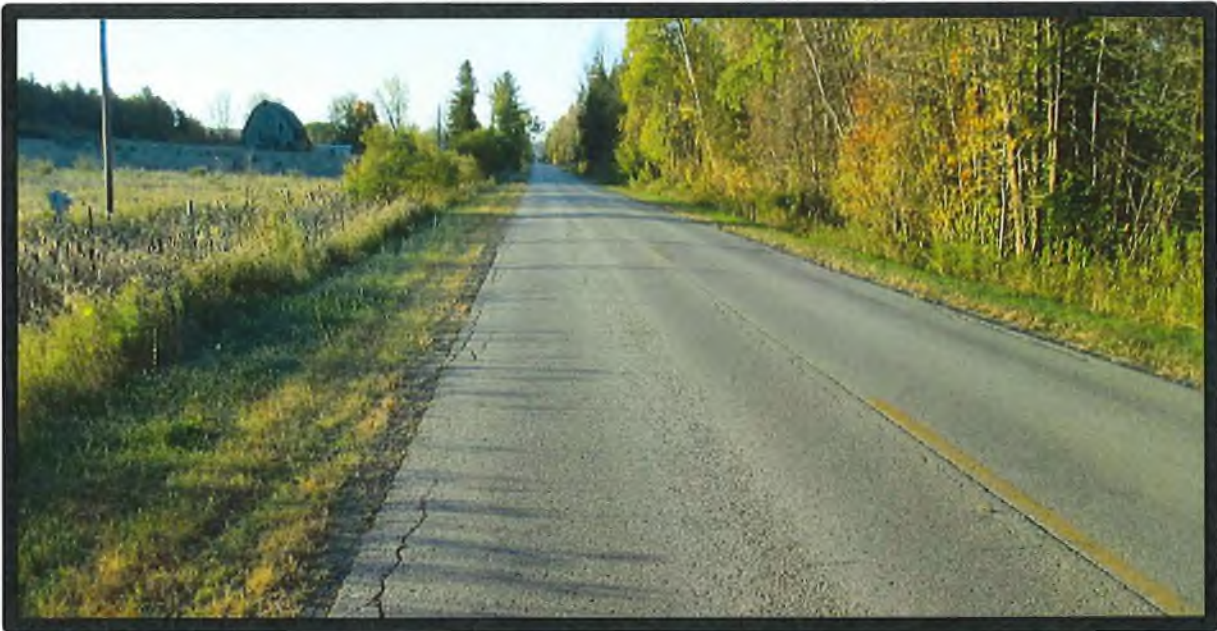
Asset management, in the context of this plan, ensures that public funds are spent as effectively as possible to maximize the condition of the road and bridge network. Asset management also provides a transparent decision-making process that allows the public to understand the technical and financial challenges of managing transportation infrastructure with a limited budget.

The City of Taylor (Taylor) has adopted an “asset management” business process to overcome the challenges presented by having limited financial, staffing, and other resources while needing to meet road users' expectations. Taylor is responsible for maintaining and operating over 195.287 centerline miles of roads and 28 bridge structures.

This 2024 plan identifies Taylor’s transportation assets and their condition as well as the strategy that Taylor uses to maintain and upgrade particular assets given Taylor’s condition goals, priorities of network’s road users, and resources. An updated plan is to be released approximately every three years both to comply with Public Act 325 and to reflect changes in road conditions, finances, and priorities.

Questions regarding the use or content of this plan should be directed to Chris Gibbs, PE, City Engineer at 25605 Goddard Road, Taylor, MI 48180 or at 734-287-6550 / cgibbs@ci.taylor.mi.us.

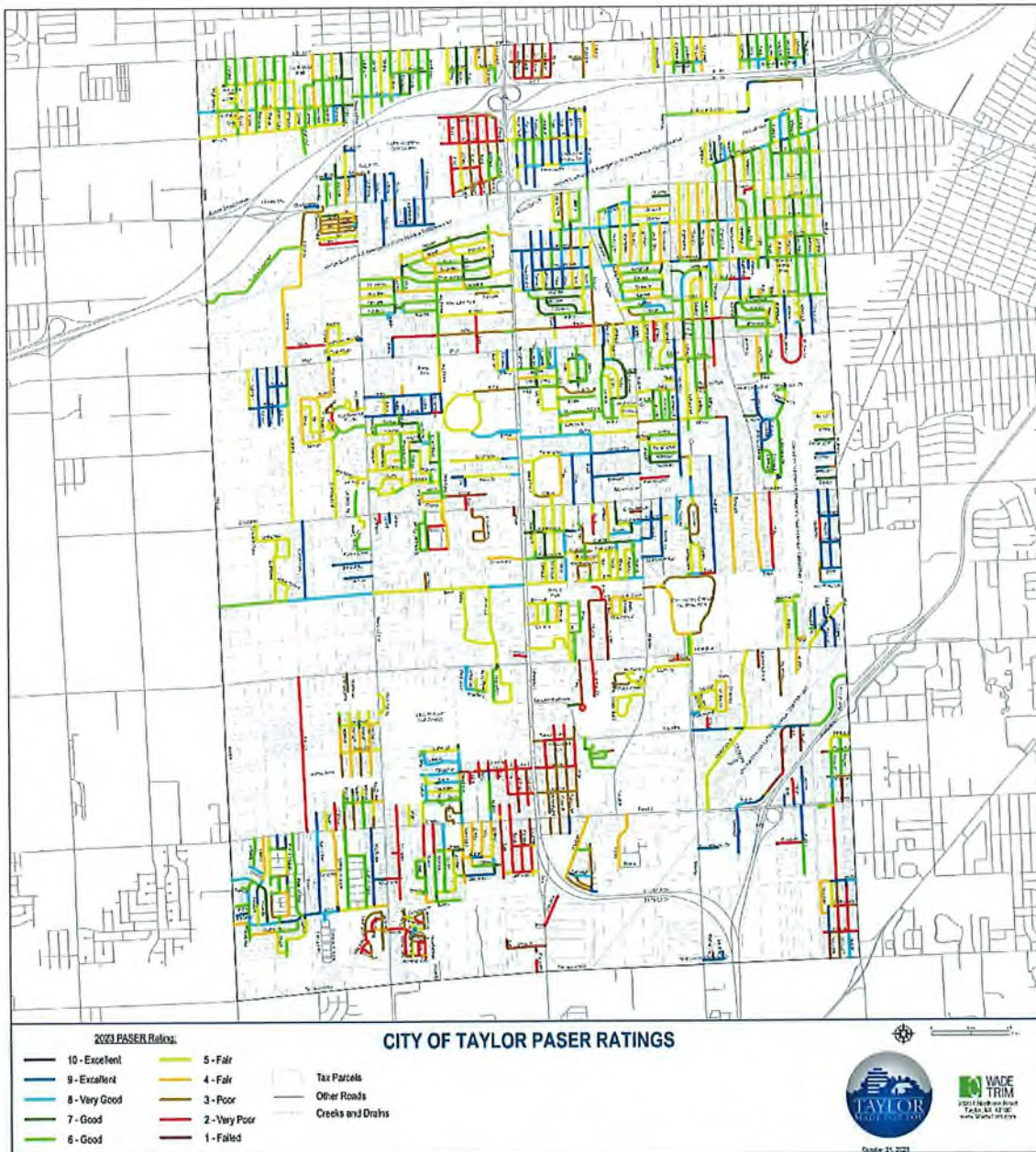
1.0 PAVEMENT ASSETS



Taylor is responsible for 195.287 centerline miles of public roads. An inventory of these miles divides them into different network classes based on road purpose/use and funding priorities as identified at the State level: City major road network, which is prioritized for state-level funding, and City local road network.

1.1 Inventory of Assets

Figure 1-1. Map showing Location of Roads Managed by Taylor and the Current Condition for Paved Roads



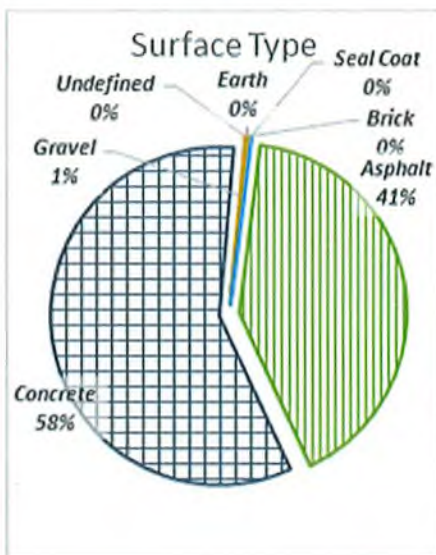
Of Taylor's 195.287 miles of road, 35.718 miles are classified as City Major and 159.569 miles are classified as City Local (Figure 1 identifies the 2023 PASER ratings for these roads). More detail

about these road assets can be found in Taylor's Roadsoft database or by contacting The City of Taylor.

1.2 Types

Taylor has multiple types of surface pavements in its jurisdiction, including primarily asphalt and concrete. A very minor percentage (1%) are identified as gravel roads. The remaining categories (undefined, earth, seal coat remain at 0%). **Figure 1-2** shows a breakdown of these surface pavement types for all of Taylor's road assets.

Figure 1-2. Pavement Type by Percentage Maintained by Taylor



1.3 Condition, Goals, and Trend

1.3.1 Paved Roads

Paved roads in Michigan are rated using the Pavement Surface Evaluation and Rating (PASER) system, which is a 1 to 10 scale with 10 being a newly constructed surface and 1 being a completely failed surface. PASER scores are grouped into TAMC definition categories of good (8-10), fair (5-7), and poor (1-4) categories. Taylor collects PASER data every two years on 100 percent of those portions of its City major and City local networks that are eligible for federal funding. In addition, Taylor uses its own staff and resources to collect PASER data on 100% percent of its City major and City local networks that are not eligible for federal funding.

Currently, the City major network has 24% of its roads in good condition, 45% in fair condition, and 31% in poor condition. The City local network has 20% of its roads in good condition, 53% in fair condition, and 27% in poor condition (Figure 3 and Figure 4). Taylor's long-range goal for the City major network is to increase the percentage of roads in good condition, maintain or reduce the

percentage in fair condition, and reduce the percentage of roads in poor condition. Taylor's long-range goal for the City local network is to increase the percentage of roads in good condition, maintain or reduce the percentage of roads in fair condition, and decrease the percentage of roads in poor condition (Figure 3 and Figure 4). Figure 3 and Figure 4 illustrate the historical and current condition (solid bars) of Taylor's City major and City local networks, respectively; they also illustrate the projected trend (shaded bars) and the overall trend in condition (trendlines).

Figure 1-3. City Major Network Condition, Goals, and Trend

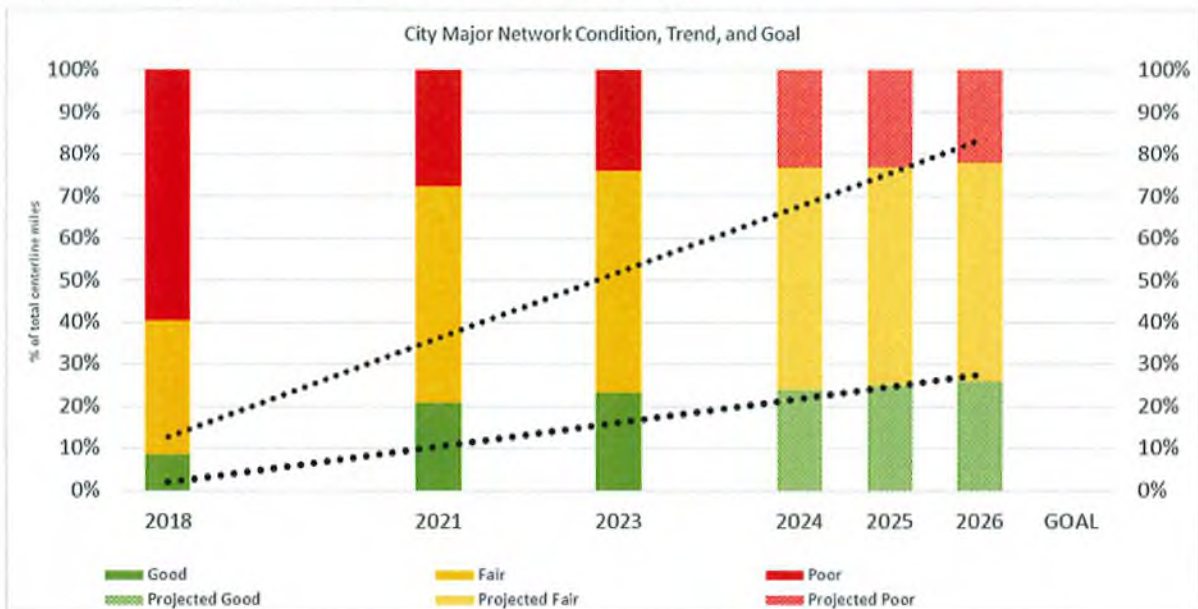
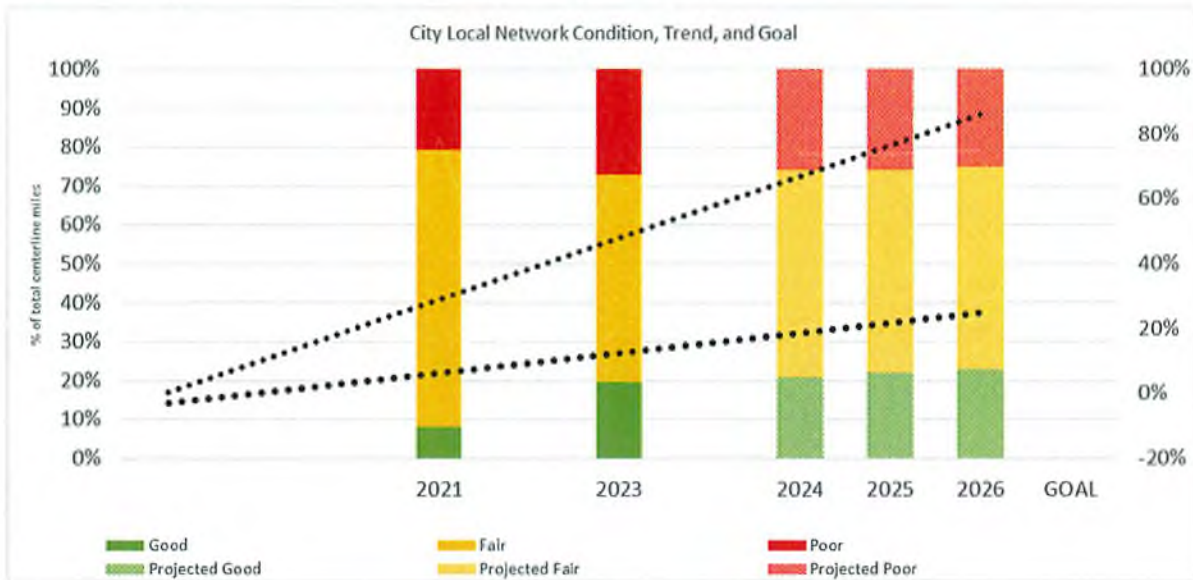


Figure 1-4. City Local Network Condition, Goals, and Trend



1.4 Modelled Trends, Gap Analysis, and Planned Projects

1.4.1 Modelled Trends & Gap Analysis

The Roadsoft network analysis of Taylor’s planned projects for the City major and City local networks from Taylor’s currently-available budget does allow Taylor to reach its pavement condition goals given the projects planned for the next three years. The list of proposed projects and their projected budgets are illustrated below.

Propose Project	Fiscal Year	Projected Budget (2024)
Non-Motorized	2027	\$200,000
Mortenview (Wick to Ecorse)	2026	\$800,000
Byers (Telegraph to Ecorse)	2026	\$400,000
Superior (Pardee to Racho)	2027	\$2,500,000
Westlake (Eureka to Dead End South)	2025	\$800,000
Lange (Eureka to Northline)	2026	\$800,000

2.0 BRIDGE ASSETS



Taylor is responsible for four bridges. The City conducts biennial inspections for these bridges.

2.1 Condition, Goals, and Trend

Bridges in Michigan are given a good, fair, or poor rating based on the National Bridge Inspection Standards (NBIS) rating scale, which was created by the Federal Highway Administration to evaluate a bridge's deficiencies and to ensure the safety of road users.

The goal of the program is the preservation and safety of Taylor's bridge network.

2.2 Taylor Bridge Responsibility

Taylor is responsible for four (4) bridges, whereby they remain compliant with biennial routine bridge safety inspections that are reportable to the Michigan Department of Transportation (MDOT). The bridge locations are as follows:

- Huron Street over Frank & Poet Drain, Identified as Structure No. 11635
- Mortenvue Drive over Sexton-Kilfoil Drain, Identified as Structure No. 12514
- Continental Drive over Sexton-Kilfoil Drain, Identified as Structure No. 12515
- Beech Daly Road over Blakely Drain, Structure No. 12978

2.3 Beech Daly Road over Blakely Drain, Identified as Structure No. 12978 Programmed/Funded Projects, Gap Analysis, and Planned Projects

Taylor uses local funds for fixes to be applied to bridges. At present, all of the bridges under Taylor's jurisdiction are in good condition and there are no fixes forecasted at this time. Preventative maintenance is a more effective use of these funds than the costly alternative of major rehabilitation or replacement. Since Taylor recognizes that limited funds are available for improving the bridge network, it seeks to identify those bridges that will benefit from a planned maintenance program. Taylor does not plan to replace any bridges within the next three years. By performing the preventative maintenance of bridge structures, Taylor should achieve its goal of keeping its overall bridge network at the same condition.

3.0 CULVERT ASSETS



3.1 Inventory of Assets

At present, Taylor does not track inventory data of its culvert assets

3.2 Goals

Taylor is responsible for preserving any and all culverts that underlie its entire road network.

3.3 Planned Projects

Taylor's policy is to replace or repair culvert assets concurrent with projects affecting road segments carried by the particular culverts.

4.0 SIGNAL ASSETS



4.1.1 Inventory of Assets

At present, Taylor tracks only inventory data for traffic signals.

More detail about these traffic signal assets can be obtained by contacting The City of Taylor.

4.1.2 Goals

The goal of Taylor's asset management program is the preservation of its traffic signals.

4.1.3 Planned Projects

Taylor does not have any planned projects for their signals.

5.0 FINANCIAL RESOURCES

Public entities must balance the quality and extent of services they can provide with the tax resources provided by citizens and businesses, all while maximizing how efficiently funds are used. Therefore, Taylor will overview its general expenditures and financial resources currently devoted to transportation infrastructure maintenance. This financial information is not intended to be a full financial disclosure or a formal report. Full details of Taylor's financial status can be found on our website at <https://www.cityoftaylor.com/177/Budgets-Financial-Reports>.

5.1 Anticipated Revenues & Expenses

Taylor receives funding from the following sources:

- **State funds** – Taylor's principal source of transportation funding is received from the Michigan Transportation Fund (MTF). This fund is supported by vehicle registration fees and the State's per-gallon gas tax. Allocations from the MTF are distributed to State and Local governmental units based on a legislated formula, which includes factors such as population, miles of certified roads, and vehicle registration fees for vehicles registered in the agency's jurisdiction. Examples of State grants also include local bridge grants, economic development funds, and metro funds.
- **Federal and State grants for individual projects** – These are typically competitive funding applications that are targeted at a specific project type to accomplish a specific purpose. These may include safety enhancement projects, economic development projects, or other targeted funding. Examples of federal funds include Surface Transportation Program (STP) funds, C and D funds, Bridge Funds, MDOT payments to private contractors, and negotiated contracts.
- **Local government entities or private developer contributions to construction projects for specific improvements** – This category includes funding received to mitigate the impact of commercial developments as a condition of construction of a specific development project, and can also include funding from a special assessment district levied by another governmental unit. Examples of contributions from local units include City, Village, and Township contributions to the County; special assessments; County appropriations; bond and note proceeds; contributions from Counties to Cities and Villages; City general fund transfers; City municipal street funds; capital improvement funds; and tax millages (see below).

Taylor is required to report transportation fund expenditures to the State of Michigan using a prescribed format with predefined expenditure categories. The definitions of these categories according to Public Act 51 of 1951 may differ from common pavement management nomenclature and practice. For the purposes of reporting under PA 51, the expenditure categories are:

- **Construction/Capacity Improvement Funds** – According to PA 51 of 1951, this financial classification of projects includes, "new construction of highways, roads, streets, or bridges, a project that increases the capacity of a highway facility to accommodate that part of traffic having

neither an origin nor destination within the local area, widening of a lane width or more, or adding turn lanes of more than 1/2 mile in length.”¹

- **Preservation and Structural Improvement Funds** – Preservation and structural improvements are “activit[ies] undertaken to preserve the integrity of the existing roadway system.”² Preservation includes items such as a reconstruction of an existing road or bridge, or adding structure to an existing road.
- **Routine and Preventive Maintenance Funds** – Routine maintenance activities are “actions performed on a regular or controllable basis or in response to uncontrollable events upon a highway, road, street, or bridge”.³ Preventive maintenance activities are “planned strategy[ies] of cost-effective treatments to an existing roadway system and its appurtenances that preserve assets by retarding deterioration and maintaining functional condition without significantly increasing structural capacity”.⁴
- **Winter Maintenance Funds** – Expenditures for snow and ice control.
- **Trunkline Maintenance Funds** – Expenditures spent under Taylor’s maintenance agreement with MDOT for maintenance it performs on MDOT trunkline routes.
- **Administrative Funds** – There are specific items that can and cannot be included in administrative expenditures as specified in PA 51 of 1951. The law also states that the amount of MTF revenues that are spent on administrative expenditures is limited to 10 percent of the annual MTF funds that are received.
- **Other Funds** – Expenditures for equipment, capital outlay, debt principal payment, interest expense, contributions to adjacent governmental units, principal, interest and bank fees, and miscellaneous for cities and villages.

Table 5-1 details the revenues and expenditures for Taylor.

¹ Public Act 51 of 1951, 247.660c Definitions

² Public Act 51 of 1951, 247.660c Definitions

³ Public Act 51 of 1951, 247.660c Definitions

⁴ Public Act 51 of 1951, 247.660c Definitions

Table 5-1. Annual Fiscal Year Revenues & Expenditures per Fiscal Year					
REVENUES			EXPENDITURES		
Item	Estimated \$	Percent of Total	Item	Estimated \$	Percent of Total
State funds	2,000,000	99.1	Construction & capacity improvement (CCI)	0	0
Federal funds	0	0.0	Preservation & structural improvement (PSI)	7,645,820	21.3
Contributions for local units	0	0.0	Routine maintenance	0	0
Interest, rents, and other	4500	0.2	Winter maintenance	510,000	5.7
Charges for services	0	0	Trunkline maintenance	0	0
			Administrative	0	0
			Other	821,200	9.1
TOTAL	2,022,500	100	TOTAL	44,885,100	335

Verify the information in this table. You can find your agency's information in the TAMC dashboard at <https://www.mcqi.state.mi.us/mitrp/tamcDashboards>.

6.0 RISK OF FAILURE ANALYSIS

Transportation infrastructure is designed to be resilient. The system of interconnecting roads and bridges maintained by Taylor provides road users with multiple alternate options in the event of an unplanned disruption of one part of the system. There are, however, key links in the transportation system that may cause significant inconvenience to users if they are unexpectedly closed to traffic. Key transportation links include:

- **Geographic divides:** Areas where a geographic feature (river, lake, hilly terrain, or limited access road) limits crossing points of the feature; bridge failures, in particular, can create loss of access to entire regions of the state
- **Emergency alternate routes for high-volume roads and bridges:** Roads and bridges that are routinely used as alternate routes for high-volume assets are included in an emergency response plan
- **Limited access areas:** Roads and bridges that serve remote or limited access areas that result in long detours if closed
- **Main access to key commercial districts:** Areas with a large concentration of businesses or where large-size business will be significantly impacted if a road is unavailable
Our road network does not have any identified critical assets.

7.0 COORDINATION WITH OTHER ENTITIES

An asset management plan provides a significant value for infrastructure owners because it serves as a platform to engage other infrastructure owners using the same shared right of way space. Taylor communicates with both public and private infrastructure owners to coordinate work in the following ways:

Taylor coordinates with multiple agencies that maintain drinking water, sanitary, and storm sewer assets in addition to transportation assets. Taylor follows an asset management process for all of its assets by coordinating the upgrade, maintenance, and operation of all major assets.

Taylor takes advantage of coordinated infrastructure work to reduce cost and maximize value using the following policies:

- Roads which are in poor condition that have a subsurface infrastructure project planned which will destroy more than half the lane width are typically rehabilitated or reconstructed full width using transportation funds to repair the balance of the road width.
- Subsurface infrastructure projects which will cause damage to pavements in good condition will be delayed if possible, or methods that do not require pavement cuts will be considered.
- Subsurface utility projects will be coordinated to allow all under pavement assets to be upgraded in the same project regardless of ownership.
- Road reconstruction projects will typically not be completed until agency owned sub surface utilities are upgraded to have at least a 40 years of remaining service life.

8. PROOF OF ACCEPTANCE

PUBLIC ACT 325



CERTIFICATION OF TRANSPORTATION ASSET MANAGEMENT PLAN

Certification Year: 2025

Local Road-owning Agency Name: CITY OF TAYLOR

Beginning October 2019 and on a three-year cycle thereafter, certification must be made for compliance to Public Act 325. A local road-owning agency with 100 certified miles or more must certify that it has developed an asset management plan for the road, bridge, culvert, and traffic signal assets. Signing this form certifies that the hitherto referred agency meets with minimum requirements as outlined by Public Act 325 and agency-defined goals and objectives.

This form must be signed by the chairperson of the local road-owning agency or the county executive and chief financial officer of the local road-owning agency.

Signature 	Signature 
Printed Name Douglas A. Geiss	Printed Name JASON COUTURE
Title Chairman	Title FINANCE DIRECTOR / CFO
Date 11/21/2025	Date 1/21/25

Due every three years based on agency submission schedule

Submittal Date: 01/24/2025

See attached council meeting minutes and/or resolution.

TIM WOOLLEY
Mayor

MICHELLE TOCCO
Treasurer

City of Taylor

OFFICE OF THE CITY CLERK

CYNTHIA A. BOWER

23555 GODDARD ROAD
TAYLOR, MICHIGAN 48180-4116

PHONE: (734) 374-1474 FAX: (734) 374-1343

www.cityoftaylor.com

CITY COUNCIL

DOUGLAS A. GEISS
Chairman

JILL BRANDANA
Chairwoman Pro-Tem

TINA DANIELS
CHARLES JOHNSON
LINDSEY ROSE
ANGELA WINTON

At the regular meeting of the Taylor City Council held on January 21, 2025 the following Resolution was adopted:

Motion by: Rose, supported by: Daniels

Resolved: Motion to approve the submission of the 2024 Transportation Asset Management Plan to the State of Michigan, and authorize the execution of the required Proof of Acceptance.

Unanimously carried.

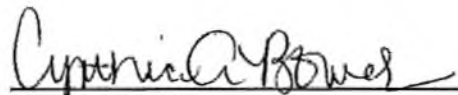
CCR #: 1.27-25

Ayes: Geiss, Brandana, Daniels, Johnson, Rose, Winton

Nays: None

Absent: None

I, Cynthia A. Bower, City Clerk of the City of Taylor, hereby certify that the foregoing is a true and complete copy of a Resolution as adopted by the City Council of the City of Taylor, County of Wayne, Michigan, at a regular meeting held on January 21, 2025 and that said meeting was conducted and public notice of said meeting was given pursuant to and in full compliance with the Open Meetings Act, being Act 267, Public Acts of Michigan, 1976, and that the Minutes of said meeting were kept and will be or have been made available as required by said Act.


Cynthia A. Bower

Date: January 22, 2025



Appendix A. Pavement Asset Management Plan

An attached pavement asset management plan follows.



City of Taylor 2024 Pavement

ASSET MANAGEMENT PLAN

A plan describing the City of Taylor's roadway assets and conditions.

Prepared by:
Wade Trim Associates, Inc.
25251 Northline Road
Taylor, MI 48180
734-947-9700

January 13, 2025



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- Appendix B1. Meeting Minutes Verifying Plan Acceptance by Governing Body

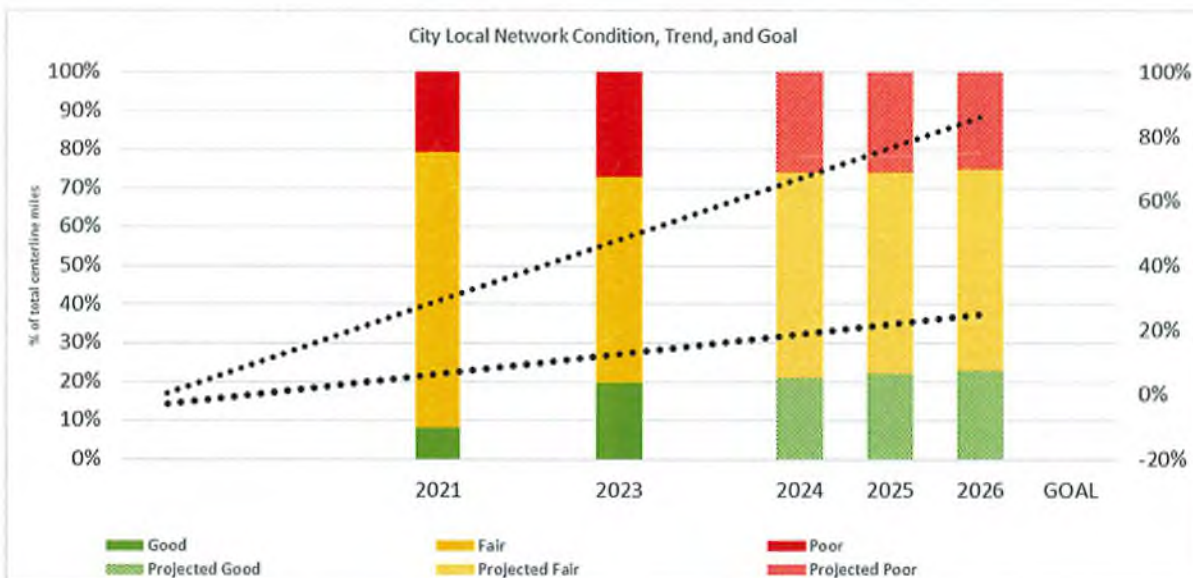
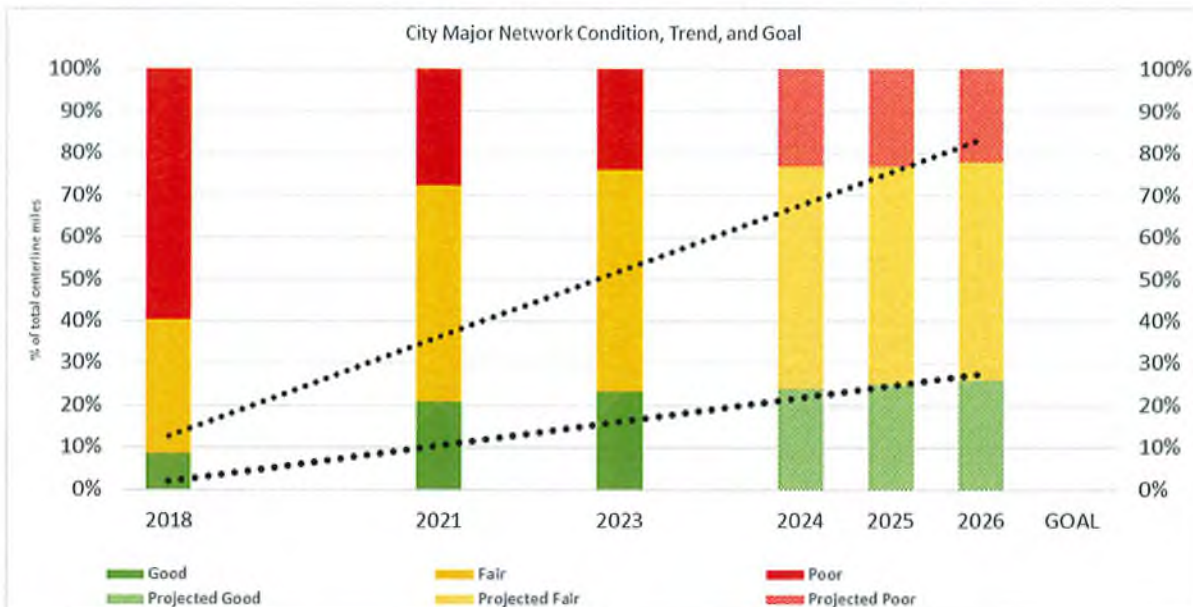
EXECUTIVE SUMMARY

As conduits for commerce and connections to vital services, roads are among the most important assets in any community along with other assets like bridges, culverts, traffic signs, traffic signals, and utilities that support and affect roads. The City of Taylor's (Tay) roads, other transportation assets, and support systems are also some of the most valuable and extensive public assets, all of which are paid for with taxes collected from ordinary citizens and businesses. The cost of building and maintaining roads, their importance to society, and the investment made by taxpayers all place a high level of responsibility on local agencies to plan, build, and maintain the road network in an efficient and effective manner. This asset management plan is intended to report on how Tay is meeting its obligations to maintain the public assets for which it is responsible.

This plan overviews Taylor's road assets and condition, and explains how Taylor works to maintain and improve the overall condition of those assets. These explanations can help answer the following questions:

- What kinds of road assets Taylor has in its jurisdiction, who owns them, and the different options for maintaining these assets.
- What tools and processes Taylor uses to track and manage road assets and funds.
- What condition Taylor's road assets are in compared to statewide averages.
- Why some road assets are in better condition than others and the path to maintaining and improving road asset conditions through proper planning and maintenance.
- How agency transportation assets are funded and where those funds come from.
- How funds are used and the costs incurred during Taylor's road assets' normal life cycle.
- What condition Taylor can expect its road assets if those assets continue to be funded at the current funding levels
- How changes in funding levels can affect the overall condition of all of Taylor's road assets.

Taylor owns and/or manages 195.287 centerline of roads. This road network can be divided into the city major network, the city local network. Based on the different factors these roads have that influence asset management decisions. A summary of Taylor historical and current network conditions, projected trends, and goals for city major network and city local network can be seen in the two figures, below:



An asset management plan is required by Michigan Public Act 325 of 2018, and this document represents fulfillment of some of Taylor’s obligations towards meeting these requirements. This asset management plan also helps demonstrate Taylor’s responsible use of public funds by providing elected and appointed officials as well as the general public with inventory and condition information of Taylor’s road assets, and gives taxpayers the information they need to make informed decisions about investing in its essential transportation infrastructure.

INTRODUCTION

Asset management is defined by Public Act 325 of 2018 as “an ongoing process of maintaining, preserving, upgrading, and operating physical assets cost effectively, based on a continuous physical inventory and condition assessment and investment to achieve established performance goals”. In other words, asset management is a process that uses data to manage and track assets, like roads and bridges, in a cost-effective manner using a combination of engineering and business principles. This process is endorsed by leaders in municipal planning and transportation infrastructure, including the Michigan Municipal League, County Road Association of Michigan, the Michigan Department of Transportation (MDOT), and the Federal Highway Administration (FHWA). Taylor is supported in its use of asset management principles and processes by the Michigan Transportation Asset Management Council (TAMC), formed by the State of Michigan.

Asset management, in the context of this plan, ensures that public funds are spent as effectively as possible to maximize the condition of the road network. Asset management also provides a transparent decision-making process that allows the public to understand the technical and financial challenges of managing road infrastructure with a limited budget.

The City of Taylor (Taylor) has adopted an “asset management” business process to overcome the challenges presented by having limited financial, staffing, and other resources while needing to meet road users’ expectations. Taylor is responsible for maintaining and operating over 195.287 centerline of roads.

This plan outlines how Taylor determines its strategy to maintain and upgrade road asset condition given agency goals, priorities of its road users, and resources provided. An updated plan is to be released approximately every three years to reflect changes in road conditions, finances, and priorities.

Questions regarding the use or content of this plan should be directed to *chris* Gibbs, PE, City Engineer at 25605 Goddard Road, Taylor, MI 48180 or at 734-287-6550 / cgibbs@ci.taylor.mi.us. Key terms used in this plan are defined in Taylor’s comprehensive transportation asset management plan (also known as the “compliance plan”) used for compliance with PA 325 or 2018. Knowing the basic features of the asset classes themselves is a crucial starting point to understanding the rationale behind an asset management approach. The following primer provides an introduction to pavements.

Pavement Primer

Roads come in two basic forms—paved and unpaved. Paved roads have hard surfaces. These hard surfaces can be constructed from asphalt, concrete, composite (asphalt and concrete), sealcoat, and brick and block materials. On the other hand, unpaved roads have no hard surfaces. Examples of these surfaces are gravel and unimproved earth.

The decision to pave with a particular material as well as the decision to leave a road unpaved allows road-owning agencies to tailor a road to a particular purpose, environment, and budget. Thus, selecting a pavement type or leaving a road unpaved depends upon purpose, materials available, and budget. Each choice represents a trade-off between budget and costs for construction and maintenance.

Maintenance enables the road to fulfill its particular purpose. To achieve the maximum service for a pavement or an unpaved road, continual monitoring of a road's pavement condition is essential for choosing the right time to apply the right fix in the right place.

Here is a brief overview of the different types of pavements, how condition is assessed, and treatment options that can lengthen a road's service life.

Surfacing

Pavement type is influenced by several different factors, such as cost of construction, cost of maintenance, frequency of maintenance, and type of maintenance. These factors can have benefits affecting asset life and road user experience.

Paved Surfacing

Typical benefits and tradeoffs for hard surface types include:

- **Concrete Pavement:** Concrete pavement, which is sometimes called a rigid pavement, is durable and lasts a long time when properly constructed and maintained. Concrete pavement can have longer service periods between maintenance activities, which can help reduce maintenance-related traffic disruptions. However, concrete pavements have a high initial cost and can be challenging to rehabilitate and maintain at the end of their service life. A typical concrete pavement design life will provide service for 30 years before major rehabilitation is necessary.
- **Hot-Mix Asphalt Pavement (HMA):** HMA pavement, sometimes known as asphalt or flexible pavement, is currently less expensive to construct than concrete pavement (this is, in some part, due to the closer link between HMA material costs and oil prices that HMA pavements have in comparison with other pavement types). However, they require frequent maintenance activities to maximize their service life. A typical HMA pavement design life will provide service for 18 years before major rehabilitation is necessary. The vast majority of local-agency-owned pavements are HMA pavements.
- **Composite Pavements:** Composite pavement is a combination of concrete and asphalt layers. Typically, composite pavements are old concrete pavements exhibiting ride-related issues that were overlaid by several inches of HMA in order to gain more service life from the pavement before it would need reconstruction. Converting a concrete pavement to a composite pavement is typically used as a "holding pattern" treatment to maintain the road in usable condition until reconstruction funds become available.

- **Sealcoat pavement:** Sealcoat pavement is a gravel road that have been sealed with a thin asphalt binder coating that has stone chips spread on top (not to be confused with a chip seal treatment over HMA pavement). This type of a pavement relies on the gravel layer to provide structure to support traffic, and the asphalt binder coating and stone chips shed water and eliminate the need for maintenance grading. Nonetheless, sealcoat pavement does require additional maintenance steps that asphalt and gravel do not require and does not last as long as HMA pavement, but it provides a low-cost alternative for lightly-trafficked areas and competes with asphalt for ride quality when properly constructed and maintained. Sealcoat pavement can provide service for ten or more years before the surface layer deteriorates and needs to be replaced.

Unpaved Surfacing

Typical benefits and tradeoffs for non-hard surfacing include:

- **Gravel:** Gravel is a low-cost, easy-to-maintain road surface made from layers of soil and aggregate (gravel). However, there are several potential drawbacks such as dust, mud, and ride smoothness when maintenance is delayed or traffic volume exceeds design expectations. Gravel roads require frequent low-cost maintenance activities. Gravel can be very cost effective for lower-volume, lower-speed roads. In the right conditions, a properly constructed and maintained gravel road can provide a service life comparable to an HMA pavement and can be significantly less expensive than the other pavement types.

Pavement Condition

Besides traffic congestion, pavement condition is what road users typically notice most about the quality of the roads that they regularly use—the better the pavement condition, the more satisfied users are with the service provided by the roadwork performed by road-owning agencies. Pavement condition is also a major factor in determining the most cost-effective treatment—that is, routine maintenance, capital preventive maintenance, or structural improvement—for a given section of pavement. As pavements age, they transition between “windows” of opportunity when a specific type of treatment can be applied to gain an increase in quality and extension of service life. Routine maintenance is day-to-day, regularly-scheduled, low-cost activity applied to “good” roads to prevent water or debris intrusion. Capital preventive maintenance (CPM) is a planned set of cost-effective treatments for “fair” roads that corrects pavement defects, slows further deterioration, and maintains the functional condition without increasing structural capacity. Taylor uses pavement condition and age to anticipate when a specific section of pavement will be a potential candidate for preventive maintenance. More detail on this topic is included in the *Pavement Treatment* section of this primer.

Pavement condition data is also important because it allows road owners to evaluate the benefits of preventive maintenance projects. This data helps road owners to identify the most cost-effective use of road construction and maintenance dollars. Further, historic pavement condition data can enable

road owners to predict future road conditions based on budget constraints and to determine if a road network's condition will improve, stay the same, or degrade at the current or planned investment level. This analysis can help determine how much additional funding is necessary to meet a network's condition improvement goals.

Paved Road Condition Rating System

Taylor is committed to monitoring the condition of its road network and using pavement condition data to drive cost-effective decision-making and preservation of valuable road assets. Taylor uses the Pavement Surface Evaluation and Rating (PASER) system to assess its paved roads. PASER was developed by the University of Wisconsin Transportation Information Center to provide a simple, efficient, and consistent method for evaluating road condition through visual inspection. The widely-used PASER system has specific criteria for assessing asphalt, concrete, sealcoat, and brick and block pavements. Information regarding the PASER system and PASER manuals may be found on the TAMC website at: http://www.michigan.gov/tamc/0,7308,7-356-82158_82627--,00.html.

The TAMC has adopted the PASER system for measuring statewide pavement conditions in Michigan for asphalt, concrete, composite, sealcoat, and brick-and-block paved roads. Broad use of the PASER system means that data collected at Taylor is consistent with data collected statewide. PASER data is collected using trained inspectors in a slow-moving vehicle using GPS-enabled data collection software provided to road-owning agencies at no cost to them. The method does not require extensive training or specialized equipment, and data can be collected rapidly, which minimizes the expense for collecting and maintaining this data.

The PASER system rates surface condition using a 1-10 scale where 10 is a brand new road with no defects that can be treated with routine maintenance, 5 is a road with distresses but is structurally sound that can be treated with preventive maintenance, and 1 is a road with extensive surface and structural distresses that is in need of total reconstruction.

Roads with lower PASER scores generally require costlier treatments to restore their quality than roads with higher PASER scores. The cost effectiveness of treatments generally decreases as the PASER number decreases. In other words, as a road deteriorates, it costs more dollars per mile to fix it, and the dollars spent are less efficient in increasing the road's service life. Nationwide experience and asset management principles tell us that a road that has deteriorated to a PASER 4 or less will cost more to improve and the dollars spent are less efficient. Understanding this cost principle helps to draw meaning from the current PASER condition assessment.

The TAMC has developed statewide definitions of road condition by creating three simplified condition categories—"good", "fair", and "poor"—that represent bin ranges of PASER scores having similar contexts with regard to maintenance and/or reconstruction. The definitions of these rating conditions are:

- "Good" roads, according to the TAMC, have PASER scores of 8, 9, or 10. Roads in this category have very few, if any, defects and only require minimal maintenance; they may be kept in this category longer using PPM. These roads may include those that have been recently seal coated or newly constructed. **Figure 0-1** illustrates an example of a road in this category.
- "Fair" roads, according to the TAMC, have PASER scores of 5, 6, or 7. Roads in this category still show good structural support, but their surface is starting to deteriorate. **Figure 0-1** illustrates two road examples in this category. CPM can be cost effective for maintaining the road's "fair" condition or even raising it to "good" condition before the structural integrity of the pavement has been severely impacted. CPM treatments can be likened to shingles on a roof of a house: while the shingles add no structural value, they protect the house from structural damage by maintaining the protective function of a roof covering.
- "Poor" roads, according to the TAMC, have PASER scores of 1, 2, 3, or 4. These roads exhibit evidence that the underlying structure is failing, such as alligator cracking and rutting. These roads must be rehabilitated with treatments like a heavy overlay, crush and shape, or total reconstruction. **Figure 0-1** illustrates a road in this category.



Figure 0-1. *Top image, right*– PASER 8 road that is considered "good" by the TAMC exhibit only minor defects. *Second image, right*– PASER 5 road that is considered "fair" by the TAMC. Exhibiting structural soundness but could benefit from CPM. *Third image, right*– PASER 6 road that is considered "fair" by the TAMC. *Bottom image, right*– PASER 2 road that is considered "poor" by the TAMC exhibiting significant structural distress.

The TAMC's good, fair, and poor categories are based solely on the definitions, above. Therefore, caution should be exercised when comparing other condition assessments with these categories

because other condition assessments may have “good”, “fair”, or “poor” designations similar to the TAMC condition categories but may not share the same definition. Often, other condition assessment systems define the “good”, “fair”, and “poor” categories differently, thus rendering the data of little use for cross-system comparison. The TAMC’s definitions provide a statewide standard for all of Michigan’s road-owning agencies to use for comparison purposes.

PASER data is collected 100 percent every two years on all federal-aid-eligible roads in Michigan. The TAMC dictates and funds the required training and the format for this collection, and it shares the data regionally and statewide. In addition, Taylor collects 100 percent of its paved non-federal-aid-eligible network using consultants to complete the work.

Pavement Treatments

Selection of repair treatments for roads aims to balance costs, benefits, and road life expectancy. All pavements are damaged by water, traffic weight, freeze/thaw cycles, and sunlight. Each of the following treatments and strategies—reconstruction, structural improvements, capital preventive maintenance, and others used by Taylor—counters at least one of these pavement-damaging forces.

Reconstruction

Pavement reconstruction treats failing or failed pavements by completely removing the old pavement and base and constructing an entirely new road (**Figure 0-2**). Every pavement has to eventually be reconstructed and it is usually done as a last resort after more cost-effective treatments are done, or if the road requires significant changes to road geometry, base, or buried utilities. Compared to the other treatments, which are all improvements of the existing road, reconstruction is the most extensive rehabilitation of the roadway and therefore, also the most expensive per mile and most disruptive to regular traffic patterns. Reconstructed pavement will subsequently require one or more of the previous maintenance treatments to maximize service life and performance. A reconstructed road lasts approximately 15 years and costs \$250,000 per lane mile. The following descriptions outline the main reconstruction treatments used by Taylor.

Figure 0-2. Examples of Reconstruction Treatments—(left) Reconstructing a Road and (right) Road Prepared for Full-Depth Repair



Full-Depth Concrete Repair

Full-depth concrete repair removes sections of damaged concrete pavement and replaces it with new concrete of the same dimensions (Figure 0-2). It is usually performed on isolated deteriorated joint locations or entire slabs that are much further deteriorated than adjacent slabs. The purpose is to restore the riding surface, delay water infiltration, restore load transfer from one slab to the next, and eliminate the need to perform costly temporary patching. This repair lasts approximately twelve years.

Ditching (for Unpaved Roads)

Water needs to drain away from any roadway to delay softening of the pavement structure, and proper drainage is critical for unpaved roads where there is no hard surface on top to stop water infiltration into the road surface and base. To improve drainage, new ditches are dug or old ones are cleaned out. Unpaved roads typically need to be re-ditched every five years.

Structural Improvement

Roads requiring structural improvements exhibit alligator cracking and rutting and rated poor in the TAMC scale. Road rutting is evidence that the underlying structure is beginning to fail and it must be either rehabilitated with a structural treatment. Examples of structural improvement treatments include HMA overlay with or without milling, and crush and shape (Figure 0-3). The following descriptions outline the main structural improvement treatments used by Taylor

Figure 0-3. Examples of Structural Improvement Treatments—from Left) HMA Overlay on an Unmilled Pavement, Milling Asphalt Pavement, and Pulverization of a Road During a Crush-and-Shape Project



Hot-Mix Asphalt (HMA) Overlay with/without Milling

An HMA overlay is a layer of new asphalt (liquid asphalt and stones) placed on an existing pavement (Figure 0-3). Depending on the overlay thickness, this treatment can add significant structural strength. This treatment also creates a new wearing surface for traffic and seals the pavement from water, debris, and sunlight damage. An HMA overlay lasts approximately five to ten years. The top layer of severely damaged pavement can be removed by the milling, a technique that helps prevent

structural problems from being quickly reflected up to the new surface. Milling is also done to keep roads at the same height of curb and gutter that is not being raised or reinstalled in the project.

Crush and Shape

During a crush and shape treatment, the existing pavement and base are pulverized and then the road surface is reshaped to correct imperfections in the road's profile (Figure 0-3). An additional layer of gravel is often added along with a new wearing surface such as an HMA overlay or chip seal. Additional gravel and an HMA overlay give an increase in the pavements structural capacity. This treatment is usually done on rural roads with severe structural distress; Adding gravel and a wearing surface makes it more prohibitive for urban roads if the curb and gutter is not raised up. Crush and shape treatments last approximately 14 years.

Capital Preventive Maintenance

Capital preventive maintenance (CPM) addresses pavement problems of fair-rated roads before the structural integrity of the pavement has been severely impacted. CPM is a planned set of cost-effective treatments applied to an existing roadway that slows further deterioration and that maintains or improves the functional condition of the system without significantly increasing the structural capacity. Examples of such treatments include crack seal, fog seal, chip seal, slurry seal, and microsurface (Figure 0-4). The purpose of the following CPM treatments is to protect the pavement structure, slow the rate of deterioration, and/or correct pavement surface deficiencies. The following descriptions outline the main CPM treatments used by Taylor.

Figure 0-4. Examples of Capital Preventive Maintenance Treatments—(from Left) Crack Seal, Fog Seal, Chip Seal, and Slurry Seal/Microsurface.



Crack Sea

Water that infiltrates the pavement surface softens the pavement structure and allows traffic loads to cause more damage to the pavement than in normal dry conditions. Crack sealing helps prevent water infiltration by sealing cracks in the pavement with asphalt sealant (Figure 0-4). Taylor seals pavement cracks early in the life of the pavement to keep it functioning as strong as it can and for as long as it can. Crack sealing lasts approximately two years. Even though it does not last very long compared to other treatments, it does not cost very much compared to other treatments. This makes

it a very cost-effective treatment when Taylor looks at what crack filling costs per year of the treatment's life.

Chip Seal

A chip seal, also known as a sealcoat, is a two-part treatment that starts with liquid asphalt sprayed onto the old pavement surface followed by a single layer of small stone chips spread onto the wet liquid asphalt layer (Figure 0-4). The liquid asphalt seals the pavement from water and debris and holds the stone chips in place, providing a new wearing surface for traffic that can correct friction problems and helping to prevent further surface deterioration. Chip seals are best applied to pavements that are not exhibiting problems with strength, and their purpose is to help preserve that strength. These treatments last approximately five years.

Partial-Depth Concrete Repair

A partial-depth concrete repair involves removing spalled (i.e., fragmented) or delaminated (i.e., separated into layers) areas of concrete pavement, usually near joints and cracks and replacing with new concrete (Figure 0-5). This is done to provide a new wearing surface in isolated areas, to slow down water infiltration, and to help delay further freeze/thaw damage. This repair lasts approximately five years.

Figure 0-5. An Examples of Capital Preventive Maintenance Treatment is Concrete Road Prepared for Partial-Depth Repair.



Maintenance

Maintenance is the most cost-effective strategy for managing road infrastructure and prevents good and fair roads from reaching the poor category, which require costly rehabilitation and reconstruction treatments to create a year of service life. It is most effective to spend money on routine maintenance and CPM treatments, first; then, when all maintenance project candidates are treated, reconstruction and rehabilitation can be performed as money is available. This strategy is called a "mix-of-fixes" approach to managing pavements.

1.0 PAVEMENT ASSETS

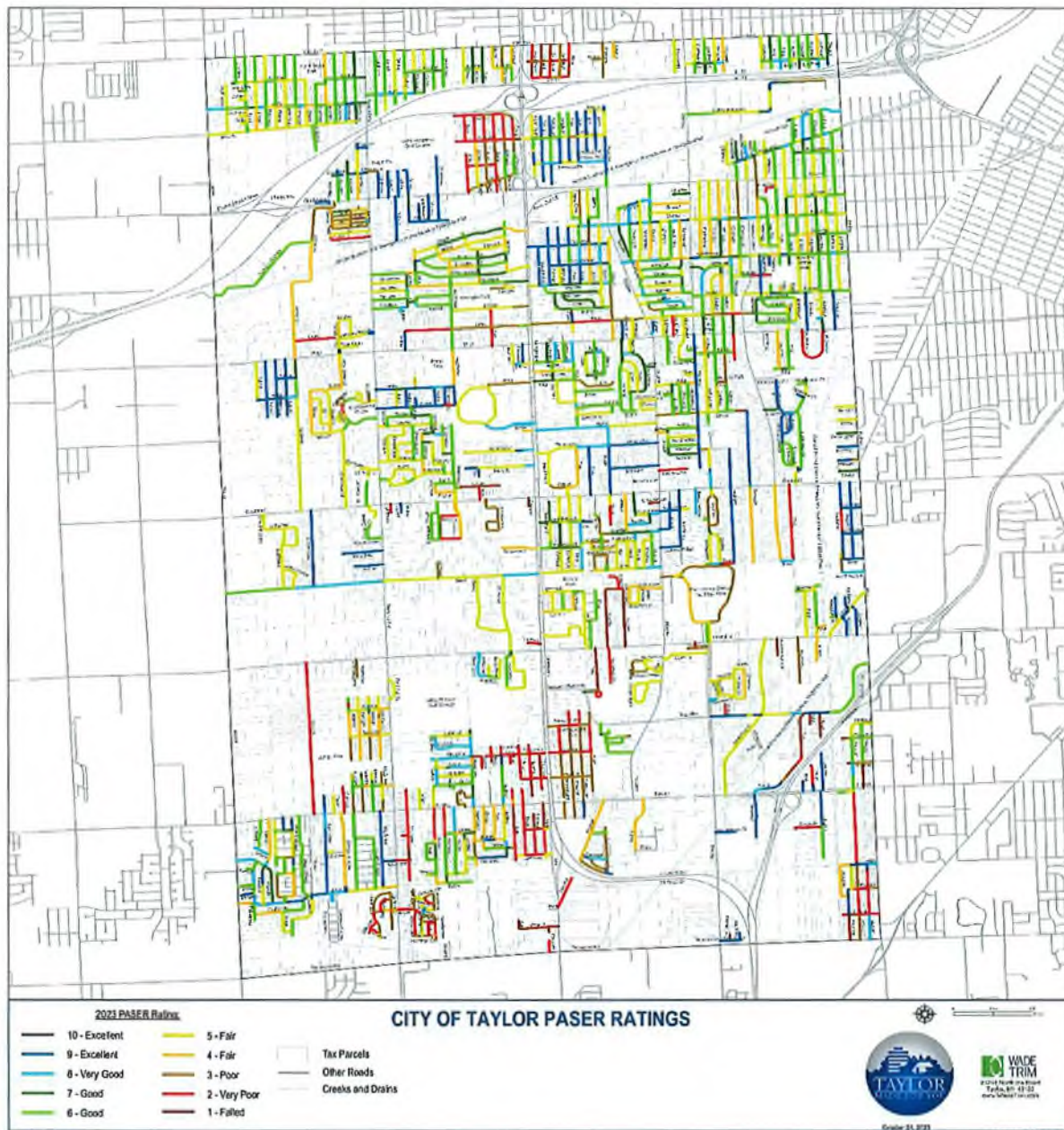
Building a mile of new road can cost over \$1 million due to the large volume of materials and equipment that are necessary. The high cost of constructing road assets underlines the critical nature of properly managing and maintaining the investments made in this vital infrastructure. The specific needs of every mile of road within an agency's overall road network is a complex assessment, especially when considering rapidly changing conditions and the varying requisites of road users; understanding each road-mile's needs is an essential duty of the road-owning agency.

In Michigan, many different governmental units (or agencies) own and maintain roads, so it can be difficult for the public to understand who is responsible for items such as planning and funding construction projects, [patching] repairs, traffic control, safety, and winter maintenance for any given road. MDOT is responsible for state trunkline roads, which are typically named with "M", "I", or "US" designations regardless of their geographic location in Michigan. Cities and villages are typically responsible for all public roads within their geographic boundary with the exception of the previously mentioned state trunkline roads managed by MDOT. County road commissions (or departments) are typically responsible for all public roads within the county's geographic boundary, with the exception of those managed by cities, villages, and MDOT.

In cases where non-trunkline roads fall along jurisdictional borders, local and intergovernmental agreements dictate ownership and maintenance responsibility. Quite frequently, roads owned by one agency may be maintained by another agency because of geographic features that make it more cost effective for a neighboring agency to maintain the road instead of the actual road owner. Other times, road-owning agencies may mutually agree to coordinate maintenance activities in order to create economies of scale and take advantage of those efficiencies.

The City of Taylor is responsible for a total of 195.287 centerline of public roads, as shown in **Figure 1-1**.

Figure 1-1. Map Showing Location of Taylor’s Paved Roads (I.E., Those Managed By Taylor) and their Current Condition for Paved Roads



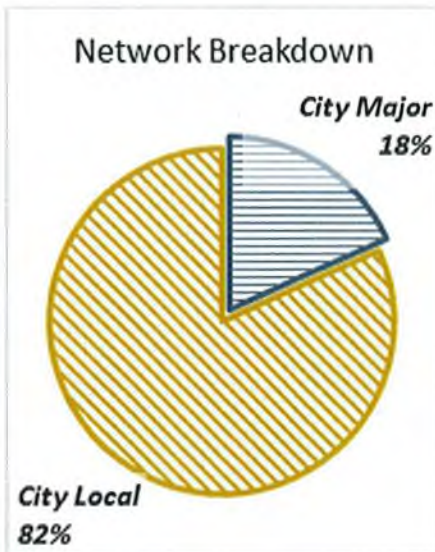
1.1 Inventory

Michigan Public Act 51 of 1951 (PA 51), which defines how funds from the Michigan Transportation Fund (MTF) are distributed to and spent by road-owning agencies, classifies roads owned by Taylor as either city major or city local roads. State statute prioritizes expenditures on the city major road network.

Of the 195.287 centerline of public roads owned and/or managed by Taylor, approximately 82% of all County Primary roads are classified as federal aid eligible, which allows them to receive federal funding for their maintenance and construction. Only 1% of County Local roads are considered federal aid eligible, which means state and local funds must be used to manage these roads.

Figure 1-2 illustrates the percentage of roads owned by Taylor that are classified as city major and city local roads.

Figure 1-2. Percentage of City Major and City Local Roads for Taylor



Taylor also owns and manages 0.955 miles of unpaved roads

1.1.1 Types

Taylor has multiple types of pavements in its jurisdiction, including: asphalt, concrete, and undefined; it also has unpaved roads (i.e., gravel and/or earth). Factors influencing pavement type include cost of construction, cost of maintenance, frequency of maintenance, type of maintenance, asset life, and road user experience. More information on pavement types is available in the Introduction's Pavement Primer.

Figure 1-3 illustrates the percentage of various pavement types that Taylor has in its network.

Figure 1-3. Percentage of Various Pavement types

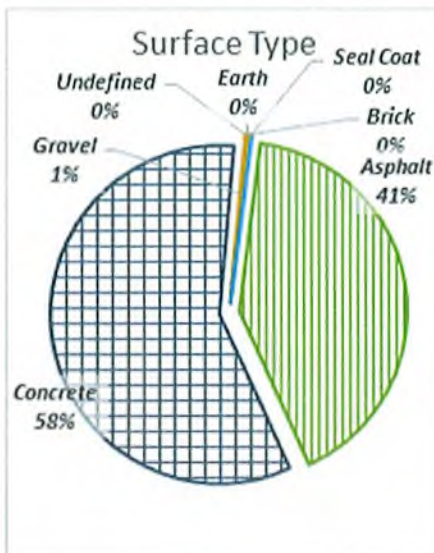


Figure 1-3 shows the pavement type by City boundary for Taylor's jurisdiction

1.1.2 Locations

Locations and sizes of each asset can be found in Taylor's Roadsoft database. For more detail, please refer to the agency contact listed in the *Introduction* of this pavement asset management plan.

1.1.3 Condition

The road characteristic that road users most readily notice is pavement condition. Pavement condition is a major factor in determining the most cost-effective treatment—that is, routine maintenance, capital preventive maintenance, or structural improvement—for a given section of pavement. Taylor uses pavement condition and age to anticipate when a specific section of pavement will be a potential candidate for preventive maintenance. Pavement condition data enables Taylor to evaluate the benefits of preventive maintenance projects and to identify the most cost-effective use of road construction and maintenance dollars. Historic pavement condition data can be used to predict future road conditions based on budget constraints and to determine if a road network's condition will improve, stay the same, or degrade at the current or planned investment level. This analysis helps to determine how much additional funding is necessary to meet a network's condition improvement goals. More detail on this topic is included in the *Introduction's Pavement Primer*.

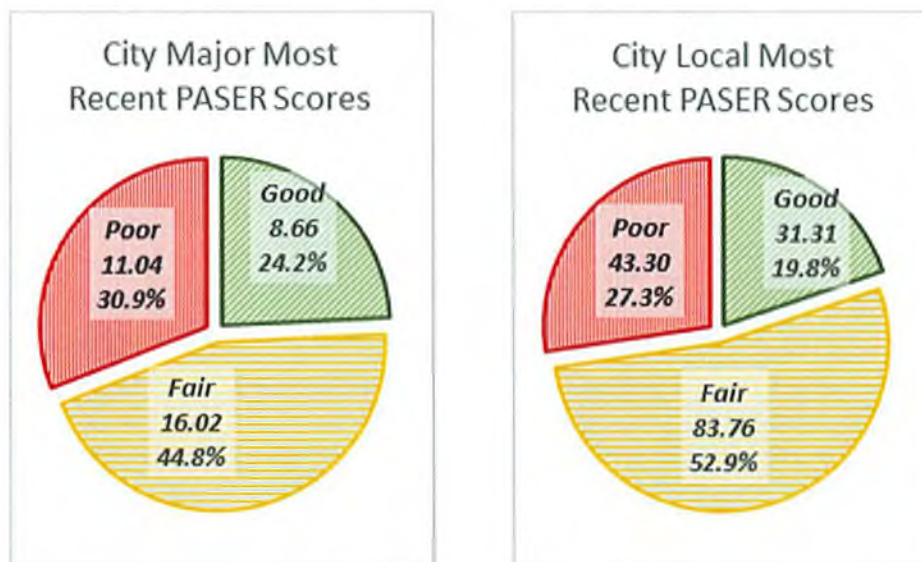
Paved Roads

Taylor is committed to monitoring the condition of its road network and using pavement condition data to drive cost-effective decision-making and preservation of valuable road assets. Taylor uses the Pavement Surface Evaluation and Rating (PASER) system, which has been adopted by the TAMC for measuring statewide pavement conditions, to assess its paved roads. The PASER system provides a simple, efficient, and consistent method for evaluating road condition through visual inspection. More information regarding the PASER system can be found in the Introduction's Pavement Primer.

Taylor collects 100 percent of its PASER data every two years on all federal-aid-eligible roads in Michigan. In addition, Taylor collects 100 percent of its paved non-federal-aid-eligible network using its own staff and resources.

Taylor's 2024 paved city major road network has 24 percent of roads in the TAMC good condition category, 45 percent in fair, and 31 percent in poor (Figure 1-4A). The paved city local road network has 20 percent in good, 53 percent in fair, and 27 percent in poor (Figure 1-4B).

Figure 1-4. (A) Left: Taylor Paved City Major Road Network Conditions By Percentage Of Good, Fair, Or Poor, And (B) Right: Paved City Local road network conditions by percentage of good, fair, or poor

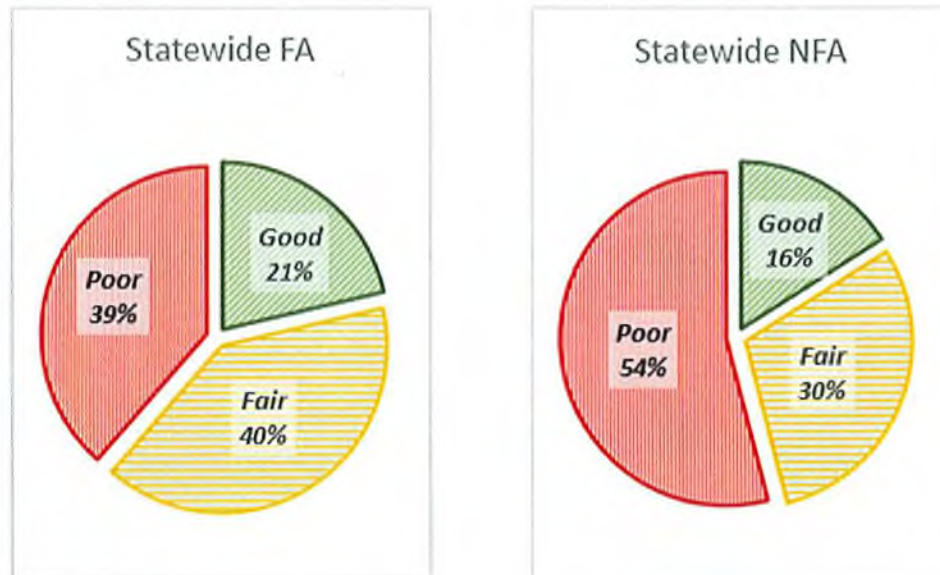


In comparison, the statewide paved city major road network has 21 percent of roads in the TAMC good condition category, 40 percent in fair, and 39 percent in poor (Figure 1-4A). The statewide paved city local road network has 16 percent in good, 30 percent in fair, and 54 percent in poor (Figure 1-4B). Comparing Figure 1-4A and Figure 1-4B shows that Taylor's paved city major road network is approximately the same than similarly-classified roads in the rest of the state, while

Figure 1-5B and Figure 1-5B show that Taylor's paved city local road network is slightly worse than similarly-classified roads in the rest of the state. Other road condition graphs can be viewed on the TAMC pavement condition dashboard at:

<http://www.mcgi.state.mi.us/mitrp/Data/PaserDashboard.aspx>.

Figure 1-5. (A) Left: Statewide Paved City Major Road Network Conditions By Percentage Of Good, Fair, Or Poor, And (B) Right: Paved City Local Road network conditions by percentage of good, fair, or poor



The city of Taylor's road network is pretty close to the state average with just some variances between the different categories. This is due to the fact that Talor is investing money into their road system.

Figure 1-6 and Figure 1-7 show the number of miles for Taylor's roads with PASER scores expressed in TAMC definition categories for the paved city major road network (Figure 1-6) and the paved city local road network (Figure 1-7). Taylor considers road miles on the transition line between good and fair (PASER 8) and the transition line between fair and poor (PASER 5) as representing parts of the road network where there is a risk of losing the opportunity to apply less expensive treatments that gain significant improvements in service life.

Figure 1-6. Taylor Paved City Local Network Condition By PASER Rating. Bar Graph Colors Correspond to Good/Fair/Poor TAMC Designations.

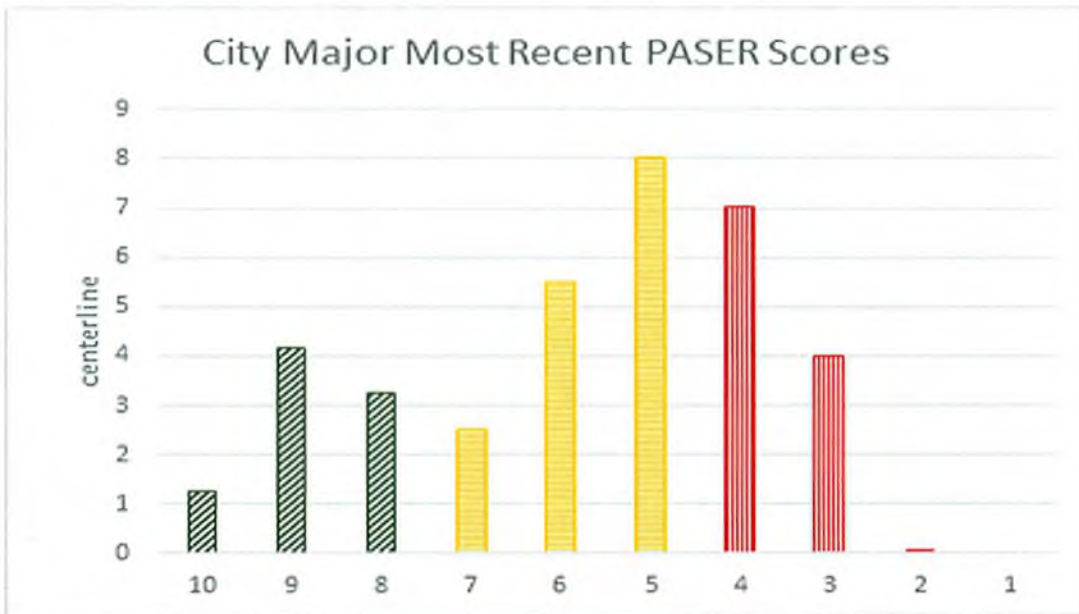


Figure 1-7. Taylor Paved City Local Network Condition By PASER Rating. Bar Graph Colors Correspond To Good/Fair/Poor TAMC Designations.

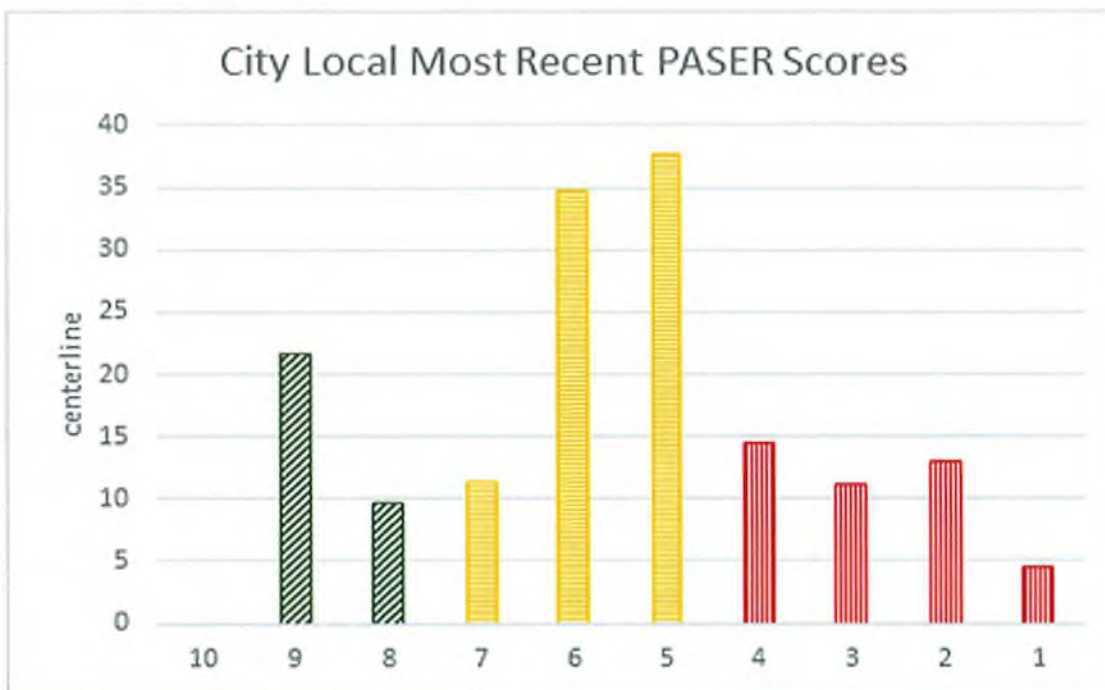
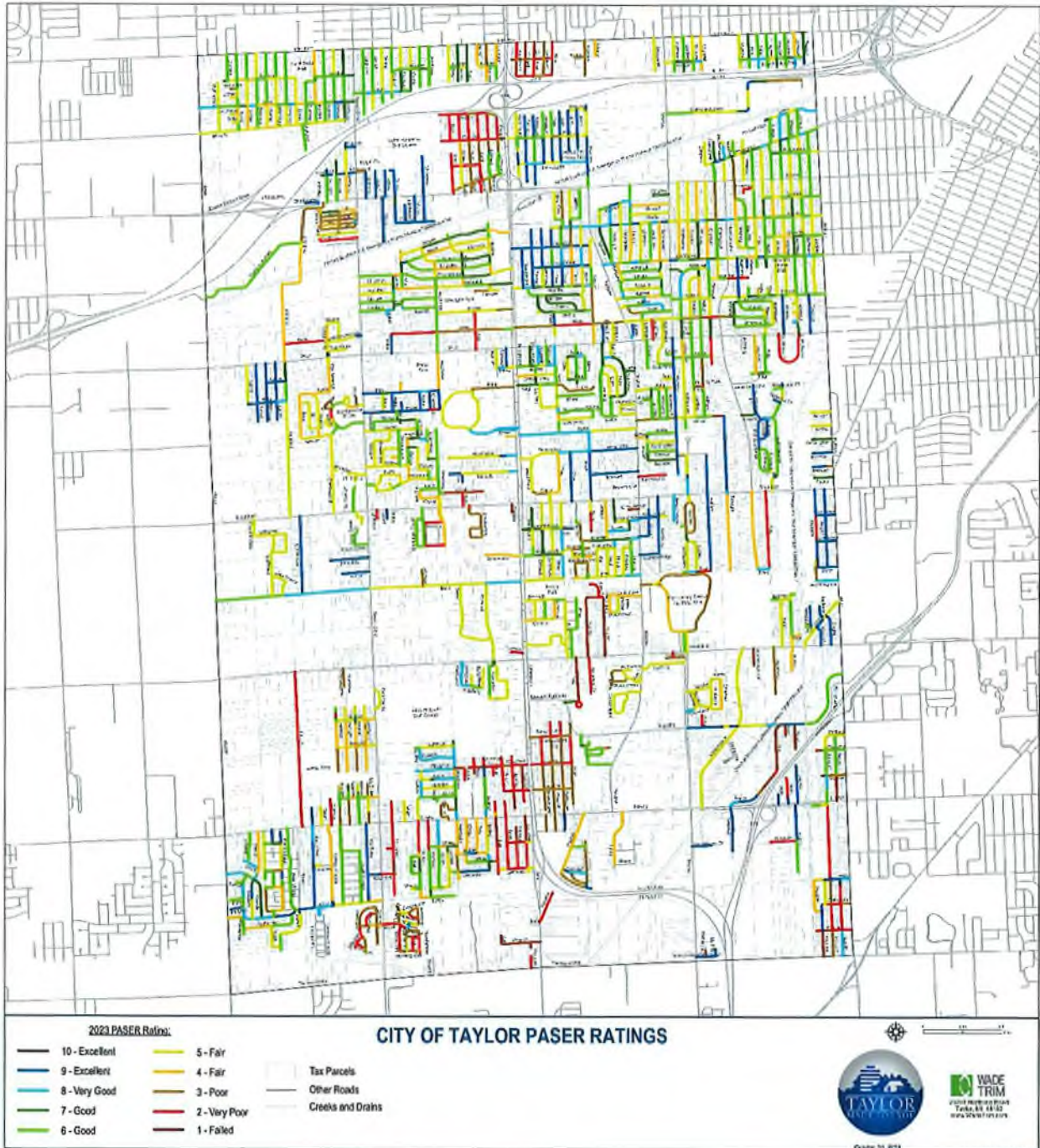


Figure 1-8 provides a map illustrating the geographic location of paved roads and their respective PASER condition. An online version of the most recent PASER data is located at <https://www.mcgi.state.mi.us/tamcMap/>.

Figure 1-8. Map of the Current Paved Road Condition



The City is working towards increasing the overall condition of their road network. Historically, the overall quality of Taylor's paved city major roads have been increasing, as can be observed in **Figure 1-9**. This is due to the fact that Talor has been actively working towards applying the right fix for a road segment at the right time.

Comparing Taylor's paved city major road condition trends illustrated in **Figure 1-9** with overall statewide condition trends for similarly-classified roads, which are illustrated in **Figure 1-10**, shows a similar trend locally as in the rest of the state.

This similar trend can be seen in recent years due to the fact that Taylor is changing their repair and maintenance methods to align better with other communities in the state.

Figure 1-9. Historical Taylor Paved City Major Road Network Condition Trend

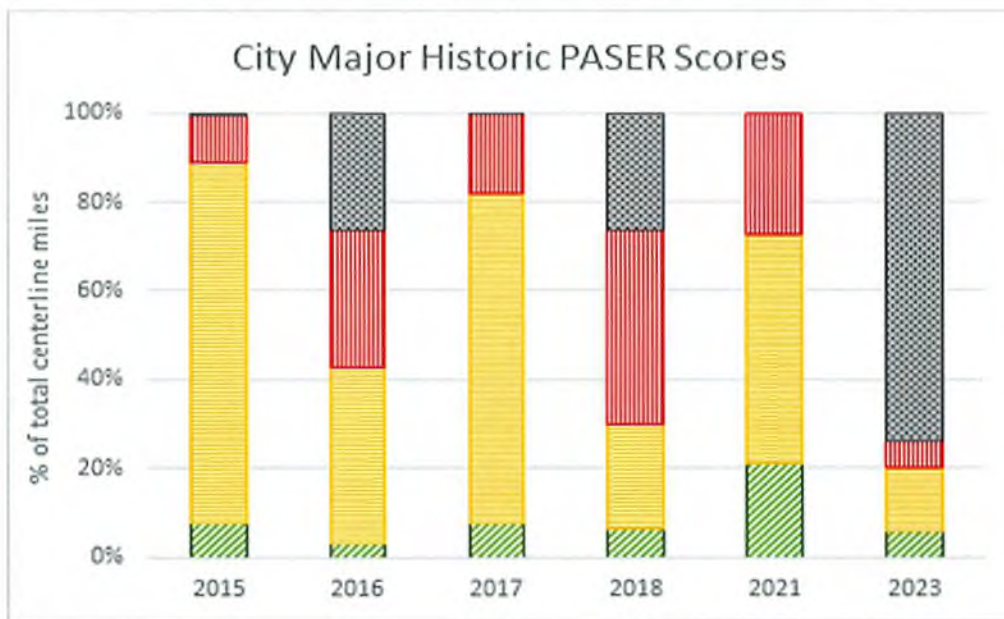
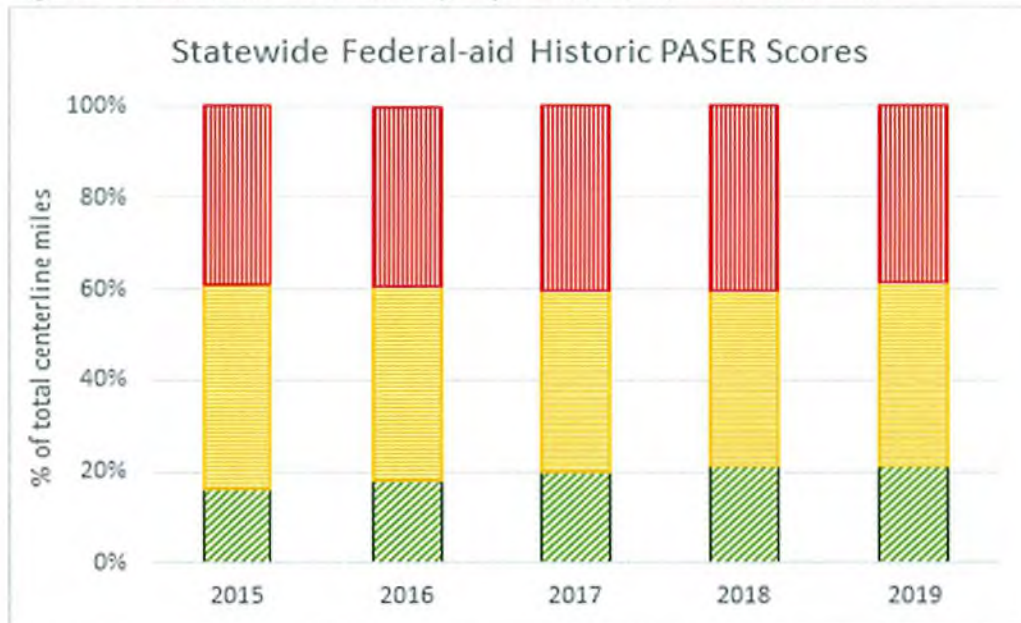


Figure 1-10: Historical Statewide City Major Road Network Condition Trend



Historically, the overall quality of Taylor’s paved city local roads have been increasing, because they have invested money to improve the system. **Figure 1-11** illustrates the condition of the paved city local road network in Taylor while **Figure 1-12** illustrates these conditions statewide.

Comparing Taylor’s paved city local road condition trends illustrated in **Figure 1-11** with overall statewide condition trends for all paved city local roads illustrated in **Figure 1-12** indicates a similar trend locally as in the rest of the state.

Figure 1-11. Historical Taylor Paved City Local Road Network Condition Trend

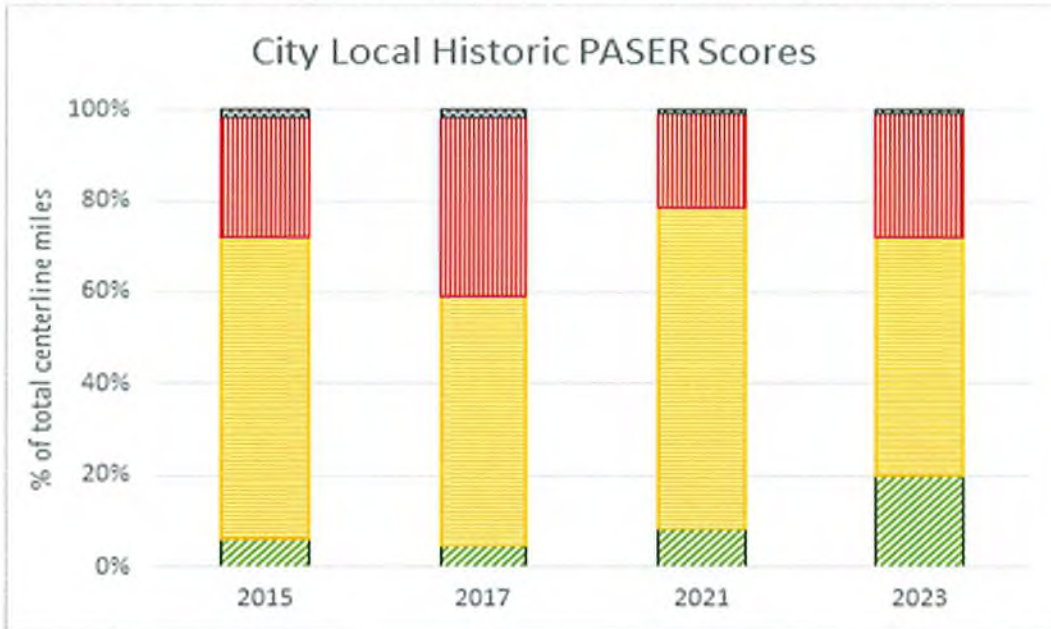
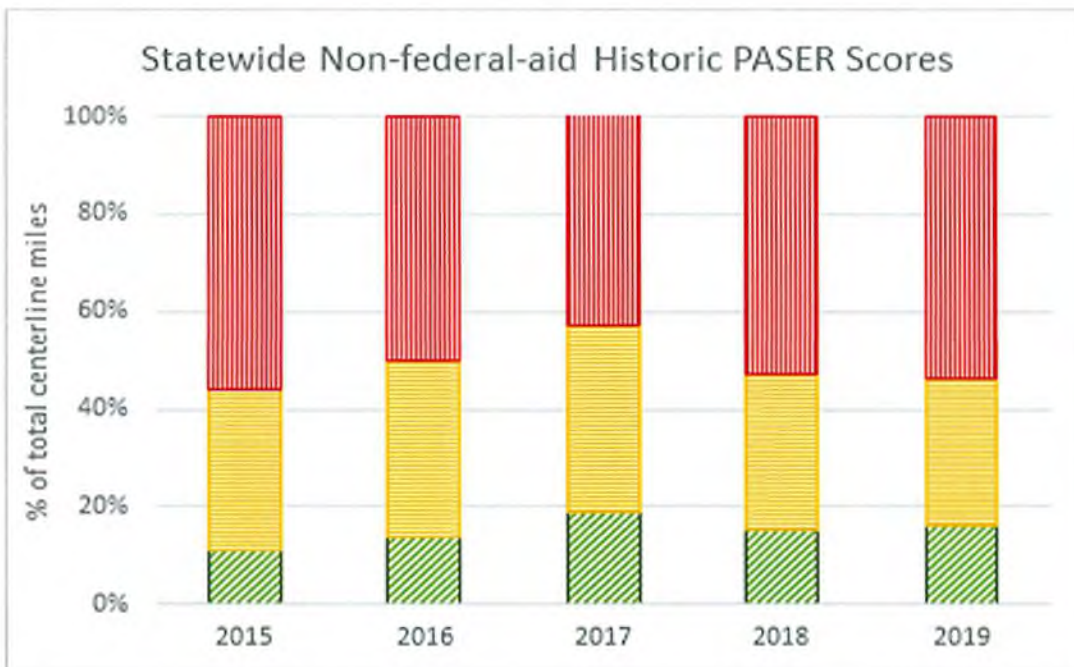


Figure 1-12. Historical Statewide Paved City Local Road Network Condition Trend



1.2 Goals

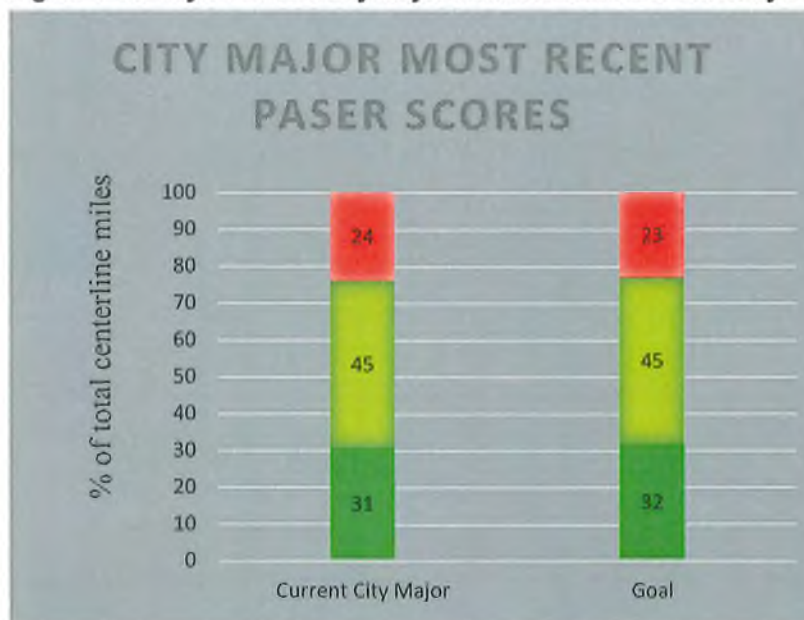
Goals help set expectations to how pavement conditions will change in the future. Pavement condition changes are influenced by water infiltration, soil conditions, sunlight exposure, traffic

loading, and repair work performed. Taylor is not able to control any of these factors fully due to seasonal weather changes, traffic pattern changes, and its limited budget. In spite of the uncontrollable variables, it is still important to set realistic network condition goals that efficiently use budget resources to build and maintain roads meeting taxpayer expectations. An assessment of the progress toward these goals is provided in **Section 1.5 - Pavement Assets: Gap Analysis** section of this plan

1.2.1 Goals for Paved City Major Roads

The overall goal for Taylor’s paved city major road network is to maintain or improve road conditions network-wide at 2024 levels. The baseline condition for this goal is illustrated in **Figure 1-14**.

Figure 1-13. Taylor’s 2024 City Major Road Network Condition by Percentage of Good/Fair/Poor



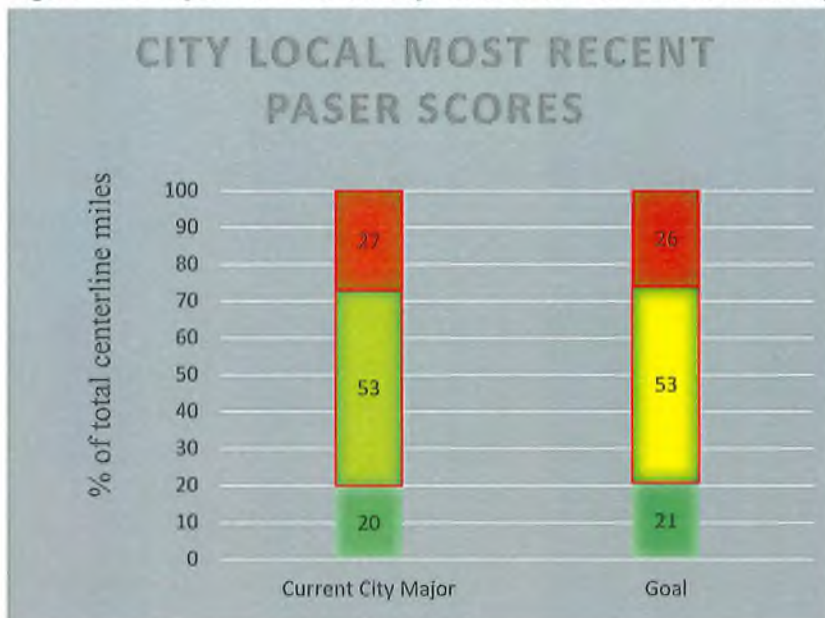
Taylor’s network-level pavement condition strategy for paved city major roads is:

1. Prevent its good and fair (PASER 10 - 5) paved city major from becoming poor (PASER 4 - 1).

1.2.2 Goals for Paved City Local Roads

The overall goal for Taylor’s paved city local road network is to maintain or improve road conditions network-wide at 2024 levels. The baseline condition for this goal is illustrated in **Figure 1-14**.

Figure 1-14. Taylor 2024 Paved City Local Road Network Condition by Percentage of Good/Fair/Poor



Taylor's network-level pavement condition strategy for paved city local roads is:

1. Prevent its good and fair (PASER 10 - 5) paved city local roads from becoming poor (PASER 4 - 1).

1.3 Modelled Trends

Roads age and deteriorate just like any other asset. All pavements are damaged by water, traffic weight, freeze/thaw cycles, sunlight, and traffic weight. To offset natural deterioration and normal wear-and-tear on the road, Taylor must complete treatment projects that either protect and/or add life to its pavements. The year-end condition of the whole network depends upon changes or preservation of individual road section condition that preservation treatments have affected.

Taylor uses many types of repair treatments for its roads, each selected to balance costs, benefits, and road life expectancy. When agency trends are modelled, any gap between goals and accomplishable work becomes evident. Financial resources influence how much work can be accomplished across the network within agency budget and what treatments and strategies can be afforded; a full discussion of Taylor's financial resources can be found in **Section 2.0 - Financial Resources**.

Treatments and strategies that counter pavement-damaging forces include reconstruction, structural improvement, capital preventive maintenance, innovative treatments, and maintenance. Correlating with each PASER score are specific types of treatments best performed either to protect the pavement (CPM) or to add strength back into the pavement (structural improvement) (Table 1). MDOT provides guidance regarding when a specific pavement may be a candidate for a particular

treatment. These identified PASER scores “trigger” the timing of projects appropriately to direct the right pavement fix at the right time, thereby providing the best chance for a successful project. The information provided in **Table 1-1** is a guide for identifying potential projects; however, this table should not be the sole criteria for pavement treatment selection. Other information such as future development, traffic volume, utility projects, and budget play a role in project selection. This table should not be a substitute for engineering judgement.

Table 1-1. Service Life Extension (in Years) for Pavement Types Gained by Fix Type¹

Fix Type	Life Extension (in years)*			PASER
	Flexible	Composite	Rigid	
HMA crack treatment	1-3	1-3	N/A	6-7
Overband crack filling	1-2	1-2	N/A	6-7
One course non-structural HMA overlay	5-7	4-7	N/A	4-5****
Mill and one course non-structural HMA overlay	5-7	4-7	N/A	3-5
Single course chip seal	3-6	N/A	N/A	5-7†
Double chip seal	4-7	3-6	N/A	5-7†
Single course microsurface	3-5	**	N/A	5-6
Multiple course microsurface	4-6	**	N/A	4-6****
Ultra-thin HMA overlay	3-6	3-6	N/A	4-6****
Paver placed surface seal	4-6	**	N/A	5-7
Full-depth concrete repair	N/A	N/A	3-10	4-5***
Concrete joint resealing	N/A	N/A	1-3	5-8
Concrete spall repair	N/A	N/A	1-3	5-7
Concrete crack sealing	N/A	N/A	1-3	4-7
Diamond grinding	N/A	N/A	3-5	4-6
Dowel bar retrofit	N/A	N/A	2-3	3-5***
Longitudinal HMA wedge/scratch coat with surface treatment	3-7	N/A	N/A	3-5****
Flexible patching	**	**	N/A	N/A
Mastic joint repair	1-3	1-3	N/A	4-7
Cape seal	4-7	4-7	N/A	4-7
Flexible interlayer “A”	4-7	4-7	N/A	4-7
Flexible interlayer “B” (SAMI)	4-7	4-7	N/A	3-7
Flexible interlayer “C”	4-7	4-7	N/A	3-7

	Life Extension (in years)*			
Fiber reinforced flexible membrane	4-7	4-7	N/A	3-7
Fog seal	**	**	N/A	7-10
GSB 88	**	**	N/A	7-10
Mastic surface treatment	**	**	N/A	7-10
Scrub seal	**	**	N/A	4-8
* The time range is the expected life extending benefit given to the pavement, not the anticipated longevity of the treatment.				
** Data is not available to quantify the life extension.				
*** The concrete slabs must be in fair to good condition.				
**** Can be used on a pavement with a PASER equal to 3 when the sole reason for rating is rutting or severe raveling of the surface asphalt layer.				
† For PASER 4 or less providing structural soundness exists and that additional pre-treatment will be required for example, wedging, bar seals, spot double chip seals, injection spray patching or other pre-treatments.				
‡ Part of Appendix D-1 from MDOT Local Agency Programs Guidelines for Geometrics on Local Agency Projects 2017 Edition Approved Preventive Maintenance Treatments				

1.4 Planned Projects

Taylor plans construction and maintenance projects several years in advance. A multi-year planning threshold is required due to the time necessary to plan, design, and finance construction and maintenance projects on the paved city major road network. This includes planning and programming requirements from state and federal agencies that must be met prior to starting a project and can include studies on environmental and archeological impacts, review of construction and design documents and plans, documentation of rights-of-way ownership, planning and permitting for storm water discharges, and other regulatory and administrative requirements.

Per PA 499 of 2002 (later amended by PA 199 of 2007), road projects for the upcoming three years are required to be reported annually to the TAMC. Planned projects represent the best estimate of future activity; however, changes in design, funding, and permitting may require Taylor to alter initial plans. Project planning information is used to predict the future condition of the road networks that Taylor maintains. **Section 1.3 – Pavement Assets: Modelled Trends** of this plan provides a detailed analysis of the impact of the proposed projects on their respective road networks.

Table 1-2. Planned Projects

Project	FY	Budget
Non-Motorized	2027	\$200,000
Mortenview (Wick to Ecorse)	2026	\$800,000
Byers (Telegraph to Ecorse)	2026	\$400,000
Superior (Pardee to Racho)	2027	\$2,500,000

Project	FY	Budget
Westlake (Eureka to Dead End South)	2025	\$800,000
Lange (Eureka to Northline)	2026	\$800,000

1.5 Gap Analysis

The current funding levels that Taylor receives are not sufficient to meet the goals for the paved city major road network, the paved city local road network, and the unpaved road network. **Section 1.2 - Pavement Assets: Goals** section of this plan provides further detail about the goals and **Section 1.3 - Pavement Assets: Modelled Trends** section provides further detail on the shortfall given the current budget. However, Taylor believes that the overall condition of this network can be maintained or improved with additional funding for construction and maintenance. An alternate strategy may be used to overcome the current shortfall and meet the goals on the paved city major road network, the paved city local road network, and the unpaved road network:

2.0 FINANCIAL RESOURCES

Public entities must balance the quality and extent of services they can provide with the tax resources provided by citizens and businesses, all while maximizing how efficiently funds are used. Taylor will overview its general expenditures and financial resources currently devoted to pavement maintenance and construction. This financial information is not intended to be a full financial disclosure or a formal report. Michigan agencies are required to submit an Act 51 Report to the Michigan Department of Transportation each year; this is a full financial report that outlines revenues and expenditures. This report can be obtained on our website at <https://www.cityoftaylor.com/177/Budgets-Financial-Reports>.

Taylor has a total budget for pavement asset management of \$2,000,000.

2.1 City Major Network

Taylor has historically spent \$1,000,000 annually on pavement-related projects. Over the next three years, Taylor plans to spend \$1,000,000 on city major-network projects consisting of, but not limited to, reconstruction, overlay, culvert replacement, and preventive maintenance. Spending on projects depends on revenue from Michigan Transportation Fund (MTF), bonds, and federal/state programs.

2.2 City Local Network

Taylor has historically spent \$1,000,000 annually on pavement-related projects. Over the next three years, Taylor plans to spend \$1,000,000 on city local-network projects consisting of, but not limited to, reconstruction, overlay, culvert replacement, and preventive maintenance. Spending on projects depends on revenue from Michigan Transportation Fund (MTF), bonds, and federal/state programs. Many local agencies in Michigan use local tax millages to supplement their road-funding budget. These taxes can provide for additional construction and maintenance for new or existing roads that are also funded using MTF or MDOT funds. Taylor does not have local tax millages in its road-funding budget.

3.0 RISK OF FAILURE ANALYSIS

Transportation infrastructure is designed to be resilient. The system of interconnecting roads and bridges maintained by Taylor provides road users with multiple alternate options in the event of an unplanned disruption of one part of the system. There are, however, key links in the transportation system that may cause significant inconvenience to users if they are unexpectedly closed to traffic. Taylor does not have any critical assets.

4.0 COORDINATION WITH OTHER ENTITIES

An asset management plan provides a significant value for infrastructure owners because it serves as a platform to engage other infrastructure owners using the same shared right of way space. Taylor communicates with both public and private infrastructure owners to coordinate work in the following ways:

Taylor maintains drinking water, sanitary and storm sewer assets in addition to transportation assets. Taylor follows an asset management process for all of its assets by coordinating the upgrade, maintenance, and operation of all major assets.

Planned projects for subsurface infrastructure that Taylor owns are coordinated with the transportation infrastructure plans to maximize value and minimize service disruptions and cost to the public.

Taylor takes advantage of coordinated infrastructure work to reduce cost and maximize value using the following policies:

- Roads which are in poor condition that have a subsurface infrastructure project planned which will destroy more than half the lane width will be rehabilitated or reconstructed full width using transportation funds to repair the balance of the road width.
- Subsurface infrastructure projects which will cause damage to pavements in good condition will be delayed as long as possible, or will consider methods that do not require pavement cuts.
- Subsurface utility projects will be coordinated to allow all under pavement assets to be upgraded in the same project regardless of ownership.
- Road reconstruction projects will not be completed until agency owned sub surface utilities are upgraded to have at least a 40 years of remaining service life.



**Appendix A1.
A Quick Check of Your Highway Network Health**

A Quick Check of Your Highway Network Health

*By Larry Galehouse, Director, National Center for Pavement Preservation
and*

Jim Sorenson, Team Leader, FHWA Office of Asset Management

Historically, many highway agency managers and administrators have tended to view their highway systems as simply a collection of projects. By viewing the network in this manner, there is a certain comfort derived from the ability to match pavement actions with their physical/functional needs. However, by only focusing on projects, opportunities for strategically managing entire road networks and asset needs are overlooked. While the “bottom up” approach is analytically possible, managing networks this way can be a daunting prospect. Instead, road agency administrators have tackled the network problem from the “top down” by allocating budgets and resources based on historical estimates of need. Implicit in this approach, is a belief that the allocated resources will be wisely used and prove adequate to achieve desirable network service levels.

Using a quick checkup tool, road agency managers and administrators can assess the needs of their network and other highway assets and determine the adequacy of their resource allocation effort. A quick checkup is readily available and can be usefully applied with minimum calculations.

It is essential to know whether present and planned program actions (reconstruction, rehabilitation, and preservation) will produce a net improvement in the condition of the network. However, before the effects of any planned actions on the highway network can be analyzed, some basic concepts should be considered.

Assume every lane-mile segment of road in the network was rated by the number of years remaining until the end of life (terminal condition). Remember that terminal condition does not mean a failed road. Rather, it is the level of deterioration that management has set as a minimum operating condition for that road or network. Consider the rated result of the current network condition as shown in Figure 1.

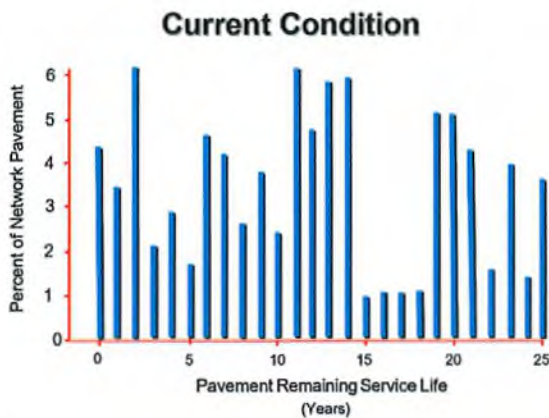


Figure 1 – Current Condition

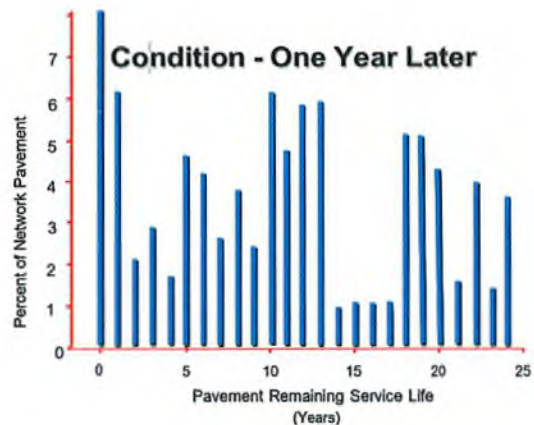


Figure 2 – Condition 1-Year Later

If no improvements are made for one year, then the number of years remaining until the end of life will decrease by one year for each road segment, except for those stacked at zero. The zero- stack will increase significantly because it maintains its previous balance and also becomes the recipient of those roads having previously been stacked with one year remaining. Thus, the entire network will age one year to the condition shown in Figure 2, with the net lane-miles in the zero stack raised from 4% to 8% of the network.

Some highway agencies still subscribe to the old practice of assigning their highest priorities to the reconstruction or rehabilitation of the worst roads. This practice of “worst first”, i.e., continually addressing only those roads in the zero-stack, is a proven death spiral strategy because reconstruction and rehabilitation are the most expensive ways to maintain or restore serviceability. Rarely does sufficient funding exist to sustain such a strategy.

The measurable loss of pavement life can be thought of as the network’s total lane-miles multiplied by 1 year, i.e., lane-mile-years. Consider the following quantitative illustration. Suppose your agency’s highway network consisted of 4,356 lane-miles. Figure 3 shows that without intervention, it will lose 4,356 lane-mile-years per year.

Agency Highway Network = 4,356 lane miles

Each year the network will lose

4,356 lane-mile-years

Figure 3 – Network Lane Miles

To offset this amount of deterioration over the entire network, the agency would need to annually perform a quantity of work equal to the total number of lane-mile-years lost just to maintain the status quo. Performing work which produces fewer than 4,356 lane-mile-years would lessen the natural decline of the overall network, but still fall short of maintaining the

status quo. However, if the agency produces more than 4,356 lane-mile-years, it will improve the network.

In the following example, an agency can easily identify the effect of an annual program consisting of reconstruction, rehabilitation, and preservation projects on its network. This assessment involves knowing the only two components for reconstruction and rehabilitation projects: lane-miles and design life of each project fix. Figure 4 displays the agency's programmed activities for reconstruction and Figure 5 displays it for rehabilitation.

Reconstruction Evaluation

Projects this Year = 2

Project	Design Life	Lane Miles	Lane Mile Years	Lane Mile Cost	Total Cost
No. 1	25 yrs	22	550	\$463,425	\$10,195,350
No. 2	30 yrs	18	540	\$556,110	\$10,009,980
Total =			1,090		\$20,205,330

Figure 4 - Reconstruction

Rehabilitation Evaluation

Projects this Year = 3

Project	Design Life	Lane Miles	Lane Mile Years	Lane Mile Cost	Total Cost
No. 10	18 yrs	22	396	\$263,268	\$5,791,896
No. 11	15 yrs	28	420	\$219,390	\$6,142,920
No. 12	12 yrs	32	384	\$115,848	\$3,707,136
Total =			1,200		\$15,641,952

Figure 5 – Rehabilitation

When evaluating pavement preservation treatments in this analysis, it is appropriate to think in terms of "extended life" rather than design life. The term design life, as used in the reconstruction and rehabilitation tables, relates better to the new pavement's structural adequacy to handle repetitive loadings and environmental factors. This is not the goal of pavement preservation. Each type of treatment/repair has unique benefits that should be targeted to the specific mode of pavement deterioration. This means that life extension depends on factors such as type and severity of distress, traffic volume, environment, etc. Figure 6 exhibits the agency's programmed activities for preservation.

Preservation Evaluation

Project	Life Extension	Lane Miles	Lane Mile Years	Lane Mile Cost	Total Cost
No. 101	2 yrs	12	24	\$2,562	\$30,744
No. 102	3 yrs	22	66	\$7,743	\$170,346
No. 103	5 yrs	26	130	\$13,980	\$363,480
No. 104	7 yrs	16	112	\$29,750	\$476,000
No. 105	10 yrs	8	80	\$54,410	\$435,280
Total =			412		\$1,475,850

Figure 6 – Preservation

To satisfy the needs of its highway network, the agency must accomplish 4,356 lane-mile-years of work per year. The agency's program will derive 1,090 lane-mile-years from reconstruction, 1,200 lane-mile-years from rehabilitation, and 412 lane-mile-years from pavement preservation, for a total of 2,702 lane-mile-years. Thus, these programmed activities fall short of the minimum required to maintain the status quo, and hence would contribute to a net loss in network pavement condition of 1,653 lane-mile-years. The agency's programmed tally is shown in Figure 7.

Network Trend

Programmed Activity	Lane-Mile-Years	Total Cost
Reconstruction	1,090	\$20,205,330
Rehabilitation	1,200	\$15,641,952
Preservation	412	\$1,475,850
Total	2,702	\$37,323,132
Network Needs (Loss)	(-) 4,356	
Deficit =	- 1,654	

Figure 7 – Programmed Tally

This exercise can be performed for any pavement network to benchmark its current trend. Using this approach, it is possible to see how various long-term strategies could be devised and evaluated against a policy objective related to total-network condition.

Once the pavement network is benchmarked, an opportunity exists to correct any shortcomings in the programmed tally. A decision must first be made whether to improve the

network condition or just to maintain the status quo. This is a management decision and system goal.

Continuing with the previous example, a strategy will be proposed to prevent further network deterioration until additional funding is secured.

The first step is to modify the reconstruction and rehabilitation (R&R) programs. An agonizing decision must be made about which projects to defer, eliminate, or phase differently with multi-year activity. In Figure 8, reductions are made in the R&R programs to recover funds for less costly treatments in the pavement preservation program. The result of this decision recovered slightly over \$6 million.

Program Modification

<u>Programmed Activity</u>	<u>Lane-Mile-Years</u>	<u>Cost Savings</u>
Reconstruction <i>31 lane miles</i> (-40 lane-miles)	<i>820</i> (-1,090)	\$5,004,990
Rehabilitation <i>77 lane miles</i> (-82 lane-miles)	<i>1,125</i> (-1,200)	\$1,096,950
Pavement Preservation (84 lane-miles)	(412)	0
Total =	<i>2,357</i> (2,702)	\$6,101,940

Figure 8 – Revised R & R Programs

Modifying the reconstruction and rehabilitation programs has reduced the number of lane-mile-years added to the network from 2,702 to 2,357 lane-mile-years. However, using less costly treatments elsewhere in the network to address roads in better condition will increase the number of lane-mile-years added to the network. A palette of pavement preservation treatments, or mix of fixes, is available to address the network needs at a much lower cost than traditional methods.

Preservation treatments are only suitable if the right treatment is used on the right road at the right time. In Figure 9, the added treatments used include concrete joint resealing, thin hot-mix asphalt (HMA) overlay (≤ 1.5”), microsurfacing, chip seal, and crack seal. By knowing the cost per lane-mile and the treatment life-extension, it is possible to create a new strategy (costing \$36,781,144) that satisfies the network need. In this example, the agency saved in excess of \$500,000 from traditional methods (costing \$37,323,132), while erasing the 1,653 lane-mile-year deficit produced by the initial program tally. Network Strategy

Programmed Activity	Lane Mile Years	Total Cost
Reconstruction (31 lane-miles)	820	\$15,200,340
Rehabilitation (77 lane-miles)	1,125	\$14,545,002
Pavement Preservation (84 lane-miles)	412	\$1,475,850
Concrete Resealing (4 years x 31 lane-miles)	124	\$979,600
Thin HMA Overlay (10 years x 16 lane-miles)	160	\$870,560
Microsurfacing (7 years x 44 lane-miles)	308	\$1,309,000
Chip Seal (5 years x 79 lane-miles)	395	\$1,104,420
Crack Seal (2 years x 506 lane-miles)	1,012	\$1,296,372
Total =	4,356	\$36,781,144

Figure 9 – New Program Tally

In a real-world situation, the highway agency would program its budget to achieve the greatest impact on its network condition. Funds allocated for reconstruction and rehabilitation projects must be viewed as investments in the infrastructure. Conversely, funds directed for preservation projects must be regarded as protecting and preserving past infrastructure investments.

Integrating reconstruction, rehabilitation, and preservation in the proper proportions will substantially improve network conditions for the taxpayer while safeguarding the highway investment.



**Appendix B1.
Meeting Minutes Verifying Plan Acceptance by Governing Body**

TIM WOOLLEY
Mayor

MICHELLE TOCCO
Treasurer

City of Taylor

OFFICE OF THE CITY CLERK

CYNTHIA A. BOWER

23555 GODDARD ROAD
TAYLOR, MICHIGAN 48180-4116

PHONE: (734) 374-1474 FAX: (734) 374-1343

www.cityoftaylor.com

CITY COUNCIL

DOUGLAS A. GEISS
Chairman

JILL BRANDANA
Chairwoman Pro-Tem

TINA DANIELS
CHARLES JOHNSON
LINDSEY ROSE
ANGELA WINTON

At the regular meeting of the Taylor City Council held on January 21, 2025 the following Resolution was adopted:

Motion by: Rose, supported by: Daniels

Resolved: Motion to approve the submission of the 2024 Transportation Asset Management Plan to the State of Michigan, and authorize the execution of the required Proof of Acceptance.
Unanimously carried.


CCR #: 1.27-25

Ayes: Geiss, Brandana, Daniels, Johnson, Rose, Winton

Nays: None

Absent: None

I, Cynthia A. Bower, City Clerk of the City of Taylor, hereby certify that the foregoing is a true and complete copy of a Resolution as adopted by the City Council of the City of Taylor, County of Wayne, Michigan, at a regular meeting held on January 21, 2025 and that said meeting was conducted and public notice of said meeting was given pursuant to and in full compliance with the Open Meetings Act, being Act 267, Public Acts of Michigan, 1976, and that the Minutes of said meeting were kept and will be or have been made available as required by said Act.


Cynthia A. Bower


Date: January 22, 2025



Appendix B. Bridge Safety Inspection Reports

Copies of the 2023 bridge inspection reports are attached for reference purposes.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635		CULVERT SAFETY INSPECTION REPORT		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	

CULVERT INSPECTION KA80

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/02/2024

GENERAL NOTES

Assisted by Adam Budzinski and Blake Cain; NE guardrail end damaged and dates back to 2016. 2'x3'x2'deep sinkhole just north of culvert, along driveway to the east.

NBI INSPECTION

	12/20	12/22	12/24	
1. Culvert Rating (SIA-62)	7	7	7	(12/24) (12/22) (12/20)
2. Channel (SIA-61)	6	6	5	Flow is West to East; Heavy sediment build-up in north and south pipe approx 2ft. Heavy tree branches and garbage build-up at upstream side. Large trees fallen into downstream channel. Heavy brush/small trees growing behind wingwalls at each quadrant. (12/24) Flow is West to East; Heavy sediment build-up in north and south pipe approx 2ft. Heavy tree branches and garbage build-up at upstream side. Large trees fallen into downstream channel. Heavy brush growing behind wingwalls at each quadrant. (12/22) Heavy sediment build-up in north and south pipe ~2ft. Heavy debris build-up at upstream side. Large trees fallen into downstream channel. Due to overnight and current rain, CMP pipes flowing 50% full. (12/20)
3. Scour	6	6	5	Uneven, silty mucky streambed with hole 2 ft deep, at downstream east end of north span pipe. Silt build-up noted at south and north pipe ~ 1ft to 1.5 ft depth. Very mucky bottom from probing. No evidence of undermining of invert. (12/24) Uneven, silty mucky streambed with hole 2 ft deep, at downstream east end of north span pipe. Silt build-up noted at south and north pipe ~ 1ft to 1.5 ft depth. Very mucky bottom from probing. No evidence of undermining of invert. (12/22) Possible scour hole 2 ft deep, near north barrel at downstream end. Silt build-up noted at south and north pipe ~ 1ft to 1.5 ft depth. Very mucky bottom from probing. (12/20)

AASHTO ELEMENTS (English Units)

Element Number	Element Name	Total Quantity	Unit	Good CS1	Fair CS2	Poor CS3	Severe CS4
240	Steel Culvert	44	ft	44 100%	0 0%	0 0%	0 0%
2 span, 2x6 Corrugated steel pipe (8 gage steel) in good condition with staining along water line in each pipe. Silt build-up in both barrels approx 1.5 ft deep. minor rust spots along seam of north barrel. North Culvert has small damage where it meets east headwall.							
861	Culvert Wingwall	4		4 100%	0 0%	0 0%	0 0%
Concrete flared wingwalls in each corner, cast with concrete headwall							
863	Culvert Headwall	2		2 100%	0 0%	0 0%	0 0%


Concrete headwall at each side of structure; minor hairline cracking along vertical construction joint at each headwall. East Headwall has (3) radial cracks around the South barrel, and (5) radial cracks around the north barrel.

MISCELLANEOUS

Guard Rail	Other Items

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635 CULVERT SAFETY INSPECTION REPORT

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
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Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	

<u>Item</u>	<u>Rating</u>	<u>Item</u>	<u>Rating</u>
36A. Bridge Railings	0	71. Water Adequacy	8
36B. Transitions	0	72. Approach Alignment	8
36C. Approach Guardrail	0	Special Insp. Equipment	2
36D. Approach Guardrail Ends	0	Underwater Insp. Method	1

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Channel Repair	M	Remove garbage and heavy debris build-up including large tree branches in waterway at both sides of culvert.
Approach Repair	H	Repair guardrail at NE quadrant
Brush Cut	H	Remove brush and trees growing in between roadway pavement and headwalls and behind wingwalls at each corner - clear brush within ROW at culvert. Remove small trees and bushes growing along guardrail and covering signs for full length of guardrail.
Railing Repair	H	Evaluate approach and departing guardrail design for each side of roadway and replace with guardrail meeting current standards.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	

SUPPORTING IMAGES

KA80 12/02/2024



Document Name: RIMG2869.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north at culvert



Document Name: RIMG2870.JPG
 Category: Approach
 Span Number:
 Comments: South Approach looking south



Document Name: RIMG2875.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south at culvert



Document Name: RIMG2876.JPG
 Category: Approach
 Span Number:
 Comments: North approach looking north

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	



Document Name: RIMG2871.JPG
 Category: Railing
 Span Number:
 Comments: West guardrail



Document Name: RIMG2874.JPG
 Category: Railing
 Span Number:
 Comments: East guardrail



Document Name: RIMG2897.JPG
 Category: Railing
 Span Number:
 Comments: NE guardrail end damaged and dates back to 2016



Document Name: RIMG2883.JPG
 Category: Culvert
 Span Number:
 Comments: North Barrel of Culvert

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG2885.JPG
 Category: Culvert
 Span Number:
 Comments: Damage to North barrel at headwall



Document Name: RIMG2888.JPG
 Category: Culvert
 Span Number:
 Comments: south barrel



Document Name: RIMG2873.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking west



Document Name: RIMG2879.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking east

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)
Feature	Length / Width / Spans	Owner	
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing



Document Name: RIMG2886.JPG
 Category: Channel
 Span Number:
 Comments: scour at north barrel of culvert



Document Name: RIMG2877.JPG
 Category: Other
 Span Number:
 Comments: 2'x3'x2'deep sinkhole just north of culvert, along driveway to the east.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

STRUCTURE INVENTORY AND APPRAISAL

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)
Feature	Length / Width / Spans	Owner	
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing



Bridge History, Type, Materials

27 - Year Built	1965
106 - Year Reconstructed	
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	3 19
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	9
79 - Rail Type	1
80 - Post Type	
107 - Deck Type	N
108A - Wearing Surface	N
108B - Membrane	0
108C - Deck Protection	0

Structure Dimensions

34 - Skew	0
35 - Struct Flared	0
45 - Num Main Spans	2
46 - Num Apprs Spans	0
48 - Max Span Length	17.7
49 - Structure Length	34.8
50A - Width Left Curb/SW	0
50B - Width Right Curb/SW	0
33 - Median	0
51 - Width Curb to Curb	0
52 - Width Out to Out	
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/02/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	1
58 - Deck Rating	N
58A/B - Deck Surface/Bottom	
59 - Superstructure Rating	N
59A - Paint Rating	
60 - Substructure Rating	N
61 - Channel Rating	5
62 - Culvert Rating	7

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	7
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	9.06
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016548 17
19 - Detour Length	0
20 - Toll Facility	3
26 - Functional Class	14
28A - Lanes On	2
29 - ADT	1750
30 - Year of ADT	2006
32 - Appr Roadway Width	36.1
32A/B - Ap Pvt Type/Width	6 35.99
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	44.0
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	1
109 - Truck %	3
110 - Truck Network	0
114 - Future ADT	2013
115 - Year Future ADT	2026
Freeway	0

Structure Appraisal

36A - Bridge Railing	0
36B - Rail Transition	0
36C - Approach Rail	0
36D - Rail Termination	0
67 - Structure Evaluation	7
68 - Deck Geometry	N
69 - Underclearance	N
71 - Waterway Adequacy	8
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	8

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	99.9
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	

Proposed Improvements

75 - Type of Work	31 1
76 - Length of Improvement	260.2
94 - Bridge Cost	224
95 - Roadway Cost	22
96 - Total Cost	263
97 - Year of Cost Estimate	1980

Load Rating and Posting

31 - Design Load	5
41 - Open, Posted, Closed	A
63 - Fed Oper Rtg Method	6
64F - Fed Oper Rtg Load	2.46
64MA - Mich Oper Rtg Method	6
64MB - Mich Oper Rtg	2.46
64MC - Mich Oper Truck	19
65 - Inv Rtg Method	6
66 - Inventory Load	2.43
70 - Posting	5
141 - Posted Loading	
193 - Overload Class	A N

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

WORK RECOMMENDATIONS

Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	


WORK RECOMMENDATIONS

KA80

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

REQUEST FOR ACTION

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	

No inspections available for bridge key 824664382191C05

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		CULVERT SAFETY INSPECTION REPORT		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

CULVERT INSPECTION UH0Y

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024

GENERAL NOTES

Assisted by Adam Budzinski and Blake Cain.

42T/66T/74T load posting signs in place Missing at south approach and also both advance warning signs missing. Guardrail at approach side only.

Weight limit signs in place on both ends of bridge NO

Required advance warning weight limit signs in place NO

Frequency Justification Comments (required when Poor Condition and frequency is equal to 24 months)

Culvert is in overall good condition-structure load posted; inspection frequency is at 24 months

NBI INSPECTION

	12/20	12/22	12/24	
1. Culvert Rating (SIA-62)	7	7	7	(12/24) (12/22) (12/20)
2. Channel (SIA-61)	6	6	6	Heavy sediment accumulating on south side. Channel bottom very silty. Erosion and washout @ SW, NE and SE wingwalls @ end of interface with gabions. Channel at severe skew to road. Culvert skewed to match creek skew (12/24) Heavy sediment accumulating on south side. Channel bottom very silty. Erosion and washout @ SW, NE and SE wingwalls @ end of interface with gabions. Channel at severe skew to road. Culvert skewed to match creek skew (12/22) Heavy sediment accumulating on south side. Channel bottom very silty. Erosion and washout @ SW, NE and SE wingwalls @ end of interface with gabions. Channel at severe skew to road. Culvert skewed to match creek skew (12/20)
3. Scour	7	7	7	No scour, but some loss of concrete at large drain in northeast quadrant. Washout & erosion on NE, SE & SW quads at wingwalls. Gabion baskets along north bank, east of culvert in need of some repair. (12/24) No scour, but some loss of concrete at large drain in northeast quadrant. Washout & erosion on NE, SE & SW quads at wingwalls. Gabion baskets along north bank, east of culvert in need of some repair. (12/22) No scour, but some loss of concrete at large drain in northeast quadrant. Washout & erosion on NE, SE & SW quads at wingwalls. Gabion baskets along north bank, east of culvert in need of some repair. (12/20)

AASHTO ELEMENTS (English Units)

Element Number	Element Name	Total Quantity	Unit	Good CS1	Fair CS2	Poor CS3	Severe CS4
851	Reinf Conc Culvert 3-Sided	33	ft	21 64%	12 36%	0 0%	0 0%

HMA surface hides condition of top of concrete.
Leaking between 2W and 3W
1" settlement between 3W and 4W
3E is displaced 1/4" down
1" gap between beams 3E and 4E
Concrete soffit in good condition with only minor leakage at some joints.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		CULVERT SAFETY INSPECTION REPORT				
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)			
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)				
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)			
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing			

861	Culvert Wingwall	4	4	0	0	0
			100%	0%	0%	0%

Reinforced concrete wingwalls at each corner; wingwalls are angled 90 degrees to fascias at SW and NE corners, 0 degrees-parallel to fascia at NW corners and 136 degrees-parallel to reference line at SE corner; no deficiencies noted;

Scour Countermeasure

830	Plain Riprap	640	sq.ft	480	160	0	0
				75%	25%	0%	0%

Riprap channel protection in place through structure

834	Gabion	100	ft	90	10	0	0
				90%	10%	0%	0%

Gabion Basket retaining walls along downstream banks - approx 50 ft long at each bank; undermining of baskets at NE quadrant adjacent to wingwall with storm outlet;

Other Elements

331	Re Conc Bridge Railing	80	ft	80	0	0	0
				100%	0%	0%	0%

MDOT Type 4 Reinforced Concrete Bridge Railing at each side; no deficiencies noted

MISCELLANEOUS

Guard Rail

Item	Rating
36A. Bridge Railings	1
36B. Transitions	0
36C. Approach Guardrail	0
36D. Approach Guardrail Ends	0

Other Items

Item	Rating
71. Water Adequacy	7
72. Approach Alignment	8
Special Insp. Equipment	2
Underwater Insp. Method	0

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Brush Cut	M	Clear weeds away from barrier railing and wingwalls.
Slope Repair	H	Backfill, geotextile and armor slopes behind all wingwalls with rip rap.
Railing Repair	H	Place departing end guardrails to meet standards.
HMA Overlay	L	Remove existing overlay, reseal culvert joints apply geotextile membrane, add HMA overlay for full clear roadway width; apply concrete surface coating to bridge railings and culvert fascias
Other	H	Add advanced warning load posting sign for SB traffic at SW quadrant of Wick/Mortenview intersection-north of bridge; add advanced warning load posting sign for NB traffic at NE corner of Goddard/Mortenview intersection; move load posting signs near bridge to within 50 ft of SE and NW corners of bridge; Update sign placement and height configuration per MMUTCD.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		CULVERT SAFETY INSPECTION REPORT		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

SUPPORTING IMAGES UH0Y 12/03/2024



Document Name: RIMG3068.JPG
 Category: Elevation
 Span Number:
 Comments: east fascia



Document Name: RIMG3080.JPG
 Category: Elevation
 Span Number:
 Comments: west fascia



Document Name: RIMG3060.JPG
 Category: Posting
 Span Number:
 Comments: 42T/66T/74T load posting sign at north approach



Document Name: RIMG3049.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)	Operational Status B Posting Recommended(B)	
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)		Scour Evaluation 8 Stable Above Footing
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y		



Document Name: RIMG3050.JPG
 Category: Approach
 Span Number:
 Comments: south approach looking south



Document Name: RIMG3058.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south



Document Name: RIMG3059.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking north



Document Name: RIMG3052.JPG
 Category: Railing
 Span Number:
 Comments: West railing

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG3062.JPG
 Category: Railing
 Span Number:
 Comments: East railing



Document Name: RIMG3070.JPG
 Category: Culvert
 Span Number:
 Comments: 3E is displaced 1/4" down



Document Name: RIMG3071.JPG
 Category: Culvert
 Span Number:
 Comments: 3E is displaced 1/4" down
 1" gap between beams 3E and 4E



Document Name: RIMG3073.JPG
 Category: Culvert
 Span Number:
 Comments: 1" gap between beams 3E and 4E

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG3074.JPG
 Category: Culvert
 Span Number:
 Comments: 1" gap between beams 3E and 4E



Document Name: RIMG3075.JPG
 Category: Culvert
 Span Number:
 Comments: south view of culvert



Document Name: RIMG3076.JPG
 Category: Culvert
 Span Number:
 Comments: north view of culvert



Document Name: RIMG3078.JPG
 Category: Culvert
 Span Number:
 Comments: Leaking between 2W and 3W
 1" settlement between 3W and 4W

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UHOY	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG3057.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking west



Document Name: RIMG3065.JPG
 Category: Channel
 Span Number:
 Comments: channel looking east



Document Name: RIMG3076.JPG
 Category:
 Span Number:
 Comments:

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

STRUCTURE INVENTORY AND APPRAISAL

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

Bridge History, Type, Materials	
27 - Year Built	1999
106 - Year Reconstructed	
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	1 19
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	0
79 - Rail Type	8
80 - Post Type	
107 - Deck Type	N
108A - Wearing Surface	N
108B - Membrane	N
108C - Deck Protection	N

Structure Dimensions	
34 - Skew	45
35 - Struct Flared	N
45 - Num Main Spans	1
46 - Num Apprs Spans	0
48 - Max Span Length	40
49 - Structure Length	40
50A - Width Left Curb/SW	0
50B - Width Right Curb/SW	0
33 - Median	0
51 - Width Curb to Curb	0
52 - Width Out to Out	32.8
112 - NBIS Length	Y

Inspection Data	
90 - Inspection Date	12/03/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	0
58 - Deck Rating	N
58A/B - Deck Surface/Bottom	
59 - Superstructure Rating	N
59A - Paint Rating	
60 - Substructure Rating	N
61 - Channel Rating	6
62 - Culvert Rating	7

Navigation Data	
38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brgd Vert Clear	

Route Carried By Structure(ON Record)	
5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	0
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	0
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016566 10
19 - Detour Length	4
20 - Toll Facility	3
26 - Functional Class	19
28A - Lanes On	2
29 - ADT	3935
30 - Year of ADT	2000
32 - Appr Roadway Width	29.9
32A/B - Ap Pvt Type/Width	0 29.99
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	39.4
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	2
109 - Truck %	15
110 - Truck Network	0
114 - Future ADT	5847
115 - Year Future ADT	2018
Freeway	0

Structure Appraisal	
36A - Bridge Railing	1
36B - Rail Transition	0
36C - Approach Rail	0
36D - Rail Termination	0
67 - Structure Evaluation	6
68 - Deck Geometry	N
69 - Underclearance	N
71 - Waterway Adequacy	7
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	8



Miscellaneous	
37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)	
5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	99.9
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	

Proposed Improvements	
75 - Type of Work	
76 - Length of Improvement	
94 - Bridge Cost	
95 - Roadway Cost	
96 - Total Cost	
97 - Year of Cost Estimate	



Load Rating and Posting	
31 - Design Load	5
41 - Open, Posted, Closed	B
63 - Fed Oper Rtg Method	6
64F - Fed Oper Rtg Load	1.25
64MA - Mich Oper Rtg Method	6
64MB - Mich Oper Rtg	.96
64MC - Mich Oper Truck	17
65 - Inv Rtg Method	6
66 - Inventory Load	.76
70 - Posting	4
141 - Posted Loading	426782
193 - Overload Class	D N

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		WORK RECOMMENDATIONS		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	


WORK RECOMMENDATIONS **UH0Y**

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		REQUEST FOR ACTION		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

No inspections available for bridge key 824664300025B01

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		BRIDGE SAFETY INSPECTION REPORT		
Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Brn/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	

NBI INSPECTION **G118**

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024


GENERAL NOTES

Assisted by Adam Budzinski and Blake Cain Residential street.

DECK

	12/20	12/22	12/24	
1. Surface (SIA-58A)	7	7	7	Tined concrete surface. 3 full length longitudinal cracks and rusting bleeding throughout deck surface. (12/24) Tined concrete surface with isolated longitudinal cracks and rusting bleeding throughout deck surface. (12/22) Tined concrete surface with isolated longitudinal cracks and rusting bleeding throughout deck surface. (12/20)
2. Expansion Joints		6	6	North & south seals cracking full length of sealant. Some loss of material near gutter line and at midspan. (12/24) North & south seals cracking full length of sealant. Some loss of material near gutter line. (12/22) (12/20)
3. Other Joints	6	N	N	N/A; Moved to expansion-end joints rating (12/24) N/A; Moved to expansion-end joints rating (12/22) North & south seals cracking full length of sealant. Some loss of material near gutter line. (12/20)
4. Railings	7	7	7	Intermittent vertical cracks along both railings, due to temp and shrinkage and very fine map cracking. Minor alligator cracking both concrete rails. No spalling. Minor cracking occurring at NW railing block where tubes fasten to concrete (12/24) Intermittent vertical cracks along both railings, due to temp and shrinkage and very fine map cracking. Minor alligator cracking both concrete rails. No spalling. Minor cracking occurring at NW railing block where tubes fasten to concrete (12/22) Intermittent vertical cracks along both railings, due to temp and shrinkage and very fine map cracking. Minor alligator cracking both concrete rails. No spalling. Minor cracking occurring at NW railing block where tubes fasten to concrete (12/20)
5. Sidewalks or Curbs	7	7	7	Minor wear along east sidewalk; trip hazards present due to settled approach sidewalk at each corner (12/24) Minor wear along east sidewalk; trip hazards present due to settled approach sidewalk at each corner (12/22) Minor wear along east side. (12/20)
6. Deck Bottom Surface (SIA-58B)	7	N	N	Deck bottom surface cannot be observed; Side by side prestressed box beams with grouted joints in place; No noted grout loss at joints. (12/24) Deck bottom surface cannot be observed; Side by side prestressed box beams with grouted joints in place; No noted grout loss at joints. (12/22) No Noted Deficiencies; box beams with grouted joints. No noted grout loss at joints. (12/20)
7. Deck (SIA-58)	7	7	7	Tined concrete surface. 3 full length longitudinal cracks and rusting bleeding throughout deck surface. Fascia no noted deficiencies, Bottom surface not visible-SBS box beams. (12/24) Surface-small tight cracks, minor rusting is leaching. Fascia no noted deficiencies, Bottom surface not visible-SBS box beams. (12/22) Surface-small tight cracks, minor rusting is leaching. Fascia no noted deficiencies, Bottom-SBS box beams. (12/20)

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		BRIDGE SAFETY INSPECTION REPORT		
Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	

8. Drainage Runoff to curb and gutter with Bridge at Crest Curve. (12/24)
Runoff to curb and gutter with Bridge at Crest Curve. (12/22)
Runoff to curb and gutter with Bridge at Crest Curve. With rain at time, some ponding showing at bridge/approach interface. No spread into lanes. (12/20)


SUPERSTRUCTURE

	12/20	12/22	12/24	
9. Stringer (SIA-59)	8	8	8	6 - 12"x73.5" adjacent prestressed concrete box beams. No noted deficiencies. (12/24) 6 - 12"x73.5" adjacent prestressed concrete box beams. No noted deficiencies. (12/22) Side-by-side box beams. No noted problems (12/20)
10. Paint (SIA-59A)	N	N	N	(12/24) (12/22) (12/20)
11. Section Loss	N	3	3	No deterioration or leakage through joints along box beam pavement seats at each end (12/24) No deterioration or leakage through joints along box beam pavement seats at each end (12/22) (12/20)
12. Bearings	7	7	7	Elastomeric pads, some movement of joint filler between bearings. Fascia beam exterior edges and bottom of deck past fascias not allowed to slide. Hairline cracks forming in concrete - monitor. (12/24) Elastomeric pads, some movement of joint filler between bearings. Fascia beam exterior edges and bottom of deck past fascias not allowed to slide. Hairline cracks forming in concrete - monitor. (12/22) Elastomeric pads, some movement of joint filler between bearings. Fascia beam exterior edges and bottom of deck past fascias not allowed to slide. Hairline cracks forming in concrete - monitor. (12/20)

SUBSTRUCTURE

	12/20	12/22	12/24	
13. Abutments (SIA-60)	7	7	6	Reinf Conc Solid Wall abutments with reconstructed beam seat; Vertical cracks at interface of wingwall and abutment. Vertical cracking at all four quadrants. SW quad has spall/crack (12/24) Reinf Conc Solid Wall abutments with reconstructed beam seat; Minor vertical cracks at interface of wingwall and abutment. Vertical cracking at all four quadrants. (12/22) Top of abutments were reconstructed for new beam seats with patching also included. Minor vertical cracks at interface of wingwall and abutment. Vertical cracking at all four quadrants. (12/20)
14. Piers (SIA-60)	N	N	N	N/A (12/24) N/A (12/22) (12/20)
15. Slope Protection	7	N	N	N/A; Bridge over waterway (12/24) N/A; Bridge over waterway (12/22) Riprap on slopes against wingwalls, along north bank. Some minor movement of riprap. South side riprap is more sporadic. (12/20)

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		BRIDGE SAFETY INSPECTION REPORT		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)	
Feature	Length / Width / Spans	Owner		
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures	

16. Channel (SIA-61)	8	7	7	Channel flows West to East; Riprap channel protection in place from Abutment to Abutment; Stream flow runs under south half of bridge against South abutment; Some minor movement of riprap. Bank erosion in northeast beyond riprap. (12/24) Channel flows West to East; Riprap channel protection in place from Abutment to Abutment; Stream flow runs under south half of bridge against South abutment; Some minor movement of riprap. Bank erosion in northeast beyond riprap. (12/22) Riprap added to channel, along south abutment and up slope to face of north abutment. Some minor movement of riprap. Stream flow is towards south half of streambed up to south abutment Bank erosion in northeast quadrant past riprap Should relocate some riprap to SW quadrant. (12/20)
17. Scour Inspection	7	7	7	No noted scour. Riprap is stable. (12/24) No noted scour. Riprap is stable. (12/22) No noted scour. Rip rap is stable. (12/20)

APPROACH

	12/20	12/22	12/24	
18. Approach Pavement	7	7	7	Concrete pavement with no significant problems. Some minor cracking. Previous concrete patching repairs done. (12/24) Concrete pavement with no significant problems. Some minor cracking. Previous concrete patching repairs done. (12/22) Concrete pavement with no significant problems. Some minor cracking. Previous concrete patching repairs done. (12/20)
19. Approach Shoulders Sidewalks	7	6	4	Concrete sidewalks at each quadrant. Settlement with potential trip hazard at each corner at first approach sidewalk section against bridge sidewalk. Settlement is 1 1/4" SW quad; 3/4" at NW quad; 3/4" at SE quad; 1" at NE quad (12/24) Concrete sidewalks at each quadrant. Settlement with potential trip hazard at each corner at first approach sidewalk section against bridge sidewalk. Settlement is 1 1/4" SW quad; 3/4" at NW quad; 3/4" at SE quad; 1" at NE quad (12/22) Concrete sidewalks at each quadrant. Settlement of 1/2" at SE, NE and NW quads. 3/4" at interface of SW sidewalk with bridge walk. Possible trip hazard. (12/20)
20. Approach Slopes				Flat lawns. (12/24) Flat lawns. (12/22) Flat lawns. (12/20)
21. Utilities				Buried gas line along west side of bridge. (12/24) Buried gas line along west side of bridge. (12/22) Buried gas line along west side of bridge. (12/20)
22. Drainage Culverts				(12/24) (12/22) (12/20)

MISCELLANEOUS

Guard Rail		Other Items	
Item	Rating	Item	Rating
36A. Bridge Railings	1	71. Water Adequacy	8
36B. Transitions	1	72. Approach Alignment	8
36C. Approach Guardrail	1	Temporary Support	0 No Temporary Supports
36D. Approach Guardrail Ends	1	High Load Hit (M)	No
		Special Insp. Equipment	2
		Underwater Insp. Method	1
False Decking (Timber) Removed to Complete Inspection		N/A - No False Decking	

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	

Critical Feature Inspections (SIA-92)

	<u>Freq</u>	<u>Date</u>
92A. Fracture Critical		
92B. Underwater		
92C. Other Special		
92D. Fatigue Sensitive		

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)	
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Operational Status A Open, no restriction(A)
			Scour Evaluation 7 Countermeasures

SUPPORTING IMAGES

G118 12/03/2024



Document Name: RIMG3031.JPG
Category: Elevation
Span Number:
Comments: east fascia



Document Name: RIMG3041.JPG
Category: Elevation
Span Number:
Comments: west fascia



Document Name: RIMG3005.JPG
Category: Approach
Span Number:
Comments: South approach looking north



Document Name: RIMG3006.JPG
Category: Approach
Span Number:
Comments: south approach looking south

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3016.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south



Document Name: RIMG3017.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking north



Document Name: RIMG3046.JPG
 Category: Approach
 Span Number:
 Comments: typical view of trip hazard at sidewalk approaches



Document Name: RIMG3011.JPG
 Category: Railing
 Span Number:
 Comments: West railing

MICHIGAN DEPARTMENT OF TRANSPORTATION

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BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)	
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Operational Status A Open, no restriction(A)
			Scour Evaluation 7 Countermeasures



Document Name: RIMG3023.JPG
 Category: Railing
 Span Number:
 Comments: East railing



Document Name: RIMG3022.JPG
 Category: Deck
 Span Number:
 Comments: Typical view of deck



Document Name: RIMG3010.JPG
 Category: Joints
 Span Number:
 Comments: West joint



Document Name: RIMG3020.JPG
 Category: Joints
 Span Number:
 Comments: East joint

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3021.JPG
 Category: Joints
 Span Number:
 Comments: Missing sealant at mid-span in joint



Document Name: RIMG3035.JPG
 Category: Superstructure
 Span Number:
 Comments: Box beams/deck bottom



Document Name: RIMG3033.JPG
 Category: Substructure
 Span Number:
 Comments: South abutment



Document Name: RIMG3037.JPG
 Category: Substructure
 Span Number:
 Comments: North abutment

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3042.JPG
 Category: Substructure
 Span Number:
 Comments: Vertical cracks at interface of wingwall and abutment



Document Name: RIMG3044.JPG
 Category: Substructure
 Span Number:
 Comments: SW quad has spall/crack



Document Name: RIMG3012.JPG
 Category: Channel
 Span Number:
 Comments: view of channel looking west



Document Name: RIMG3038.JPG
 Category: Channel
 Span Number:
 Comments: view of channel looking east

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3009.JPG
 Category: Other
 Span Number:
 Comments: bridge plaque

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

STRUCTURE INVENTORY AND APPRAISAL

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)
Feature	Length / Width / Spans	Owner	
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovlv.	TSC	Operational Status
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures



Bridge History, Type, Materials

27 - Year Built	1970
106 - Year Reconstructed	2007
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	5 05
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	0
79 - Rail Type	7
80 - Post Type	
107 - Deck Type	1
108A - Wearing Surface	1
108B - Membrane	0
108C - Deck Protection	1

Structure Dimensions

34 - Skew	14
35 - Struct Flared	N
45 - Num Main Spans	1
46 - Num Apprs Spans	0
48 - Max Span Length	28.9
49 - Structure Length	31.9
50A - Width Left Curb/SW	5.2
50B - Width Right Curb/SW	4.9
33 - Median	0
51 - Width Curb to Curb	26.9
52 - Width Out to Out	36.9
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/03/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	1
58 - Deck Rating	7
58A/B - Deck Surface/Bottom	7 N
59 - Superstructure Rating	8
59A - Paint Rating	N
60 - Substructure Rating	6
61 - Channel Rating	7
62 - Culvert Rating	N

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	0
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	0
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016581 06
19 - Detour Length	2
20 - Toll Facility	3
26 - Functional Class	19
28A - Lanes On	2
29 - ADT	850
30 - Year of ADT	2006
32 - Appr Roadway Width	25.9
32A/B - Ap Pvt Type/Width	6 24.02
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	26.2
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	2
109 - Truck %	1
110 - Truck Network	0
114 - Future ADT	1100
115 - Year Future ADT	2026
Freeway	0

Structure Appraisal

36A - Bridge Railing	1
36B - Rail Transition	1
36C - Approach Rail	1
36D - Rail Termination	1
67 - Structure Evaluation	7
68 - Deck Geometry	5
69 - Underclearance	N
71 - Waterway Adequacy	8
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	7

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	99.9
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	


Proposed Improvements

75 - Type of Work	
76 - Length of Improvement	
94 - Bridge Cost	
95 - Roadway Cost	
96 - Total Cost	
97 - Year of Cost Estimate	

Load Rating and Posting

31 - Design Load	9
41 - Open, Posted, Closed	A
63 - Fed Oper Rtg Method	6
64F - Fed Oper Rtg Load	3.35
64MA - Mich Oper Rtg Method	6
64MB - Mich Oper Rtg	2.24
64MC - Mich Oper Truck	18
65 - Inv Rtg Method	6
66 - Inventory Load	2.01
70 - Posting	5
141 - Posted Loading	
193 - Overload Class	

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		WORK RECOMMENDATIONS		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)	
Feature	Length / Width / Spans	Owner		
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures	


WORK RECOMMENDATIONS **G118**

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Approach Repair	H	Re-seal end joints and cracks beyond new approach pavement and repair approach sidewalks to remove trip hazards at each corner
Super Repair	M	Penetrating healer sealer on deck surface to seal cracks.
Substr Repair	M	Repair cracks at wingwalls and abutment interface. Remove brush at northeast corner

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		REQUEST FOR ACTION		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)	
Feature	Length / Width / Spans	Owner		
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures	

No inspections available for bridge key 824664300701B01

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978		CULVERT SAFETY INSPECTION REPORT		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
BEECH DALY ROAD	42.1839 / -83.2868	824664300030B01	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
BLAKELY DRAIN	41 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
CITY OF TAYLOR	2004 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	1 Concrete / 19 Culvert	12/03/2024 / 28HE	5 Stable w/in footing	

CULVERT INSPECTION 28HE

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024

GENERAL NOTES

Assisted by Blake Cain
SW guardrail impact attenuator has impact damage.

NBI INSPECTION

	12/20	12/22	12/24	
1. Culvert Rating (SIA-62)	7	7	7	(12/24) (12/22) (12/20)
2. Channel (SIA-61)	7	7	5	Dam full width of channel under culvert, approx 9' from east end. Trees and brush covering channel banks. Riprap channel protection in place around each return wall. Debris both US and DS in waterway including large trees NE Quad - Heavy weeds/trees. (12/24) Trees and brush covering channel banks. Riprap channel protection in place around each return wall. Debris both US and DS in waterway including large trees NE Quad - Heavy weeds/trees. (12/22) Trees and brush covering channel banks. Debris both US and DS in waterway including large trees NE Quad - Heavy weeds/trees. (12/20)
3. Scour	8	8	8	Some rip rap movement but no scour found. Temp sheeting left-in-place holding up well to protect against scour. South culvert wall riprap is sporadic. (12/24) Some rip rap movement but no scour found. Temp sheeting left-in-place holding up well to protect against scour. South culvert wall riprap is sporadic. (12/22) Some rip rap movement but no scour found. Temp sheeting left-in-place holding up well to protect against scour. South culvert wall riprap is sporadic. (12/20)


AASHTO ELEMENTS (English Units)

Element Number	Element Name	Total Quantity	Unit	Good CS1	Fair CS2	Poor CS3	Severe CS4
Culvert							
851	Reinf Conc Culvert 3-Sided	72	ft	72 100%	0 0%	0 0%	0 0%
38 ft span x 7 ft rise culvert 3-sided hyspan culvert structure x 11 sections; Minor leakage evident in culvert joints with no noted movements; Reflective cracking along culvert joints evident on deck. Staining along joints and along vertical legs at each weep hole; Concrete pavement surface with no earth fill-previous patching along end joints ~8sf; South end joint missing 80% of joint length							
857	Culvert Joints	10		0 0%	10 100%	0 0%	0 0%
10 joints with minor leakage evident; no differential between sections							
861	Culvert Wingwall	4		4 100%	0 0%	0 0%	0 0%
Conc capped steel sheet pile wingwalls at each corner; no deterioration evident; heavy brush growing between guardrail and return wall at each corner							

Scour Countermeasure

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978 CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	

830	Plain Riprap	960	sq.ft	860	100	0	0
				90%	10%	0%	0%

Approx 6x80 x 2 sides; minor movement along each side; areas of channel aggradation with riprap buried

836	Sheet Piling	160	ft	160	0	0	0
				100%	0%	0%	0%

Existing steel sheet piling left in place from original bridge; Top 6 inches visible in few locations along each end of bridge

Other Elements

330	Metal Bridge Railing	60	ft	60	0	0	0
				100%	0%	0%	0%

Metal 2-tube railing attached to posts and end walls; Bridge Railing, Aesthetic Parapet Tube attached to concrete slab at each fascia; Galvanized Coating in good condition at each railing (Galv 2.2 sft per ft)

515	Steel Protective Coating	130	sq.ft	130	0	0	0
				100%	0%	0%	0%

331	Re Conc Bridge Railing	82	ft	78	0	4	0
				95%	0%	5%	0%

Bridge Railing, Aesthetic Parapet Tube attached to concrete slab at each fascia; Minor hairline cracks in bridge railing along each side; 4 ft long diagonal through crack at Northwest end wall-at metal railing anchorage point with concrete end wall

840	Reinf Conc Sidewalk	588	sq.ft	588	0	0	0
				100%	0%	0%	0%

7'-2" wide sidewalks at each side x 41'-0" length; debris and dirt build up along toe of railing at each sidewalk; minor scaling along curb each sidewalk.

MISCELLANEOUS

Guard Rail

Item	Rating
36A. Bridge Railings	1
36B. Transitions	1
36C. Approach Guardrail	1
36D. Approach Guardrail Ends	0

Other Items

Item	Rating
71. Water Adequacy	8
72. Approach Alignment	8
Special Insp. Equipment	2
Underwater Insp. Method	0

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Channel Repair	H	Remove dam from under culvert that is restricting flow
Approach Repair	M	Repair NW and SW guardrail end terminals from previous accidents.
Brush Cut	M	Cut weeds down at each side of bridge, and along lengths of guardrail all quadrants.
Slope Repair	M	Replace riprap along north bank where geotextile fabric is exposed.
Bridge Washing	M	Wash approach and bridge sidewalks of debris and gravel build up at each side of road.
Joint Repair	M	Fill end joints and patch concrete pavement at each end of bridge; Fill joints between culvert sections to prevent leakage and joint degradation; coordinate with healer sealer application
Super Repair	M	Penetrating healer sealer for deck-pavement surface

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978

CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	

SUPPORTING IMAGES

28HE 12/03/2024



Document Name: RIMG2986.JPG
 Category: Elevation
 Span Number:
 Comments: West fascia



Document Name: RIMG2999.JPG
 Category: Elevation
 Span Number:
 Comments: East Fascia



Document Name: RIMG2966.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north



Document Name: RIMG2967.JPG
 Category: Approach
 Span Number:
 Comments: south approach looking south

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CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
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Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2976.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south



Document Name: RIMG2977.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking north



Document Name: RIMG2970.JPG
 Category: Railing
 Span Number:
 Comments: West railing



Document Name: RIMG2979.JPG
 Category: Railing
 Span Number:
 Comments: East railing

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STR 12978 CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2981.JPG
 Category: Railing
 Span Number:
 Comments: 4 ft long diagonal through crack at Northwest end wall- at metal railing anchorage point with concrete end wall



Document Name: RIMG3004.JPG
 Category: Railing
 Span Number:
 Comments: SW guardrail impact attenuator damage.



Document Name: RIMG2967.JPG
 Category: Deck
 Span Number:
 Comments: typical view of deck



Document Name: RIMG2971.JPG
 Category: Joints
 Span Number:
 Comments: West joint

MICHIGAN DEPARTMENT OF TRANSPORTATION

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CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2980.JPG
 Category: Joints
 Span Number:
 Comments: East joint



Document Name: RIMG2992.JPG
 Category: Culvert
 Span Number:
 Comments: view of culvert, south



Document Name: RIMG2994.JPG
 Category: Culvert
 Span Number:
 Comments: view of culvert, north



Document Name: RIMG2982.JPG
 Category: Channel
 Span Number:
 Comments: channel looking east

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CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2985.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking west



Document Name: RIMG2995.JPG
 Category: Channel
 Span Number:
 Comments: View of dam under culvert



Document Name: RIMG2996.JPG
 Category: Channel
 Span Number:
 Comments: view of dam blocking channel



Document Name: RIMG2968.JPG
 Category: Other
 Span Number:
 Comments: bridge plaque

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STRUCTURE INVENTORY AND APPRAISAL

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
BEECH DALY ROAD	42.1839 / -83.2868	824664300030B01	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
BLAKELY DRAIN	41 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
CITY OF TAYLOR	2004 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	1 Concrete / 19 Culvert	12/03/2024 / 28HE	5 Stable w/in footing	

Bridge History, Type, Materials

27 - Year Built	2004
106 - Year Reconstructed	
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	1 19
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	0
79 - Rail Type	7
80 - Post Type	
107 - Deck Type	2
108A - Wearing Surface	1
108B - Membrane	0
108C - Deck Protection	1

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	0
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016929 03
19 - Detour Length	2
20 - Toll Facility	3
26 - Functional Class	16
28A - Lanes On	4
29 - ADT	6100
30 - Year of ADT	2003
32 - Appr Roadway Width	60
32A/B - Ap Pvt Type/Width	6 60.01
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	62.0
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	2
109 - Truck %	0
110 - Truck Network	0
114 - Future ADT	8200
115 - Year Future ADT	2023
Freeway	0

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	0
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	

Structure Dimensions

34 - Skew	0
35 - Struct Flared	0
45 - Num Main Spans	1
46 - Num Apprs Spans	0
48 - Max Span Length	38
49 - Structure Length	41
50A - Width Left Curb/SW	6
50B - Width Right Curb/SW	6
33 - Median	0
51 - Width Curb to Curb	0
52 - Width Out to Out	
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/03/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	0
58 - Deck Rating	N
58A/B - Deck Surface/Bottom	
59 - Superstructure Rating	N
59A - Paint Rating	
60 - Substructure Rating	N
61 - Channel Rating	5
62 - Culvert Rating	7

Structure Appraisal

36A - Bridge Railing	1
36B - Rail Transition	1
36C - Approach Rail	1
36D - Rail Termination	0
67 - Structure Evaluation	7
68 - Deck Geometry	N
69 - Underclearance	N
71 - Waterway Adequacy	8
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	5

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	1
148 - No. of Pin & Hangers	

Proposed Improvements

75 - Type of Work	
76 - Length of Improvement	
94 - Bridge Cost	
95 - Roadway Cost	
96 - Total Cost	
97 - Year of Cost Estimate	

Load Rating and Posting

31 - Design Load	9
41 - Open, Posted, Closed	A
63 - Fed Oper Rtg Method	1
64F - Fed Oper Rtg Load	66.8
64MA - Mich Oper Rtg Method	
64MB - Mich Oper Rtg	109
64MC - Mich Oper Truck	
65 - Inv Rtg Method	1
66 - Inventory Load	40.1
70 - Posting	5
141 - Posted Loading	
193 - Overload Class	

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

MICHIGAN DEPARTMENT OF TRANSPORTATION

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WORK RECOMMENDATIONS

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	

WORK RECOMMENDATIONS

28HE

MICHIGAN DEPARTMENT OF TRANSPORTATION

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REQUEST FOR ACTION

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
BEECH DALY ROAD	42.1839 / -83.2868	824664300030B01	Good Condition(7)
Feature	Length / Width / Spans	Owner	
BLAKELY DRAIN	41 /	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status
CITY OF TAYLOR	2004 / / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	1 Concrete / 19 Culvert	12/03/2024 / 28HE	5 Stable w/in footing



No inspections available for bridge key 824664300030B01



**Appendix C.
Glossary & Acronyms**

Glossary

Alligator Cracking: Cracking of the surface layer of an asphalt pavement that creates a pattern of interconnected cracks resembling alligator hide. This is often due to overloading a pavement, sub-base failure, or poor drainage.⁵

Asset Management: A process that uses data to manage and track road assets in a cost-effective manner using a combination of engineering and business principles. Public Act 325 of 2018 provides a legal definition: “an ongoing process of maintaining, preserving, upgrading, and operating physical assets cost effectively, based on a continuous physical inventory and condition assessment and investment to achieve established performance goals”.⁶

Biennial Inspection: Inspection of an agency's bridges every other year, which happens in accordance with National Bridge Inspection Standards and Michigan Department of Transportation requirements.

Bridge Inspection Program: A program implemented by a local agency to inspect the bridges within its jurisdiction systematically in order to ensure proper functioning and structural soundness.

Capital Preventative Maintenance: Also known as CPM, a planned set of cost-effective treatments to address of fair-rated infrastructure before the structural integrity of the system has been severely impacted. These treatments aim to slow deterioration and to maintain or improve the functional condition of the system without significantly increasing the structural capacity. Light capital preventive maintenance is a set of treatments designed to seal isolated areas of the pavement from water, such as crack and joint sealing, to protect and restore pavement surface from oxidation with limited surface thickness material, such as fog seal; generally, application of a light CPM treatment does not provide a corresponding increase in a segment's PASER score. Heavy capital preventive maintenance is a set of surface treatments designed to protect pavement from water intrusion or environmental weathering without adding significant structural strength, such as slurry seal, chip seal, or thin (less than 1.5-inch) overlays for bituminous surfaces or patching or partial-depth (less than 1/3 of pavement depth) repair for concrete surfaces.

Chip Seal: An asphalt pavement treatment method consisting of, first, spraying liquid asphalt onto the old pavement surface and, then, a single layer of small stone chips spread onto the wet asphalt layer.

City Major: A road classification, defined in Michigan Public Act 51, that encompasses the generally more important roads in a city or village. City major roads are designated by a municipality's governing body and are subject to approval by the State Transportation Commission. These roads do not include roads under the jurisdiction of a county road commission or trunkline highways.

City Minor: A road classification, defined in Michigan Public Act 51, that encompasses the generally less important roads in a city or village. These roads include all city or village roads that are not city major road and do not include roads under the jurisdiction of a county road commission.

Composite Pavement: A pavement consisting of concrete and asphalt layers. Typically, composite pavements are old concrete pavements that were overlaid with HMA in order to gain more service life.

⁵ https://en.wikipedia.org/wiki/Crocodile_cracking

⁶ Inventory-based Rating System for Gravel Roads: Training Manual

Concrete Joint Resealing: Resealing the joints of a concrete pavement with a flexible sealant to prevent moisture and debris from entering the joints. When debris becomes lodged inside a joint, it inhibits proper movement of the pavement and leads to joint deterioration and spalling.

Concrete Pavement: Also known as rigid pavement, a pavement made from Portland Cement concrete. Concrete pavement has an average service life of 30 years and typically does not require as much periodic maintenance as HMA.

Cost per Lane Mile: Associated cost of construction, measured on a per lane, per mile basis. Also see *lane-mile segment*.

County Local: A road classification, defined in Michigan Public Act 51, that encompasses the generally less important and low-traffic roads in a county. This includes all county roads that are not classified as county primary roads.

County Primary: A road classification, defined in Michigan Public Act 51, that encompasses the generally more important and high-traffic roads in a county. County primary roads are designated by board members of the county road commissions and are subject to approval by the State Transportation Commission.

CPM: See *Capital preventive maintenance*.

Crack and Seat: A concrete pavement treatment method that involves breaking old concrete pavement into small chunks and leaving the broken pavement in place to provide a base for a new surface. This provides a new wear surface that resists water infiltration and helps prevent damaged concrete from reflecting up to the new surface.

Crack Seal: A pavement treatment method for both asphalt and concrete pavements that fills cracks with asphalt materials, which seals out water and debris and slows down the deterioration of the pavement. Crack seal may encompass the term "crack filling".

Crush and Shape: An asphalt pavement treatment method that involves pulverizing the existing asphalt pavement and base and then reshaping the road surface to correct imperfections in the road's profile. Often, a layer of gravel is added along with a new wearing surface such as an HMA overlay or chip seal

Crust: A very tightly compacted surface on an unpaved road that sheds water with ease but takes time to be created.

Culvert: A pipe or structure used under a roadway that allows cross-road drainage while allowing traffic to pass without being impeded; culverts span up to 20 feet.⁷

Dowel Bar Retrofit Repair: A concrete pavement treatment method that involves cutting slots in a cracked concrete slab, inserting steel bars into the slots, and placing concrete to cover the new bars and fill the slots. It aims to reinforce cracks in a concrete pavement.

Dust Control: A gravel road surface treatment method that involves spraying chloride or other chemicals on the gravel surface to reduce dust loss, aggregate loss, and maintenance. This is a relatively short-term fix that helps create a crusted surface.

⁷ Adapted from Inventory-based Rating System for Gravel Roads: Training Manual

Expansion Joint: Joints in a bridge that allow for slight expansion and contraction changes in response to temperature. Expansion joints prevent the build up of excessive pressure, which can cause structural damage to the bridge.

Federal Highway Administration: Also known as FHWA, this is an agency within the U.S. Department of Transportation that supports state and local governments in the design, construction, and maintenance of the nation's highway system.⁸

Federal-Aid Network: Portion of road network that is comprised of federal-aid routes. According to Title 23 of the United States Code, federal-aid-eligible roads are "highways on the federal-aid highways systems and all other public roads not classified as local roads or rural minor collectors".⁹ Roads that are part of the federal-aid network are eligible for federal gas-tax monies.

FHWA: See *Federal Highway Administration*.

Flexible Pavement: See *hot-mix asphalt pavement*.

Fog seal: An asphalt pavement treatment method that involves spraying a liquid asphalt coating onto the entire pavement surface to fill hairline cracks and prevent damage from sunlight and oxidation. This method works best for good to very good pavements.

Full-Depth Concrete Repair: A concrete pavement treatment method that involves removing sections of damaged concrete pavement and replacing it with new concrete of the same dimensions in order to restore the riding surface, delay water infiltration, restore load transfer from one slab to the next, and eliminate the need to perform costly temporary patching.

Geographic Divides: Areas where a geographic feature (e.g., river, lake, mountain) limits crossing points of the feature.

Grants: Competitive funding gained through an application process and targeted at a specific project type to accomplish a specific purpose. Grants can be provided both on the federal and state level and often make up part of the funds that a transportation agency receives.

Gravel Surfacing: A low-cost, easy-to-maintain road surface made from aggregate and fines.

Heavy Capital Preventive Maintenance: See *Capital preventive maintenance*.

HMA: See *hot-mix asphalt pavement*.

Hot-Mix Asphalt Overlay: Also known as HMA overlay, this a surface treatment that involves layering new asphalt over an existing pavement, either asphalt or concrete. It creates a new wearing surface for traffic and to seal the pavement from water, debris, and sunlight damage, and it often adds significant structural strength.

Hot-Mix Asphalt Pavement: Also known as HMA pavement, this type of asphalt creates a flexible pavement composed of aggregates, asphalt binder, and air voids. HMA is heated for placement and compaction at high temperatures. HMA is less expensive to construct than concrete pavement,

⁸ Federal Highway Administration webpage <https://www.fhwa.dot.gov/>

⁹ Inventory-based Rating System for Gravel Roads: Training Manual

however it requires frequent maintenance activities and generally lasts 18 years before major rehabilitation is necessary. HMA makes up the vast majority of local-agency-owned pavements.

IBR: See *IBR element, IBR number, and/or Inventory-based Rating System™*.

IBR Element: A feature used in the IBR System™ for assessing the condition of roads. The system relies on assessing three elements: surface width, drainage adequacy, and structural adequacy.¹⁰

IBR Number: The 1-10 rating determined from assessments of the weighted IBR elements. The weighting relates each element to the intensity road work needed to improve or enhance the IBR element category.¹¹

Interstate Highway System: The road system owned and operated by each state consisting of routes that cross between states, make travel easier and faster. The interstate roads are denoted by the prefix "I" or "U.S." and then a number, where odd routes run north-south and even routes run east-west. Examples are I-75 or U.S. 2.¹²

Inventory-Based Rating System™: Also known as the IBR System™, a rating system designed to assess the capabilities of gravel and unpaved roads to support intended traffic volumes and types year round. It assesses roads based on how three IBR elements, or features—surface width, drainage adequacy, and structural adequacy—compare to a baseline, or "good", road.¹³

Investment Reporting Tool: Also known as IRT, a web-based system used to manage the process for submitting required items to the Michigan Transportation Asset Management Council. Required items include planned and completed maintenance and construction activity for roads and bridges and comprehensive asset management plans.

IRT: See *Investment Reporting Tool*.

Jurisdiction: Administrative power of an entity to make decisions for something. In Michigan, the three levels of jurisdiction classification for transportation assets are state highways, county roads, and city and village streets. State highways are under the jurisdiction of the Michigan Department of Transportation, county roads are under the jurisdiction of the road commission for the county in which the roads are located, and city and village streets are under the jurisdiction of the municipality in which the roads are located.

Jurisdictional Borders: Borders between two road-owning-agency jurisdictions, or where the roads owned by one agency turn into roads owned by another agency. Examples of jurisdictional borders are township or county lines.

Lane-Mile Segment: A segment of road that is measured by multiplying the centerline miles of a roadway by the number of lanes present.

Lane-Mile-Years: A network's total lane-miles multiplied by one year; a method to quantify the measurable loss of pavement life.

¹⁰ Inventory-based Rating System for Gravel Roads: Training Manual

¹¹ Inventory-based Rating System for Gravel Roads: Training Manual

¹² <https://www.fhwa.dot.gov/interstate/faq.cfm#question3>

¹³ Adapted from Inventory-based Rating System for Gravel Roads: Training Manual

Light Capital Preventive Maintenance: See *Capital preventive maintenance*.

Limited Access Areas: Areas—typically remote areas—served by few or seasonal roads that require long detours routes if servicing roads are closed.

Main Access to Key Commercial Districts: Areas where large number or large size business will be significantly impacted if a road is unavailable.

Maintenance Grading: A surface treatment method for unpaved roads that involves re-grading the road to remove isolated potholes, washboarding, and ruts, and then restoring the compacted crust layer.

MDOT: See *Michigan Department of Transportation*.

MDOT's Local Bridge Program Call for Projects: A call for project proposals for replacement, rehabilitation, and/or preventive maintenance of local bridges that, if granted, receives bridge funding from the Michigan Department of Transportation. The Call for Projects is made by the Local Bridge Program.

MGF: See *Michigan Geographic Framework*.

Michigan Department of Transportation: Also known as MDOT, this is the state of Michigan's department of transportation, which oversees roads and bridges owned by the state or federal government in Michigan.

Michigan Geographic Framework: Also known as MGF, this is the state of Michigan's official digital base map that contains location and road information necessary to conduct state business. The Michigan Department of Transportation uses the MGF to link transportation assets to a physical location.

Michigan Public Act 51 of 1951: Also known as PA 51, this is a Michigan legislative act that served as the foundation for establishing a road funding structure by creating transportation funding distribution methods and means. It has been amended many times.¹⁴

Michigan Public Act 325 of 2018: Also known as PA 325, this legislation modified PA 51 of 1951 in regards to asset management in Michigan, specifically 1) re-designating the TAMC under Michigan Infrastructure Council (MIC); 2) promoting and overseeing the implementation of recommendations from the regional infrastructure asset management pilot program; 3) requiring local road three-year asset management plans beginning October 1, 2020; 4) adding asset classes that impact system performance, safety or risk management, including culverts and signals; 5) allowing MDOT to withhold funds if no asset management plan submitted; and 6) prohibiting shifting funds from a county primary to a county local, or from a city major to a city minor if no progress toward achieving the condition goals described in its asset plan.¹⁵

Michigan Public Act 499 of 2002: Also known as PA 499, this legislation requires road projects for the upcoming three years to be reported to the TAMC.

Michigan Transportation Asset Management Council: Also known as the TAMC, a council comprised of professionals from county road commissions, cities, a county commissioner, a township official,

¹⁴ Inventory-based Rating System for Gravel Roads: Training Manual

¹⁵ Inventory-based Rating System for Gravel Roads: Training Manual

regional and metropolitan planning organizations, and state transportation department personnel. The council reports directly to the Michigan Infrastructure Council.¹⁶ The TAMC provides resources and support to Michigan's road-owning agencies, and serves as a liaison in data collection requirements between agencies and the state.

Michigan Transportation Fund: Also known as MTF, this is a source of transportation funding supported by vehicle registration fees and the state's per-gallon gas tax.

Microsurface Treatment: An asphalt pavement treatment method that involves applying modified liquid asphalt, small stones, water, and Portland cement for the purpose of protecting a pavement from damage caused by water and sunlight.

Mill and Hot-Mix Asphalt Overlay: Also known as a mill and HMA overlay, this is a surface treatment that involves the removal of the top layer of pavement by milling and the replacement of the removed layer with a new HMA layer.

Mix-of-Fixes: A strategy of maintaining roads and bridges that includes generally prioritizes the spending of money on routine maintenance and capital preventive maintenance treatments to impede deterioration and then, as money is available, performing reconstruction and rehabilitation.

MTF: See *Michigan Transportation Fund*.

National Bridge Inspection Standards: Also known as NBIS, standards created by the Federal Highway Administration to locate and evaluate existing bridge deficiencies in the federal-aid highway system to ensure the safety of the traveling public. The standards define the proper safety for inspection and evaluation of all highway bridges.¹⁷

National Center for Pavement Preservation: Also known as the NCPP, a center that offers education, research, and outreach in current and innovative pavement preservation practices. This collaborative effort of government, industry, and academia entities was established at Michigan State University.

National Functional Class: Also known as NFC, a federal grouping system for public roads that classifies roads according to the type of service that the road is intended to provide.

National Highway System: Also known as NHS, this is a network of roads that includes the interstate highway system and other major roads managed by state and local agencies that serve major airports, marine, rail, pipelines, truck terminals, railway stations, military bases, and other strategic facilities.

NBIS: See *National Bridge Inspection Standards*.

NCPP: See *National Center for Pavement Preservation*.

NCPP Quick Check: A system created by the National Center for Pavement Preservation that works under the premise that a one-mile road segment loses one year of life each year that it is not treated with a maintenance, rehabilitation, or reconstruction project.

NFC: See *National Functional Class*.

¹⁶ Inventory-based Rating System for Gravel Roads: Training Manual

¹⁷ <https://www.fhwa.dot.gov/bridge/nbis/>

Non-Trunkline: A local road intended to be used over short distances but not recommended for long-distance travel.

Other Funds: Expenditures for equipment, capital outlay, debt principal payment, interest expense, contributions to adjacent governmental units, principal, interest and bank fees, and miscellaneous for cities and villages.

PA: See *Michigan Public Act 51*, *Michigan Public Act 325*, and/or *Michigan Public Act 499*.

Partial-Depth Concrete Repair: A concrete pavement treatment method that involves removing spalled or delaminated areas of concrete pavement, usually near joints and cracks, and replacing with new concrete. This is done to provide a new wearing surface in isolated areas, to slow down water infiltration, and to help delay further freeze-thaw damage.

PASER: See *Pavement Surface Evaluation and Rating system*.

Pavement Reconstruction: A complete removal of the old pavement and base and construction of an entirely new road. This is the most expensive rehabilitation of the roadway and also the most disruptive to traffic patterns.

Pavement Surface Evaluation and Rating system: Also known as the PASER system, the PASER system rates surface condition on a 1-10 scale, where 10 is a brand new road with no defects, 5 is a road with distress but that is structurally sound and requires only preventative maintenance, and 1 is a road with extensive surface and structural distresses that is in need of total reconstruction. This system provides a simple, efficient, and consistent method for evaluating the condition of paved roads.¹⁸

Pothole: A defect in a road that produces a localized depression.¹⁹

Preventive Maintenance: Planned treatments to an existing asset to prevent deterioration and maintain functional condition. This can be a more effective use of funds than the costly alternative of major rehabilitation or replacement.

Proactive Preventive Maintenance: Also known as PPM, a method of performing capital preventive maintenance treatments very early in a pavement's life, often before it exhibits signs of pavement defect.

Public Act 51: See *Michigan Public Act 51 of 1951*

Public Act 325: See *Michigan Public Act 325 of 2018*

Public Act 499: See *Michigan Public Act 499 of 2002*

Reconstruction and Rehabilitation Programs: Programs intended to reconstruct and rehabilitate a road.

Restricted Load Postings: A restriction enacted on a bridge structure when is incapable of transporting a state's legal vehicle loads.

¹⁸ Adapted from *Inventory-based Rating System for Gravel Roads: Training Manual*

¹⁹ *Inventory-based Rating System for Gravel Roads: Training Manual*

Rights-of-Way Ownership: The owning of the right-of-way, which is the land over which a road or bridge travels. In order to build a road, road agencies must own the right-of-way or get permission to build on it.

Rigid Pavement: See *concrete pavement*.

Road Infrastructure: An agency's road network and assets necessary to make it function, such as traffic signage and ditches.

Road: The area consisting of the roadway (i.e., the travelled way or the portion of the road on which vehicles are intended to drive), shoulders, ditches, and areas of the right of way containing signage.²⁰

Roadsoft: An asset management software suit that enables agencies to manage road and bridge related infrastructure. The software provides tools for collecting, storing, and analyzing data associated with transportation infrastructure. Built on an optimum combination of database engine and GIS mapping tools, Roadsoft provides a quick, smooth user experience and almost unlimited data handling capabilities.²¹

Ruts/Rutting: Deformation of a road that usually forms as a permanent depression concentrated under the wheel path parallel to the direction of travel.²²

Scheduled Maintenance: Low-cost, day-to-day activities applied to bridges on a scheduled basis that mitigates deterioration.²³

Sealcoat Pavement: A gravel road that has been sealed with a thin asphalt binder coating that has stone chips spread on top.

Service Life: Time from when a road or treatment is first constructed to when it reaches a point where the distresses present change from age-related to structural-related (also known as the critical distress point).²⁴

Slurry Seal: An asphalt pavement treatment method that involves applying liquid asphalt, small stones, water, and Portland Cement in a very thin layer with the purpose of protecting an existing pavement from being damaged by water and sunlight.

Structural Improvement: Pavement treatment that adds strength to the pavement. Roads requiring structural improvement exhibit alligator cracking and rutting and are considered poor by the TAMC definitions for condition.

Subsurface Infrastructure: Infrastructure maintained by local agencies that reside underground, for example, drinking water distribution systems, wastewater collection systems, and storm sewer systems.

TAMC: See *Michigan Transportation Asset Management Council*.

²⁰ Inventory-based Rating System for Gravel Roads: Training Manual

²¹ Inventory-based Rating System for Gravel Roads: Training Manual

²² Paving Class Glossary

²³ Inventory-based Rating System for Gravel Roads: Training Manual

²⁴ Inventory-based Rating System for Gravel Roads: Training Manual

TAMC Pavement Condition Dashboard: Website for viewing graphs of pavement and bridge conditions, traffic and miles travelled, safety statistics, maintenance activities, and financial data for Michigan's cities and villages, counties, and regions, as well as the state of Michigan.

TAMC's Good/Fair/Poor Condition Classes: Classification of road conditions defined by the Michigan Transportation Asset Management Council based on bin ranges of PASER scores and similarities in defects and treatment options. Good roads have PASER scores of 8, 9, or 10, have very few defects, and require minimal maintenance. Fair roads have PASER scores of 5, 6, or 7, have good structural support but a deteriorating surface, and can be maintained with CPM treatments. Poor roads have PASER scores of 1, 2, 3, or 4, exhibit evidence that the underlying structure is failing, such as alligator cracking and rutting. These roads must be rehabilitated with treatments like heavy overlay, crush and shape, or total reconstruction.

Tax Millages: Local tax implemented to supplement an agency's budget, such as road funding.

Thin Hot-Mix Asphalt Overlay: Application of a thin layer of hot-mix asphalt on an existing road to re-seal the road and protect it from damage caused by water. This also improves the ride quality and provides a smoother, uniform appearance that improves visibility of pavement markings.²⁵

Transportation Infrastructure: All of the elements that work together to make the surface transportation system function including roads, bridges, culverts, traffic signals, and signage.

Trigger: When a PASER score gives insight to the preferred timeline of a project for applying the correct treatment at the correct time.

Trunkline Abbreviations: The prefixes *M*-, *I*-, and *US* indicate roads in Michigan that are part of the state trunkline system, the Interstate system, and the US Highway system. These roads consist of anything from 10-lane urban freeways to two-lane rural highways and even one non-motorized highway; they cover 9,668 centerline miles. Most of the roads are maintained by MDOT.

Trunkline Bridges: Bridge present on a trunkline road, which typically connects cities or other strategic places and is the recommended rout for long-distance travel.²⁶

Trunkline Maintenance Funds: Expenditures under a maintenance agreement with MDOT for maintenance activities performed on MDOT trunkline routes.

Trunkline: Major road that typically connects cities or other strategic places and is the recommended route for long-distance travel.²⁷

Washboarding: Ripples in the road surface that are perpendicular to the direction of travel.²⁸

Wedge/Patch Sealcoat Treatment: An asphalt pavement treatment method that involves correcting the damage frequently found at the edge of a pavement by installing a narrow, 2- to 6-foot-wide wedge along the entire outside edge of a lane and layering with HMA. This extends the life of an HMA pavement or chip seal overlay by adding strength to significantly settled areas of the pavement.

²⁵ [second sentence] <http://www.kentcountyroads.net/road-work/road-treatments/ultra-thin-overlay>

²⁶ https://en.wikipedia.org/wiki/Trunk_road

²⁷ https://en.wikipedia.org/wiki/Trunk_road

²⁸ Inventory-based Rating System for Gravel Roads: Training Manual

Worst-First Strategy: Asset management strategy that treats only the problems, often addressing the worst problems first, and ignoring preventive maintenance. This strategy is the opposite of the “mix of fixes” strategy. An example of a worst-first approach would be purchasing a new automobile, never changing the oil, and waiting till the engine fails to address any deterioration of the car.

List of Acronyms

CPM: capital preventive maintenance
FHWA: Federal Highway Administration
HMA: hot-mix asphalt
I: trunkline abbreviation for routes on the Interstate system
IBR: Inventory-based Rating
M: trunkline abbreviation for Michigan state highways
MDOT: Michigan Department of Transportation
MTF: Michigan Transportation Fund
NBIS: National Bridge Inspection Standards
NCPP: National Center for Pavement Preservation
NHS: National Highway System
PA 51: Michigan Public Act 51 of 1951
PASER: Pavement Surface Evaluation and Rating
R&R: reconstruction and rehabilitation programs
TAMC: (Michigan) Transportation Asset Management Council
US: trunkline abbreviation for routes on the US Highway system



City of Taylor 2024 Transportation

ASSET MANAGEMENT PLAN

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EXECUTIVE SUMMARY

As conduits for commerce and connections to vital services, roads and bridges are some of the most important assets in any community, and other assets like culverts, traffic signs, traffic signals, and utilities support and affect roads and bridges. The City of Taylor's (Taylor) roads, bridges, and support systems are also some of the most valuable and extensive public assets, all of which are paid for with taxes collected from ordinary citizens and businesses. The cost of building and maintaining these assets, their importance to society, and the investment made by taxpayers all place a high level of responsibility on local agencies to plan, build, and maintain roads, bridges, and support assets in an efficient and effective manner. This asset management plan is intended to report on how Taylor is meeting its obligations to maintain the public assets for which it is responsible.

This plan identifies Taylor's assets and condition and how Taylor maintains and plans to improve the overall condition of those assets. An Asset Management Plan (AMP) is required by Michigan Public Act 325 of 2018, and this document represents fulfillment of some of Taylor's obligations towards meeting these requirements. However, this plan and its supporting documents are intended to be much more than a fulfillment of required reporting. This asset management plan helps to demonstrate Taylor's responsible use of public funds by providing elected and appointed officials as well as the general public with the inventory and condition information of Taylor's assets, and it gives taxpayers the information they need to make informed decisions about investing in Taylor's essential transportation infrastructure.

INTRODUCTION

Asset management is defined by Public Act 325 of 2018 as “an ongoing process of maintaining, preserving, upgrading, and operating physical assets cost effectively, based on a continuous physical inventory and condition assessment and investment to achieve established performance goals”. In other words, asset management is a process that uses data to manage and track assets, like roads and bridges, in a cost-effective manner using a combination of engineering and business principles. This process is endorsed by leaders in municipal planning and transportation infrastructure, including the Michigan Municipal League, County Road Association of Michigan, the Michigan Department of Transportation (MDOT), and the Federal Highway Administration (FHWA). The City of Taylor is supported in its use of asset management principles and processes by the Michigan Transportation Asset Management Council (TAMC), formed by the State of Michigan.

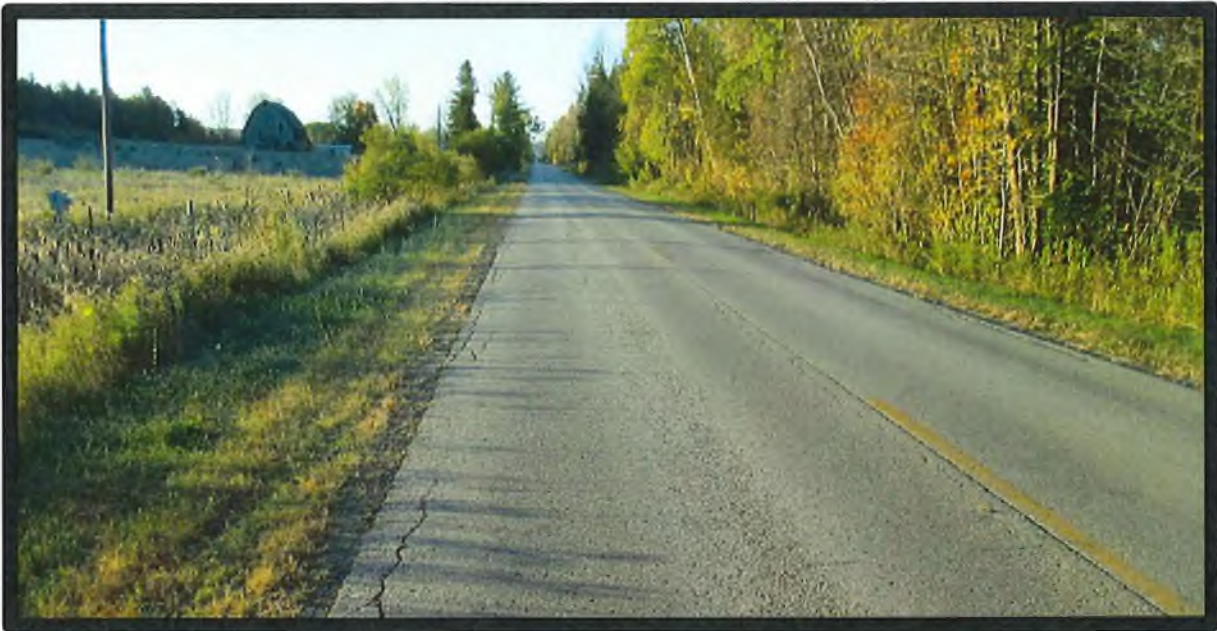
Asset management, in the context of this plan, ensures that public funds are spent as effectively as possible to maximize the condition of the road and bridge network. Asset management also provides a transparent decision-making process that allows the public to understand the technical and financial challenges of managing transportation infrastructure with a limited budget.

The City of Taylor (Taylor) has adopted an “asset management” business process to overcome the challenges presented by having limited financial, staffing, and other resources while needing to meet road users' expectations. Taylor is responsible for maintaining and operating over 195.287 centerline miles of roads and 28 bridge structures.

This 2024 plan identifies Taylor’s transportation assets and their condition as well as the strategy that Taylor uses to maintain and upgrade particular assets given Taylor’s condition goals, priorities of network's road users, and resources. An updated plan is to be released approximately every three years both to comply with Public Act 325 and to reflect changes in road conditions, finances, and priorities.

Questions regarding the use or content of this plan should be directed to Chris Gibbs, PE, City Engineer at 25605 Goddard Road, Taylor, MI 48180 or at 734-287-6550 / cgibbs@ci.taylor.mi.us.

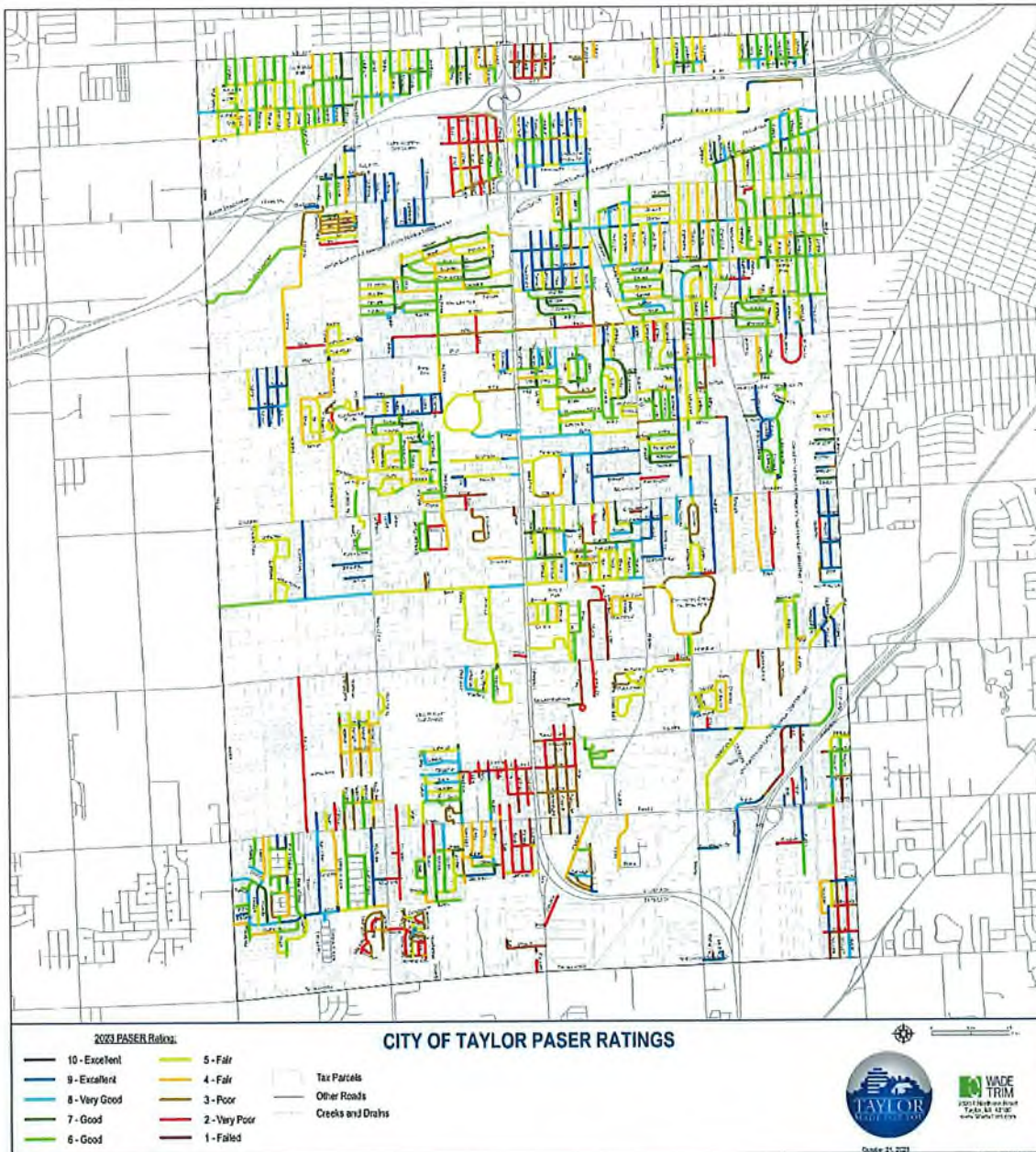
1.0 PAVEMENT ASSETS



Taylor is responsible for 195.287 centerline miles of public roads. An inventory of these miles divides them into different network classes based on road purpose/use and funding priorities as identified at the State level: City major road network, which is prioritized for state-level funding, and City local road network.

1.1 Inventory of Assets

Figure 1-1. Map showing Location of Roads Managed by Taylor and the Current Condition for Paved Roads



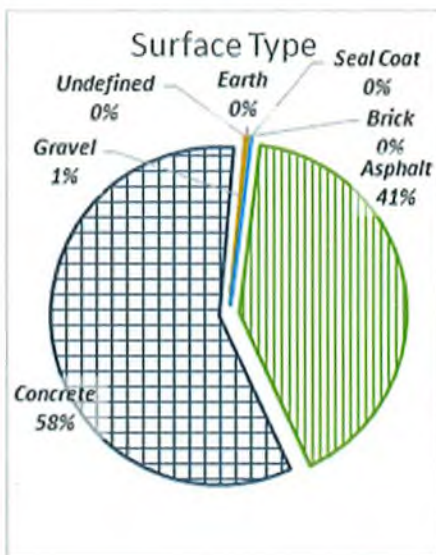
Of Taylor's 195.287 miles of road, 35.718 miles are classified as City Major and 159.569 miles are classified as City Local (Figure 1 identifies the 2023 PASER ratings for these roads). More detail

about these road assets can be found in Taylor's Roadsoft database or by contacting The City of Taylor.

1.2 Types

Taylor has multiple types of surface pavements in its jurisdiction, including primarily asphalt and concrete. A very minor percentage (1%) are identified as gravel roads. The remaining categories (undefined, earth, seal coat remain at 0%). **Figure 1-2** shows a breakdown of these surface pavement types for all of Taylor's road assets.

Figure 1-2. Pavement Type by Percentage Maintained by Taylor



1.3 Condition, Goals, and Trend

1.3.1 Paved Roads

Paved roads in Michigan are rated using the Pavement Surface Evaluation and Rating (PASER) system, which is a 1 to 10 scale with 10 being a newly constructed surface and 1 being a completely failed surface. PASER scores are grouped into TAMC definition categories of good (8-10), fair (5-7), and poor (1-4) categories. Taylor collects PASER data every two years on 100 percent of those portions of its City major and City local networks that are eligible for federal funding. In addition, Taylor uses its own staff and resources to collect PASER data on 100% percent of its City major and City local networks that are not eligible for federal funding.

Currently, the City major network has 24% of its roads in good condition, 45% in fair condition, and 31% in poor condition. The City local network has 20% of its roads in good condition, 53% in fair condition, and 27% in poor condition (Figure 3 and Figure 4). Taylor's long-range goal for the City major network is to increase the percentage of roads in good condition, maintain or reduce the

percentage in fair condition, and reduce the percentage of roads in poor condition. Taylor's long-range goal for the City local network is to increase the percentage of roads in good condition, maintain or reduce the percentage of roads in fair condition, and decrease the percentage of roads in poor condition (Figure 3 and Figure 4). Figure 3 and Figure 4 illustrate the historical and current condition (solid bars) of Taylor's City major and City local networks, respectively; they also illustrate the projected trend (shaded bars) and the overall trend in condition (trendlines).

Figure 1-3. City Major Network Condition, Goals, and Trend

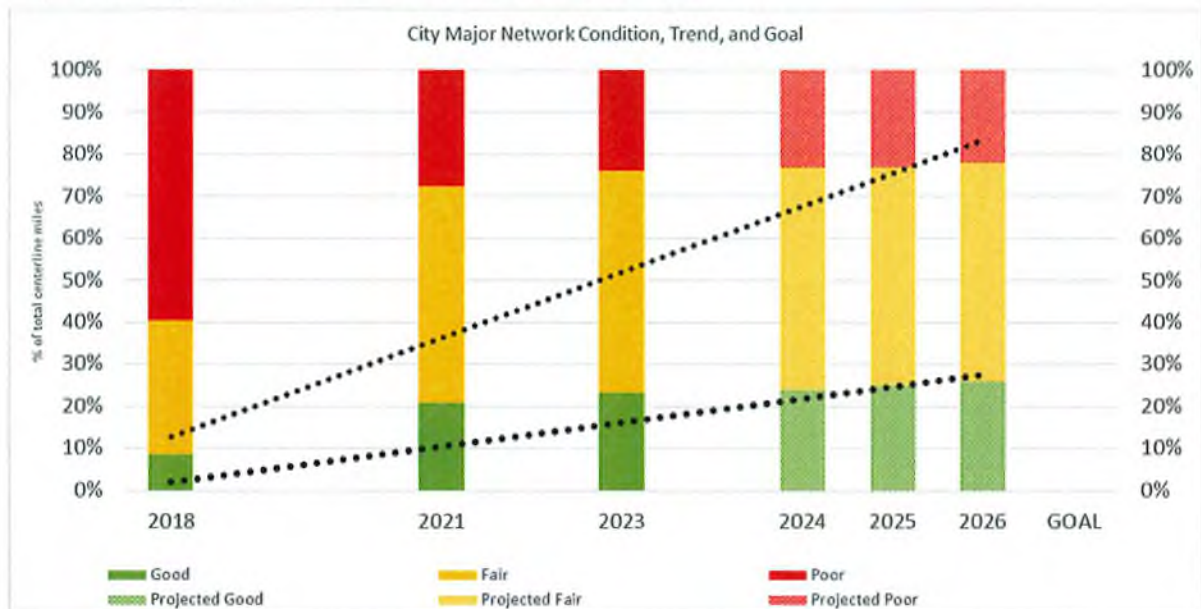
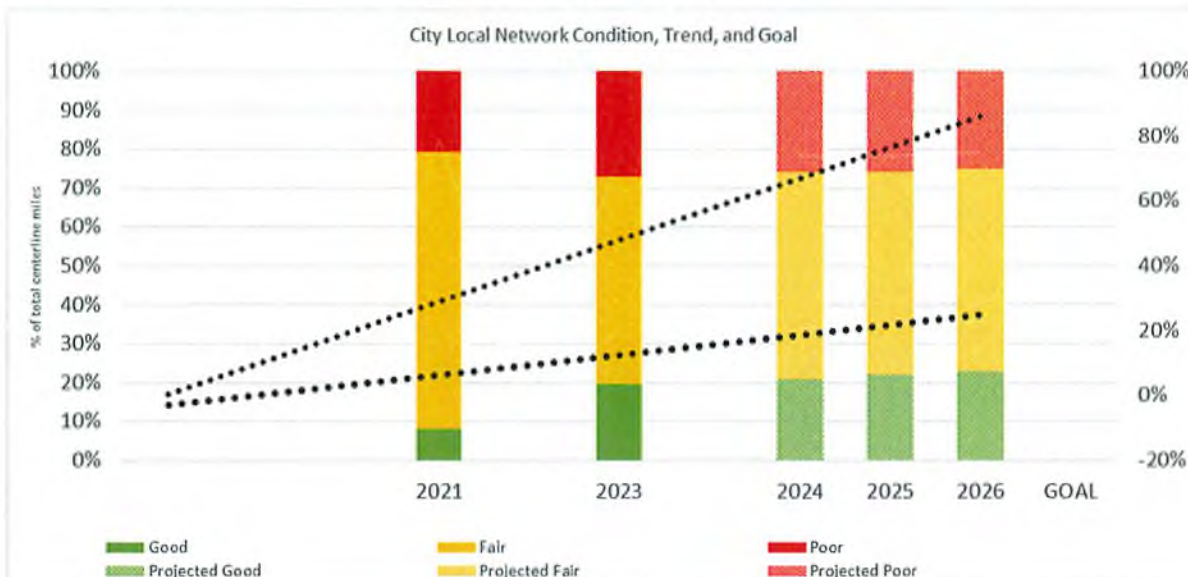


Figure 1-4. City Local Network Condition, Goals, and Trend



1.4 Modelled Trends, Gap Analysis, and Planned Projects

1.4.1 Modelled Trends & Gap Analysis

The Roadsoft network analysis of Taylor’s planned projects for the City major and City local networks from Taylor’s currently-available budget does allow Taylor to reach its pavement condition goals given the projects planned for the next three years. The list of proposed projects and their projected budgets are illustrated below.

Table 1-1 Proposed Project and Projected Budgets

Propose Project	Fiscal Year	Projected Budget (2024)
Non-Motorized	2027	\$200,000
Mortenview (Wick to Ecorse)	2026	\$800,000
Byers (Telegraph to Ecorse)	2026	\$400,000
Superior (Pardee to Racho)	2027	\$2,500,000
Westlake (Eureka to Dead End South)	2025	\$800,000
Lange (Eureka to Northline)	2026	\$800,000

2.0 BRIDGE ASSETS



Taylor is responsible for four bridges. The City conducts biennial inspections for these bridges.

2.1 Condition, Goals, and Trend

Bridges in Michigan are given a good, fair, or poor rating based on the National Bridge Inspection Standards (NBIS) rating scale, which was created by the Federal Highway Administration to evaluate a bridge's deficiencies and to ensure the safety of road users.

The goal of the program is the preservation and safety of Taylor's bridge network.

2.2 Taylor Bridge Responsibility

Taylor is responsible for four (4) bridges, whereby they remain compliant with biennial routine bridge safety inspections that are reportable to the Michigan Department of Transportation (MDOT). The bridge locations are as follows:

- Huron Street over Frank & Poet Drain, Identified as Structure No. 11635
- Mortenview Drive over Sexton-Kilfoil Drain, Identified as Structure No. 12514
- Continental Drive over Sexton-Kilfoil Drain, Identified as Structure No. 12515
- Beech Daly Road over Blakely Drain, Structure No. 12978

2.3 Beech Daly Road over Blakely Drain, Identified as Structure No. 12978 Programmed/Funded Projects, Gap Analysis, and Planned Projects

Taylor uses local funds for fixes to be applied to bridges. At present, all of the bridges under Taylor's jurisdiction are in good condition and there are no fixes forecasted at this time. Preventative maintenance is a more effective use of these funds than the costly alternative of major rehabilitation or replacement. Since Taylor recognizes that limited funds are available for improving the bridge network, it seeks to identify those bridges that will benefit from a planned maintenance program. Taylor does not plan to replace any bridges within the next three years. By performing the preventative maintenance of bridge structures, Taylor should achieve its goal of keeping its overall bridge network at the same condition.

3.0 CULVERT ASSETS



3.1 Inventory of Assets

At present, Taylor does not track inventory data of its culvert assets

3.2 Goals

Taylor is responsible for preserving any and all culverts that underlie its entire road network.

3.3 Planned Projects

Taylor's policy is to replace or repair culvert assets concurrent with projects affecting road segments carried by the particular culverts.

4.0 SIGNAL ASSETS



4.1.1 Inventory of Assets

At present, Taylor tracks only inventory data for traffic signals.

More detail about these traffic signal assets can be obtained by contacting The City of Taylor.

4.1.2 Goals

The goal of Taylor's asset management program is the preservation of its traffic signals.

4.1.3 Planned Projects

Taylor does not have any planned projects for their signals.

5.0 FINANCIAL RESOURCES

Public entities must balance the quality and extent of services they can provide with the tax resources provided by citizens and businesses, all while maximizing how efficiently funds are used. Therefore, Taylor will overview its general expenditures and financial resources currently devoted to transportation infrastructure maintenance. This financial information is not intended to be a full financial disclosure or a formal report. Full details of Taylor's financial status can be found on our website at <https://www.cityoftaylor.com/177/Budgets-Financial-Reports>.

5.1 Anticipated Revenues & Expenses

Taylor receives funding from the following sources:

- **State funds** – Taylor's principal source of transportation funding is received from the Michigan Transportation Fund (MTF). This fund is supported by vehicle registration fees and the State's per-gallon gas tax. Allocations from the MTF are distributed to State and Local governmental units based on a legislated formula, which includes factors such as population, miles of certified roads, and vehicle registration fees for vehicles registered in the agency's jurisdiction. Examples of State grants also include local bridge grants, economic development funds, and metro funds.
- **Federal and State grants for individual projects** – These are typically competitive funding applications that are targeted at a specific project type to accomplish a specific purpose. These may include safety enhancement projects, economic development projects, or other targeted funding. Examples of federal funds include Surface Transportation Program (STP) funds, C and D funds, Bridge Funds, MDOT payments to private contractors, and negotiated contracts.
- **Local government entities or private developer contributions to construction projects for specific improvements** – This category includes funding received to mitigate the impact of commercial developments as a condition of construction of a specific development project, and can also include funding from a special assessment district levied by another governmental unit. Examples of contributions from local units include City, Village, and Township contributions to the County; special assessments; County appropriations; bond and note proceeds; contributions from Counties to Cities and Villages; City general fund transfers; City municipal street funds; capital improvement funds; and tax millages (see below).

Taylor is required to report transportation fund expenditures to the State of Michigan using a prescribed format with predefined expenditure categories. The definitions of these categories according to Public Act 51 of 1951 may differ from common pavement management nomenclature and practice. For the purposes of reporting under PA 51, the expenditure categories are:

- **Construction/Capacity Improvement Funds** – According to PA 51 of 1951, this financial classification of projects includes, "new construction of highways, roads, streets, or bridges, a project that increases the capacity of a highway facility to accommodate that part of traffic having

neither an origin nor destination within the local area, widening of a lane width or more, or adding turn lanes of more than 1/2 mile in length.”¹

- **Preservation and Structural Improvement Funds** – Preservation and structural improvements are “activit[ies] undertaken to preserve the integrity of the existing roadway system.”² Preservation includes items such as a reconstruction of an existing road or bridge, or adding structure to an existing road.
- **Routine and Preventive Maintenance Funds** – Routine maintenance activities are “actions performed on a regular or controllable basis or in response to uncontrollable events upon a highway, road, street, or bridge”.³ Preventive maintenance activities are “planned strategy[ies] of cost-effective treatments to an existing roadway system and its appurtenances that preserve assets by retarding deterioration and maintaining functional condition without significantly increasing structural capacity”.⁴
- **Winter Maintenance Funds** – Expenditures for snow and ice control.
- **Trunkline Maintenance Funds** – Expenditures spent under Taylor’s maintenance agreement with MDOT for maintenance it performs on MDOT trunkline routes.
- **Administrative Funds** – There are specific items that can and cannot be included in administrative expenditures as specified in PA 51 of 1951. The law also states that the amount of MTF revenues that are spent on administrative expenditures is limited to 10 percent of the annual MTF funds that are received.
- **Other Funds** – Expenditures for equipment, capital outlay, debt principal payment, interest expense, contributions to adjacent governmental units, principal, interest and bank fees, and miscellaneous for cities and villages.

Table 5-1 details the revenues and expenditures for Taylor.

¹ Public Act 51 of 1951, 247.660c Definitions

² Public Act 51 of 1951, 247.660c Definitions

³ Public Act 51 of 1951, 247.660c Definitions

⁴ Public Act 51 of 1951, 247.660c Definitions

Table 5-1. Annual Fiscal Year Revenues & Expenditures per Fiscal Year					
REVENUES			EXPENDITURES		
Item	Estimated \$	Percent of Total	Item	Estimated \$	Percent of Total
State funds	2,000,000	99.1	Construction & capacity improvement (CCI)	0	0
Federal funds	0	0.0	Preservation & structural improvement (PSI)	7,645,820	21.3
Contributions for local units	0	0.0	Routine maintenance	0	0
Interest, rents, and other	4500	0.2	Winter maintenance	510,000	5.7
Charges for services	0	0	Trunkline maintenance	0	0
			Administrative	0	0
			Other	821,200	9.1
TOTAL	2,022,500	100	TOTAL	44,885,100	335

Verify the information in this table. You can find your agency's information in the TAMC dashboard at <https://www.mcqi.state.mi.us/mitrp/tamcDashboards>.

6.0 RISK OF FAILURE ANALYSIS

Transportation infrastructure is designed to be resilient. The system of interconnecting roads and bridges maintained by Taylor provides road users with multiple alternate options in the event of an unplanned disruption of one part of the system. There are, however, key links in the transportation system that may cause significant inconvenience to users if they are unexpectedly closed to traffic. Key transportation links include:

- **Geographic divides:** Areas where a geographic feature (river, lake, hilly terrain, or limited access road) limits crossing points of the feature; bridge failures, in particular, can create loss of access to entire regions of the state
- **Emergency alternate routes for high-volume roads and bridges:** Roads and bridges that are routinely used as alternate routes for high-volume assets are included in an emergency response plan
- **Limited access areas:** Roads and bridges that serve remote or limited access areas that result in long detours if closed
- **Main access to key commercial districts:** Areas with a large concentration of businesses or where large-size business will be significantly impacted if a road is unavailable
Our road network does not have any identified critical assets.

7.0 COORDINATION WITH OTHER ENTITIES

An asset management plan provides a significant value for infrastructure owners because it serves as a platform to engage other infrastructure owners using the same shared right of way space. Taylor communicates with both public and private infrastructure owners to coordinate work in the following ways:

Taylor coordinates with multiple agencies that maintain drinking water, sanitary, and storm sewer assets in addition to transportation assets. Taylor follows an asset management process for all of its assets by coordinating the upgrade, maintenance, and operation of all major assets.

Taylor takes advantage of coordinated infrastructure work to reduce cost and maximize value using the following policies:

- Roads which are in poor condition that have a subsurface infrastructure project planned which will destroy more than half the lane width are typically rehabilitated or reconstructed full width using transportation funds to repair the balance of the road width.
- Subsurface infrastructure projects which will cause damage to pavements in good condition will be delayed if possible, or methods that do not require pavement cuts will be considered.
- Subsurface utility projects will be coordinated to allow all under pavement assets to be upgraded in the same project regardless of ownership.
- Road reconstruction projects will typically not be completed until agency owned sub surface utilities are upgraded to have at least a 40 years of remaining service life.

8. PROOF OF ACCEPTANCE

PUBLIC ACT 325



CERTIFICATION OF TRANSPORTATION ASSET MANAGEMENT PLAN

Certification Year: 2025

Local Road-owning Agency Name: CITY OF TAYLOR

Beginning October 2019 and on a three-year cycle thereafter, certification must be made for compliance to Public Act 325. A local road-owning agency with 100 certified miles or more must certify that it has developed an asset management plan for the road, bridge, culvert, and traffic signal assets. Signing this form certifies that the hitherto referred agency meets with minimum requirements as outlined by Public Act 325 and agency-defined goals and objectives.

This form must be signed by the chairperson of the local road-owning agency or the county executive and chief financial officer of the local road-owning agency.

Signature 	Signature 
Printed Name Douglas A. Geiss	Printed Name JASON COUTURE
Title Chairman	Title FINANCE DIRECTOR / CFO
Date 11/21/2025	Date 1/21/25

Due every three years based on agency submission schedule

Submittal Date: 01/24/2025

See attached council meeting minutes and/or resolution.

TIM WOOLLEY
Mayor

MICHELLE TOCCO
Treasurer

City of Taylor

OFFICE OF THE CITY CLERK

CYNTHIA A. BOWER

23555 GODDARD ROAD
TAYLOR, MICHIGAN 48180-4116

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CITY COUNCIL

DOUGLAS A. GEISS
Chairman

JILL BRANDANA
Chairwoman Pro-Tem

TINA DANIELS
CHARLES JOHNSON
LINDSEY ROSE
ANGELA WINTON

At the regular meeting of the Taylor City Council held on January 21, 2025 the following Resolution was adopted:

Motion by: Rose, supported by: Daniels

Resolved: Motion to approve the submission of the 2024 Transportation Asset Management Plan to the State of Michigan, and authorize the execution of the required Proof of Acceptance.
Unanimously carried.

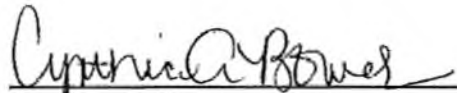
CCR #: 1.27-25

Ayes: Geiss, Brandana, Daniels, Johnson, Rose, Winton

Nays: None

Absent: None

I, Cynthia A. Bower, City Clerk of the City of Taylor, hereby certify that the foregoing is a true and complete copy of a Resolution as adopted by the City Council of the City of Taylor, County of Wayne, Michigan, at a regular meeting held on January 21, 2025 and that said meeting was conducted and public notice of said meeting was given pursuant to and in full compliance with the Open Meetings Act, being Act 267, Public Acts of Michigan, 1976, and that the Minutes of said meeting were kept and will be or have been made available as required by said Act.


Cynthia A. Bower

Date: January 22, 2025



Appendix A. Pavement Asset Management Plan

An attached pavement asset management plan follows.



City of Taylor 2024 Pavement

ASSET MANAGEMENT PLAN

A plan describing the City of Taylor's roadway assets and conditions.

Prepared by:
Wade Trim Associates, Inc.
25251 Northline Road
Taylor, MI 48180
734-947-9700

January 13, 2025



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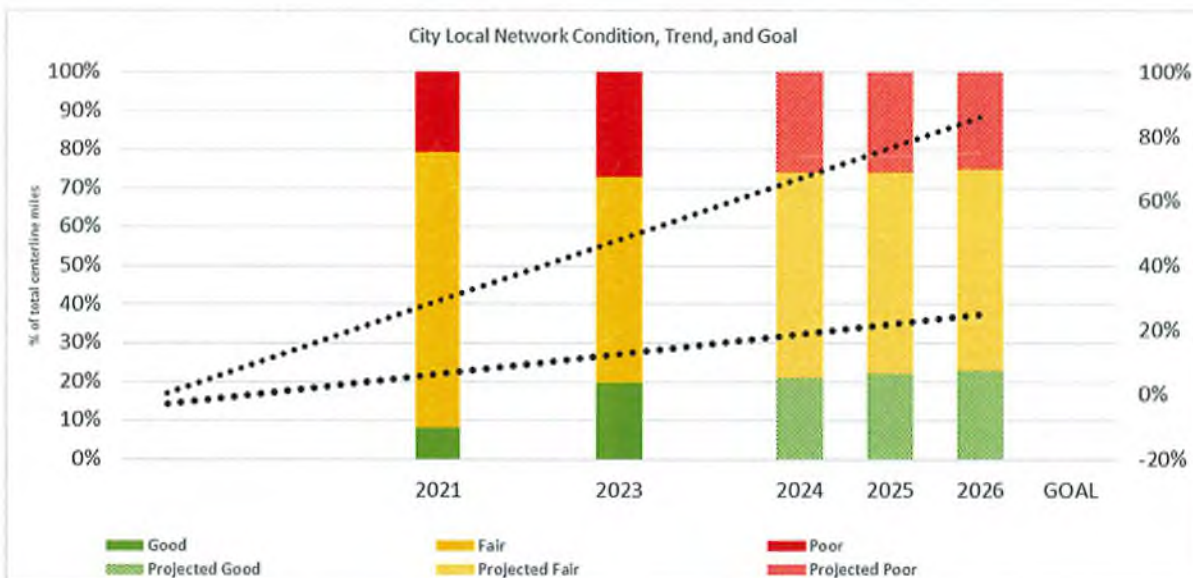
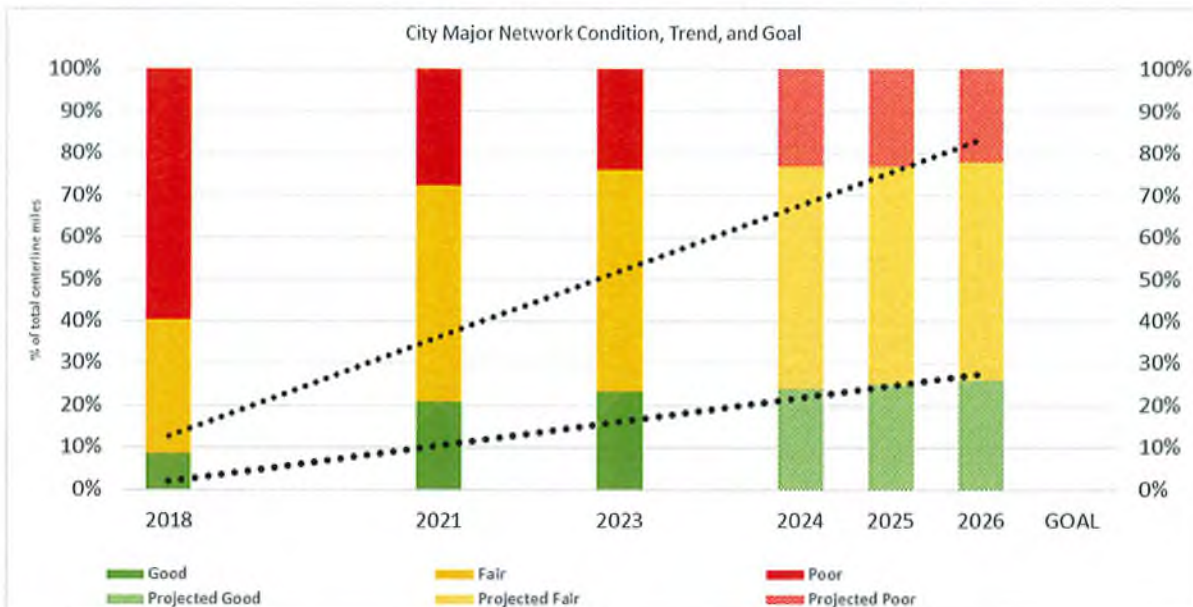
EXECUTIVE SUMMARY

As conduits for commerce and connections to vital services, roads are among the most important assets in any community along with other assets like bridges, culverts, traffic signs, traffic signals, and utilities that support and affect roads. The City of Taylor's (Tay) roads, other transportation assets, and support systems are also some of the most valuable and extensive public assets, all of which are paid for with taxes collected from ordinary citizens and businesses. The cost of building and maintaining roads, their importance to society, and the investment made by taxpayers all place a high level of responsibility on local agencies to plan, build, and maintain the road network in an efficient and effective manner. This asset management plan is intended to report on how Tay is meeting its obligations to maintain the public assets for which it is responsible.

This plan overviews Taylor's road assets and condition, and explains how Taylor works to maintain and improve the overall condition of those assets. These explanations can help answer the following questions:

- What kinds of road assets Taylor has in its jurisdiction, who owns them, and the different options for maintaining these assets.
- What tools and processes Taylor uses to track and manage road assets and funds.
- What condition Taylor's road assets are in compared to statewide averages.
- Why some road assets are in better condition than others and the path to maintaining and improving road asset conditions through proper planning and maintenance.
- How agency transportation assets are funded and where those funds come from.
- How funds are used and the costs incurred during Taylor's road assets' normal life cycle.
- What condition Taylor can expect its road assets if those assets continue to be funded at the current funding levels
- How changes in funding levels can affect the overall condition of all of Taylor's road assets.

Taylor owns and/or manages 195.287 centerline of roads. This road network can be divided into the city major network, the city local network. Based on the different factors these roads have that influence asset management decisions. A summary of Taylor historical and current network conditions, projected trends, and goals for city major network and city local network can be seen in the two figures, below:



An asset management plan is required by Michigan Public Act 325 of 2018, and this document represents fulfillment of some of Taylor’s obligations towards meeting these requirements. This asset management plan also helps demonstrate Taylor’s responsible use of public funds by providing elected and appointed officials as well as the general public with inventory and condition information of Taylor’s road assets, and gives taxpayers the information they need to make informed decisions about investing in its essential transportation infrastructure.

INTRODUCTION

Asset management is defined by Public Act 325 of 2018 as “an ongoing process of maintaining, preserving, upgrading, and operating physical assets cost effectively, based on a continuous physical inventory and condition assessment and investment to achieve established performance goals”. In other words, asset management is a process that uses data to manage and track assets, like roads and bridges, in a cost-effective manner using a combination of engineering and business principles. This process is endorsed by leaders in municipal planning and transportation infrastructure, including the Michigan Municipal League, County Road Association of Michigan, the Michigan Department of Transportation (MDOT), and the Federal Highway Administration (FHWA). Taylor is supported in its use of asset management principles and processes by the Michigan Transportation Asset Management Council (TAMC), formed by the State of Michigan.

Asset management, in the context of this plan, ensures that public funds are spent as effectively as possible to maximize the condition of the road network. Asset management also provides a transparent decision-making process that allows the public to understand the technical and financial challenges of managing road infrastructure with a limited budget.

The City of Taylor (Taylor) has adopted an “asset management” business process to overcome the challenges presented by having limited financial, staffing, and other resources while needing to meet road users’ expectations. Taylor is responsible for maintaining and operating over 195.287 centerline of roads.

This plan outlines how Taylor determines its strategy to maintain and upgrade road asset condition given agency goals, priorities of its road users, and resources provided. An updated plan is to be released approximately every three years to reflect changes in road conditions, finances, and priorities.

Questions regarding the use or content of this plan should be directed to *chris* Gibbs, PE, City Engineer at 25605 Goddard Road, Taylor, MI 48180 or at 734-287-6550 / cgibbs@ci.taylor.mi.us. Key terms used in this plan are defined in Taylor’s comprehensive transportation asset management plan (also known as the “compliance plan”) used for compliance with PA 325 or 2018. Knowing the basic features of the asset classes themselves is a crucial starting point to understanding the rationale behind an asset management approach. The following primer provides an introduction to pavements.

Pavement Primer

Roads come in two basic forms—paved and unpaved. Paved roads have hard surfaces. These hard surfaces can be constructed from asphalt, concrete, composite (asphalt and concrete), sealcoat, and brick and block materials. On the other hand, unpaved roads have no hard surfaces. Examples of these surfaces are gravel and unimproved earth.

The decision to pave with a particular material as well as the decision to leave a road unpaved allows road-owning agencies to tailor a road to a particular purpose, environment, and budget. Thus, selecting a pavement type or leaving a road unpaved depends upon purpose, materials available, and budget. Each choice represents a trade-off between budget and costs for construction and maintenance.

Maintenance enables the road to fulfill its particular purpose. To achieve the maximum service for a pavement or an unpaved road, continual monitoring of a road's pavement condition is essential for choosing the right time to apply the right fix in the right place.

Here is a brief overview of the different types of pavements, how condition is assessed, and treatment options that can lengthen a road's service life.

Surfacing

Pavement type is influenced by several different factors, such as cost of construction, cost of maintenance, frequency of maintenance, and type of maintenance. These factors can have benefits affecting asset life and road user experience.

Paved Surfacing

Typical benefits and tradeoffs for hard surface types include:

- **Concrete Pavement:** Concrete pavement, which is sometimes called a rigid pavement, is durable and lasts a long time when properly constructed and maintained. Concrete pavement can have longer service periods between maintenance activities, which can help reduce maintenance-related traffic disruptions. However, concrete pavements have a high initial cost and can be challenging to rehabilitate and maintain at the end of their service life. A typical concrete pavement design life will provide service for 30 years before major rehabilitation is necessary.
- **Hot-Mix Asphalt Pavement (HMA):** HMA pavement, sometimes known as asphalt or flexible pavement, is currently less expensive to construct than concrete pavement (this is, in some part, due to the closer link between HMA material costs and oil prices that HMA pavements have in comparison with other pavement types). However, they require frequent maintenance activities to maximize their service life. A typical HMA pavement design life will provide service for 18 years before major rehabilitation is necessary. The vast majority of local-agency-owned pavements are HMA pavements.
- **Composite Pavements:** Composite pavement is a combination of concrete and asphalt layers. Typically, composite pavements are old concrete pavements exhibiting ride-related issues that were overlaid by several inches of HMA in order to gain more service life from the pavement before it would need reconstruction. Converting a concrete pavement to a composite pavement is typically used as a "holding pattern" treatment to maintain the road in usable condition until reconstruction funds become available.

- **Sealcoat pavement:** Sealcoat pavement is a gravel road that have been sealed with a thin asphalt binder coating that has stone chips spread on top (not to be confused with a chip seal treatment over HMA pavement). This type of a pavement relies on the gravel layer to provide structure to support traffic, and the asphalt binder coating and stone chips shed water and eliminate the need for maintenance grading. Nonetheless, sealcoat pavement does require additional maintenance steps that asphalt and gravel do not require and does not last as long as HMA pavement, but it provides a low-cost alternative for lightly-trafficked areas and competes with asphalt for ride quality when properly constructed and maintained. Sealcoat pavement can provide service for ten or more years before the surface layer deteriorates and needs to be replaced.

Unpaved Surfacing

Typical benefits and tradeoffs for non-hard surfacing include:

- **Gravel:** Gravel is a low-cost, easy-to-maintain road surface made from layers of soil and aggregate (gravel). However, there are several potential drawbacks such as dust, mud, and ride smoothness when maintenance is delayed or traffic volume exceeds design expectations. Gravel roads require frequent low-cost maintenance activities. Gravel can be very cost effective for lower-volume, lower-speed roads. In the right conditions, a properly constructed and maintained gravel road can provide a service life comparable to an HMA pavement and can be significantly less expensive than the other pavement types.

Pavement Condition

Besides traffic congestion, pavement condition is what road users typically notice most about the quality of the roads that they regularly use—the better the pavement condition, the more satisfied users are with the service provided by the roadwork performed by road-owning agencies. Pavement condition is also a major factor in determining the most cost-effective treatment—that is, routine maintenance, capital preventive maintenance, or structural improvement—for a given section of pavement. As pavements age, they transition between “windows” of opportunity when a specific type of treatment can be applied to gain an increase in quality and extension of service life. Routine maintenance is day-to-day, regularly-scheduled, low-cost activity applied to “good” roads to prevent water or debris intrusion. Capital preventive maintenance (CPM) is a planned set of cost-effective treatments for “fair” roads that corrects pavement defects, slows further deterioration, and maintains the functional condition without increasing structural capacity. Taylor uses pavement condition and age to anticipate when a specific section of pavement will be a potential candidate for preventive maintenance. More detail on this topic is included in the *Pavement Treatment* section of this primer.

Pavement condition data is also important because it allows road owners to evaluate the benefits of preventive maintenance projects. This data helps road owners to identify the most cost-effective use of road construction and maintenance dollars. Further, historic pavement condition data can enable

road owners to predict future road conditions based on budget constraints and to determine if a road network's condition will improve, stay the same, or degrade at the current or planned investment level. This analysis can help determine how much additional funding is necessary to meet a network's condition improvement goals.

Paved Road Condition Rating System

Taylor is committed to monitoring the condition of its road network and using pavement condition data to drive cost-effective decision-making and preservation of valuable road assets. Taylor uses the Pavement Surface Evaluation and Rating (PASER) system to assess its paved roads. PASER was developed by the University of Wisconsin Transportation Information Center to provide a simple, efficient, and consistent method for evaluating road condition through visual inspection. The widely-used PASER system has specific criteria for assessing asphalt, concrete, sealcoat, and brick and block pavements. Information regarding the PASER system and PASER manuals may be found on the TAMC website at: http://www.michigan.gov/tamc/0,7308,7-356-82158_82627--,00.html.

The TAMC has adopted the PASER system for measuring statewide pavement conditions in Michigan for asphalt, concrete, composite, sealcoat, and brick-and-block paved roads. Broad use of the PASER system means that data collected at Taylor is consistent with data collected statewide. PASER data is collected using trained inspectors in a slow-moving vehicle using GPS-enabled data collection software provided to road-owning agencies at no cost to them. The method does not require extensive training or specialized equipment, and data can be collected rapidly, which minimizes the expense for collecting and maintaining this data.

The PASER system rates surface condition using a 1-10 scale where 10 is a brand new road with no defects that can be treated with routine maintenance, 5 is a road with distresses but is structurally sound that can be treated with preventive maintenance, and 1 is a road with extensive surface and structural distresses that is in need of total reconstruction.

Roads with lower PASER scores generally require costlier treatments to restore their quality than roads with higher PASER scores. The cost effectiveness of treatments generally decreases as the PASER number decreases. In other words, as a road deteriorates, it costs more dollars per mile to fix it, and the dollars spent are less efficient in increasing the road's service life. Nationwide experience and asset management principles tell us that a road that has deteriorated to a PASER 4 or less will cost more to improve and the dollars spent are less efficient. Understanding this cost principle helps to draw meaning from the current PASER condition assessment.

The TAMC has developed statewide definitions of road condition by creating three simplified condition categories—"good", "fair", and "poor"—that represent bin ranges of PASER scores having similar contexts with regard to maintenance and/or reconstruction. The definitions of these rating conditions are:

- "Good" roads, according to the TAMC, have PASER scores of 8, 9, or 10. Roads in this category have very few, if any, defects and only require minimal maintenance; they may be kept in this category longer using PPM. These roads may include those that have been recently seal coated or newly constructed. **Figure 0-1** illustrates an example of a road in this category.
- "Fair" roads, according to the TAMC, have PASER scores of 5, 6, or 7. Roads in this category still show good structural support, but their surface is starting to deteriorate. **Figure 0-1** illustrates two road examples in this category. CPM can be cost effective for maintaining the road's "fair" condition or even raising it to "good" condition before the structural integrity of the pavement has been severely impacted. CPM treatments can be likened to shingles on a roof of a house: while the shingles add no structural value, they protect the house from structural damage by maintaining the protective function of a roof covering.
- "Poor" roads, according to the TAMC, have PASER scores of 1, 2, 3, or 4. These roads exhibit evidence that the underlying structure is failing, such as alligator cracking and rutting. These roads must be rehabilitated with treatments like a heavy overlay, crush and shape, or total reconstruction. **Figure 0-1** illustrates a road in this category.



Figure 0-1. *Top image, right*– PASER 8 road that is considered "good" by the TAMC exhibit only minor defects. *Second image, right*– PASER 5 road that is considered "fair" by the TAMC. Exhibiting structural soundness but could benefit from CPM. *Third image, right*– PASER 6 road that is considered "fair" by the TAMC. *Bottom image, right*– PASER 2 road that is considered "poor" by the TAMC exhibiting significant structural distress.

The TAMC's good, fair, and poor categories are based solely on the definitions, above. Therefore, caution should be exercised when comparing other condition assessments with these categories

because other condition assessments may have “good”, “fair”, or “poor” designations similar to the TAMC condition categories but may not share the same definition. Often, other condition assessment systems define the “good”, “fair”, and “poor” categories differently, thus rendering the data of little use for cross-system comparison. The TAMC’s definitions provide a statewide standard for all of Michigan’s road-owning agencies to use for comparison purposes.

PASER data is collected 100 percent every two years on all federal-aid-eligible roads in Michigan. The TAMC dictates and funds the required training and the format for this collection, and it shares the data regionally and statewide. In addition, Taylor collects 100 percent of its paved non-federal-aid-eligible network using consultants to complete the work.

Pavement Treatments

Selection of repair treatments for roads aims to balance costs, benefits, and road life expectancy. All pavements are damaged by water, traffic weight, freeze/thaw cycles, and sunlight. Each of the following treatments and strategies—reconstruction, structural improvements, capital preventive maintenance, and others used by Taylor—counters at least one of these pavement-damaging forces.

Reconstruction

Pavement reconstruction treats failing or failed pavements by completely removing the old pavement and base and constructing an entirely new road (**Figure 0-2**). Every pavement has to eventually be reconstructed and it is usually done as a last resort after more cost-effective treatments are done, or if the road requires significant changes to road geometry, base, or buried utilities. Compared to the other treatments, which are all improvements of the existing road, reconstruction is the most extensive rehabilitation of the roadway and therefore, also the most expensive per mile and most disruptive to regular traffic patterns. Reconstructed pavement will subsequently require one or more of the previous maintenance treatments to maximize service life and performance. A reconstructed road lasts approximately 15 years and costs \$250,000 per lane mile. The following descriptions outline the main reconstruction treatments used by Taylor.

Figure 0-2. Examples of Reconstruction Treatments—(left) Reconstructing a Road and (right) Road Prepared for Full-Depth Repair



Full-Depth Concrete Repair

Full-depth concrete repair removes sections of damaged concrete pavement and replaces it with new concrete of the same dimensions (Figure 0-2). It is usually performed on isolated deteriorated joint locations or entire slabs that are much further deteriorated than adjacent slabs. The purpose is to restore the riding surface, delay water infiltration, restore load transfer from one slab to the next, and eliminate the need to perform costly temporary patching. This repair lasts approximately twelve years.

Ditching (for Unpaved Roads)

Water needs to drain away from any roadway to delay softening of the pavement structure, and proper drainage is critical for unpaved roads where there is no hard surface on top to stop water infiltration into the road surface and base. To improve drainage, new ditches are dug or old ones are cleaned out. Unpaved roads typically need to be re-ditched every five years.

Structural Improvement

Roads requiring structural improvements exhibit alligator cracking and rutting and rated poor in the TAMC scale. Road rutting is evidence that the underlying structure is beginning to fail and it must be either rehabilitated with a structural treatment. Examples of structural improvement treatments include HMA overlay with or without milling, and crush and shape (Figure 0-3). The following descriptions outline the main structural improvement treatments used by Taylor

Figure 0-3. Examples of Structural Improvement Treatments—from Left) HMA Overlay on an Unmilled Pavement, Milling Asphalt Pavement, and Pulverization of a Road During a Crush-and-Shape Project



Hot-Mix Asphalt (HMA) Overlay with/without Milling

An HMA overlay is a layer of new asphalt (liquid asphalt and stones) placed on an existing pavement (Figure 0-3). Depending on the overlay thickness, this treatment can add significant structural strength. This treatment also creates a new wearing surface for traffic and seals the pavement from water, debris, and sunlight damage. An HMA overlay lasts approximately five to ten years. The top layer of severely damaged pavement can be removed by the milling, a technique that helps prevent

structural problems from being quickly reflected up to the new surface. Milling is also done to keep roads at the same height of curb and gutter that is not being raised or reinstalled in the project.

Crush and Shape

During a crush and shape treatment, the existing pavement and base are pulverized and then the road surface is reshaped to correct imperfections in the road's profile (Figure 0-3). An additional layer of gravel is often added along with a new wearing surface such as an HMA overlay or chip seal. Additional gravel and an HMA overlay give an increase in the pavements structural capacity. This treatment is usually done on rural roads with severe structural distress; Adding gravel and a wearing surface makes it more prohibitive for urban roads if the curb and gutter is not raised up. Crush and shape treatments last approximately 14 years.

Capital Preventive Maintenance

Capital preventive maintenance (CPM) addresses pavement problems of fair-rated roads before the structural integrity of the pavement has been severely impacted. CPM is a planned set of cost-effective treatments applied to an existing roadway that slows further deterioration and that maintains or improves the functional condition of the system without significantly increasing the structural capacity. Examples of such treatments include crack seal, fog seal, chip seal, slurry seal, and microsurface (Figure 0-4). The purpose of the following CPM treatments is to protect the pavement structure, slow the rate of deterioration, and/or correct pavement surface deficiencies. The following descriptions outline the main CPM treatments used by Taylor.

Figure 0-4. Examples of Capital Preventive Maintenance Treatments—(from Left) Crack Seal, Fog Seal, Chip Seal, and Slurry Seal/Microsurface.



Crack Sea

Water that infiltrates the pavement surface softens the pavement structure and allows traffic loads to cause more damage to the pavement than in normal dry conditions. Crack sealing helps prevent water infiltration by sealing cracks in the pavement with asphalt sealant (Figure 0-4). Taylor seals pavement cracks early in the life of the pavement to keep it functioning as strong as it can and for as long as it can. Crack sealing lasts approximately two years. Even though it does not last very long compared to other treatments, it does not cost very much compared to other treatments. This makes

it a very cost-effective treatment when Taylor looks at what crack filling costs per year of the treatment's life.

Chip Seal

A chip seal, also known as a sealcoat, is a two-part treatment that starts with liquid asphalt sprayed onto the old pavement surface followed by a single layer of small stone chips spread onto the wet liquid asphalt layer (Figure 0-4). The liquid asphalt seals the pavement from water and debris and holds the stone chips in place, providing a new wearing surface for traffic that can correct friction problems and helping to prevent further surface deterioration. Chip seals are best applied to pavements that are not exhibiting problems with strength, and their purpose is to help preserve that strength. These treatments last approximately five years.

Partial-Depth Concrete Repair

A partial-depth concrete repair involves removing spalled (i.e., fragmented) or delaminated (i.e., separated into layers) areas of concrete pavement, usually near joints and cracks and replacing with new concrete (Figure 0-5). This is done to provide a new wearing surface in isolated areas, to slow down water infiltration, and to help delay further freeze/thaw damage. This repair lasts approximately five years.

Figure 0-5. An Examples of Capital Preventive Maintenance Treatment is Concrete Road Prepared for Partial-Depth Repair.



Maintenance

Maintenance is the most cost-effective strategy for managing road infrastructure and prevents good and fair roads from reaching the poor category, which require costly rehabilitation and reconstruction treatments to create a year of service life. It is most effective to spend money on routine maintenance and CPM treatments, first; then, when all maintenance project candidates are treated, reconstruction and rehabilitation can be performed as money is available. This strategy is called a "mix-of-fixes" approach to managing pavements.

1.0 PAVEMENT ASSETS

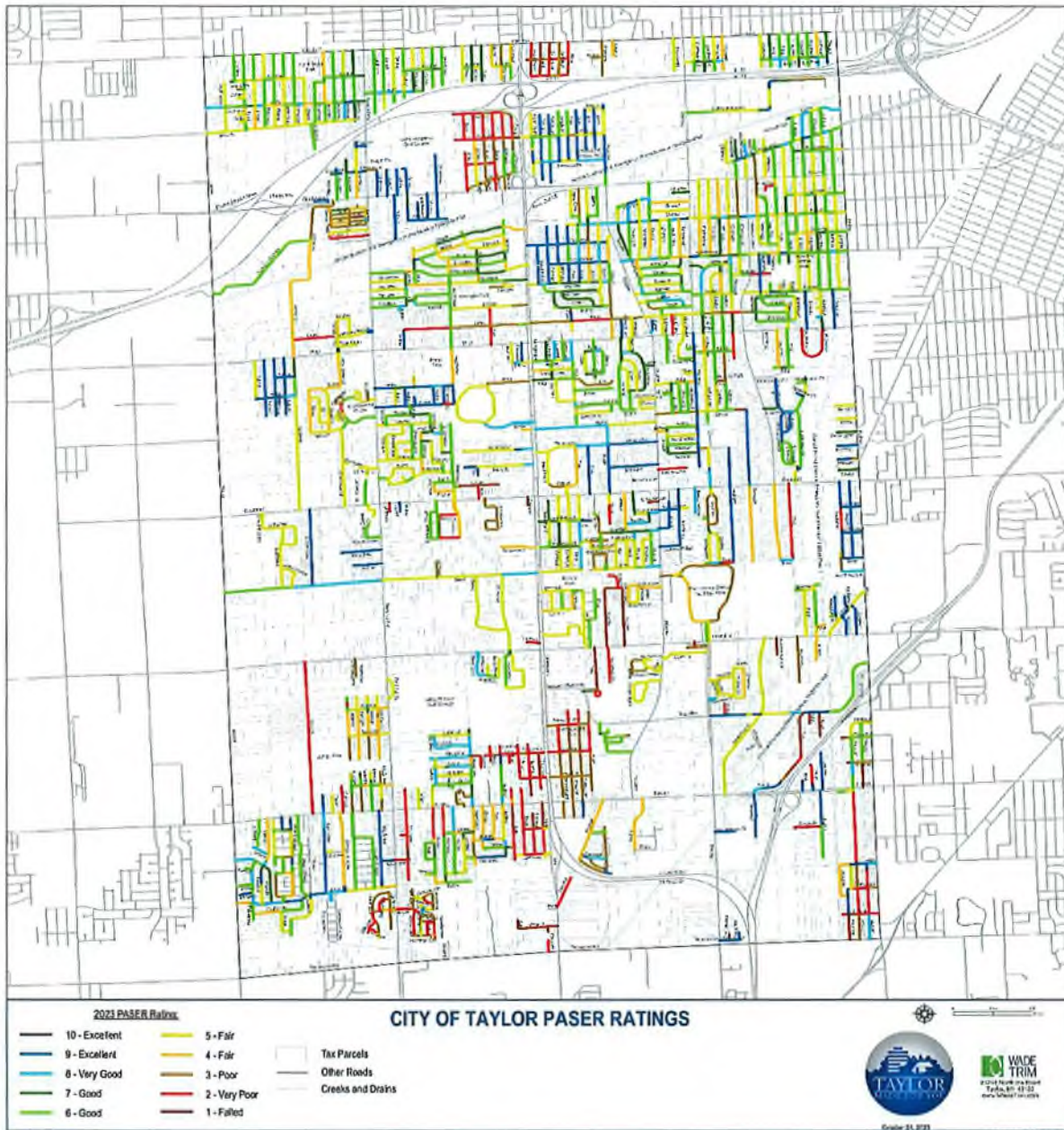
Building a mile of new road can cost over \$1 million due to the large volume of materials and equipment that are necessary. The high cost of constructing road assets underlines the critical nature of properly managing and maintaining the investments made in this vital infrastructure. The specific needs of every mile of road within an agency's overall road network is a complex assessment, especially when considering rapidly changing conditions and the varying requisites of road users; understanding each road-mile's needs is an essential duty of the road-owning agency.

In Michigan, many different governmental units (or agencies) own and maintain roads, so it can be difficult for the public to understand who is responsible for items such as planning and funding construction projects, [patching] repairs, traffic control, safety, and winter maintenance for any given road. MDOT is responsible for state trunkline roads, which are typically named with "M", "I", or "US" designations regardless of their geographic location in Michigan. Cities and villages are typically responsible for all public roads within their geographic boundary with the exception of the previously mentioned state trunkline roads managed by MDOT. County road commissions (or departments) are typically responsible for all public roads within the county's geographic boundary, with the exception of those managed by cities, villages, and MDOT.

In cases where non-trunkline roads fall along jurisdictional borders, local and intergovernmental agreements dictate ownership and maintenance responsibility. Quite frequently, roads owned by one agency may be maintained by another agency because of geographic features that make it more cost effective for a neighboring agency to maintain the road instead of the actual road owner. Other times, road-owning agencies may mutually agree to coordinate maintenance activities in order to create economies of scale and take advantage of those efficiencies.

The City of Taylor is responsible for a total of 195.287 centerline of public roads, as shown in **Figure 1-1**.

Figure 1-1. Map Showing Location of Taylor’s Paved Roads (I.E., Those Managed By Taylor) and their Current Condition for Paved Roads



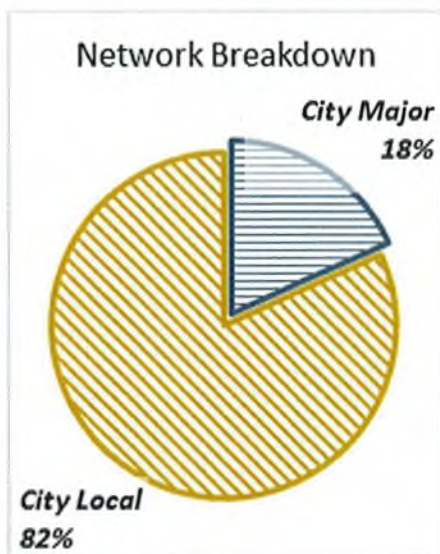
1.1 Inventory

Michigan Public Act 51 of 1951 (PA 51), which defines how funds from the Michigan Transportation Fund (MTF) are distributed to and spent by road-owning agencies, classifies roads owned by Taylor as either city major or city local roads. State statute prioritizes expenditures on the city major road network.

Of the 195.287 centerline of public roads owned and/or managed by Taylor, approximately 82% of all County Primary roads are classified as federal aid eligible, which allows them to receive federal funding for their maintenance and construction. Only 1% of County Local roads are considered federal aid eligible, which means state and local funds must be used to manage these roads.

Figure 1-2 illustrates the percentage of roads owned by Taylor that are classified as city major and city local roads.

Figure 1-2. Percentage of City Major and City Local Roads for Taylor



Taylor also owns and manages 0.955 miles of unpaved roads

1.1.1 Types

Taylor has multiple types of pavements in its jurisdiction, including: asphalt, concrete, and undefined; it also has unpaved roads (i.e., gravel and/or earth). Factors influencing pavement type include cost of construction, cost of maintenance, frequency of maintenance, type of maintenance, asset life, and road user experience. More information on pavement types is available in the Introduction's Pavement Primer.

Figure 1-3 illustrates the percentage of various pavement types that Taylor has in its network.

Figure 1-3. Percentage of Various Pavement types

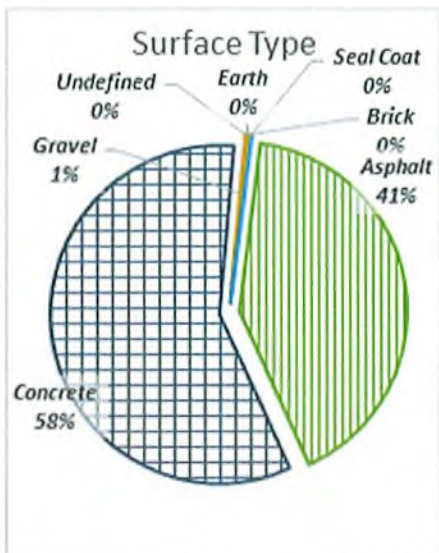


Figure 1-3 shows the pavement type by City boundary for Taylor's jurisdiction

1.1.2 Locations

Locations and sizes of each asset can be found in Taylor's Roadsoft database. For more detail, please refer to the agency contact listed in the *Introduction* of this pavement asset management plan.

1.1.3 Condition

The road characteristic that road users most readily notice is pavement condition. Pavement condition is a major factor in determining the most cost-effective treatment—that is, routine maintenance, capital preventive maintenance, or structural improvement—for a given section of pavement. Taylor uses pavement condition and age to anticipate when a specific section of pavement will be a potential candidate for preventive maintenance. Pavement condition data enables Taylor to evaluate the benefits of preventive maintenance projects and to identify the most cost-effective use of road construction and maintenance dollars. Historic pavement condition data can be used to predict future road conditions based on budget constraints and to determine if a road network's condition will improve, stay the same, or degrade at the current or planned investment level. This analysis helps to determine how much additional funding is necessary to meet a network's condition improvement goals. More detail on this topic is included in the *Introduction's Pavement Primer*.

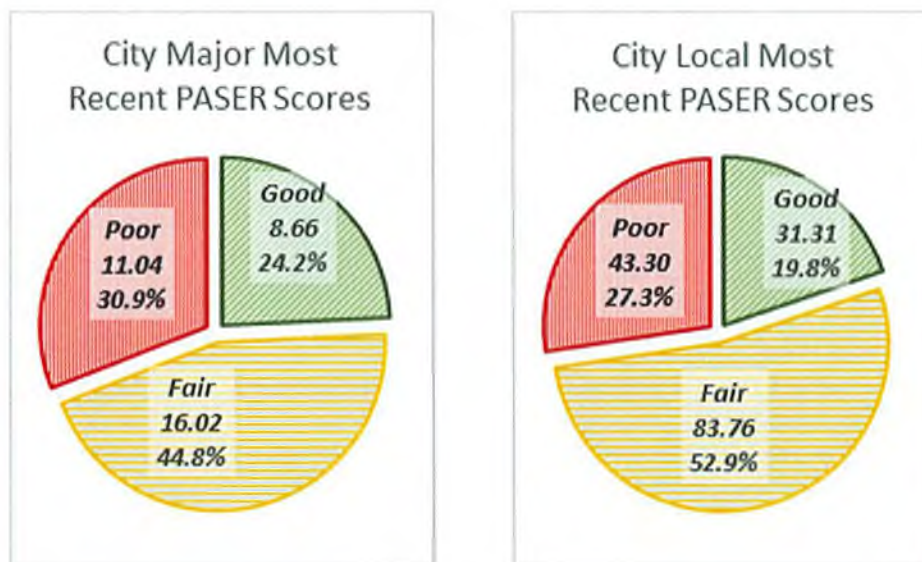
Paved Roads

Taylor is committed to monitoring the condition of its road network and using pavement condition data to drive cost-effective decision-making and preservation of valuable road assets. Taylor uses the Pavement Surface Evaluation and Rating (PASER) system, which has been adopted by the TAMC for measuring statewide pavement conditions, to assess its paved roads. The PASER system provides a simple, efficient, and consistent method for evaluating road condition through visual inspection. More information regarding the PASER system can be found in the Introduction's Pavement Primer.

Taylor collects 100 percent of its PASER data every two years on all federal-aid-eligible roads in Michigan. In addition, Taylor collects 100 percent of its paved non-federal-aid-eligible network using its own staff and resources.

Taylor's 2024 paved city major road network has 24 percent of roads in the TAMC good condition category, 45 percent in fair, and 31 percent in poor (Figure 1-4A). The paved city local road network has 20 percent in good, 53 percent in fair, and 27 percent in poor (Figure 1-4B).

Figure 1-4. (A) Left: Taylor Paved City Major Road Network Conditions By Percentage Of Good, Fair, Or Poor, And (B) Right: Paved City Local road network conditions by percentage of good, fair, or poor

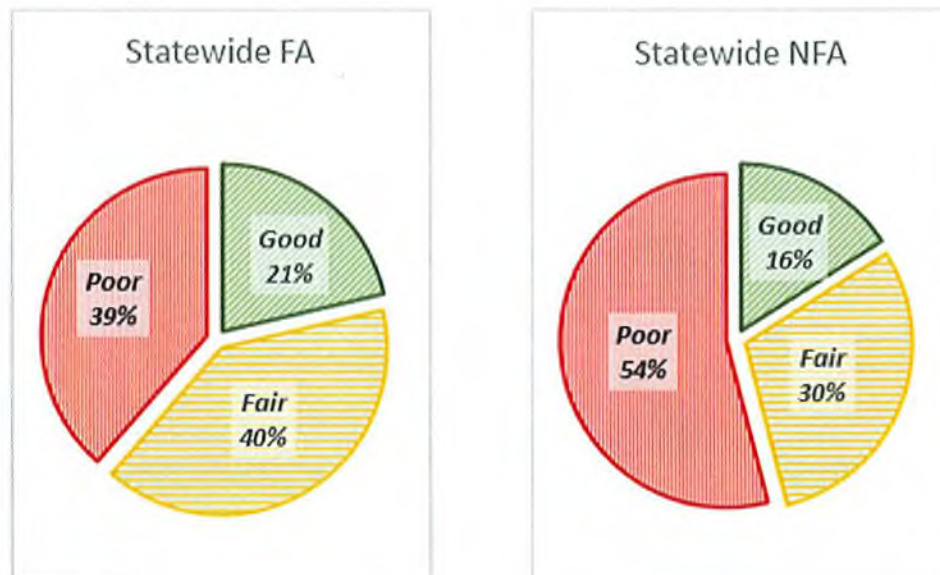


In comparison, the statewide paved city major road network has 21 percent of roads in the TAMC good condition category, 40 percent in fair, and 39 percent in poor (Figure 1-4A). The statewide paved city local road network has 16 percent in good, 30 percent in fair, and 54 percent in poor (Figure 1-4B). Comparing Figure 1-4A and Figure 1-4B shows that Taylor's paved city major road network is approximately the same than similarly-classified roads in the rest of the state, while

Figure 1-5B and Figure 1-5B show that Taylor's paved city local road network is slightly worse than similarly-classified roads in the rest of the state. Other road condition graphs can be viewed on the TAMC pavement condition dashboard at:

<http://www.mcgi.state.mi.us/mitrp/Data/PaserDashboard.aspx>.

Figure 1-5. (A) Left: Statewide Paved City Major Road Network Conditions By Percentage Of Good, Fair, Or Poor, And (B) Right: Paved City Local Road network conditions by percentage of good, fair, or poor



The city of Taylor's road network is pretty close to the state average with just some variances between the different categories. This is due to the fact that Talor is investing money into their road system.

Figure 1-6 and Figure 1-7 show the number of miles for Taylor's roads with PASER scores expressed in TAMC definition categories for the paved city major road network (Figure 1-6) and the paved city local road network (Figure 1-7). Taylor considers road miles on the transition line between good and fair (PASER 8) and the transition line between fair and poor (PASER 5) as representing parts of the road network where there is a risk of losing the opportunity to apply less expensive treatments that gain significant improvements in service life.

Figure 1-6. Taylor Paved City Local Network Condition By PASER Rating. Bar Graph Colors Correspond to Good/Fair/Poor TAMC Designations.

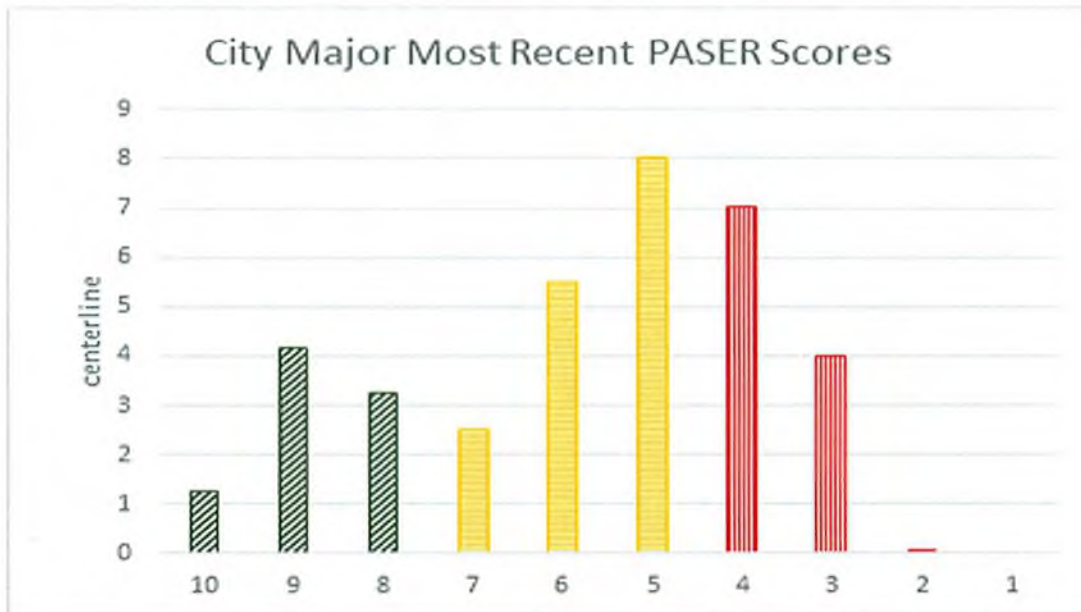


Figure 1-7. Taylor Paved City Local Network Condition By PASER Rating. Bar Graph Colors Correspond To Good/Fair/Poor TAMC Designations.

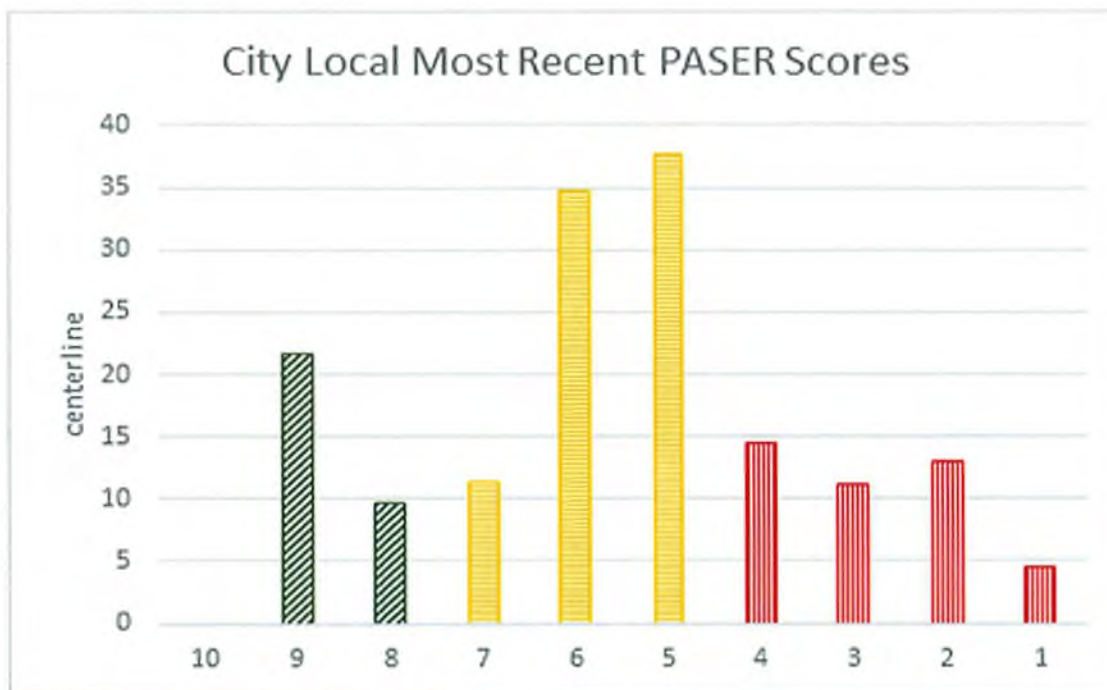
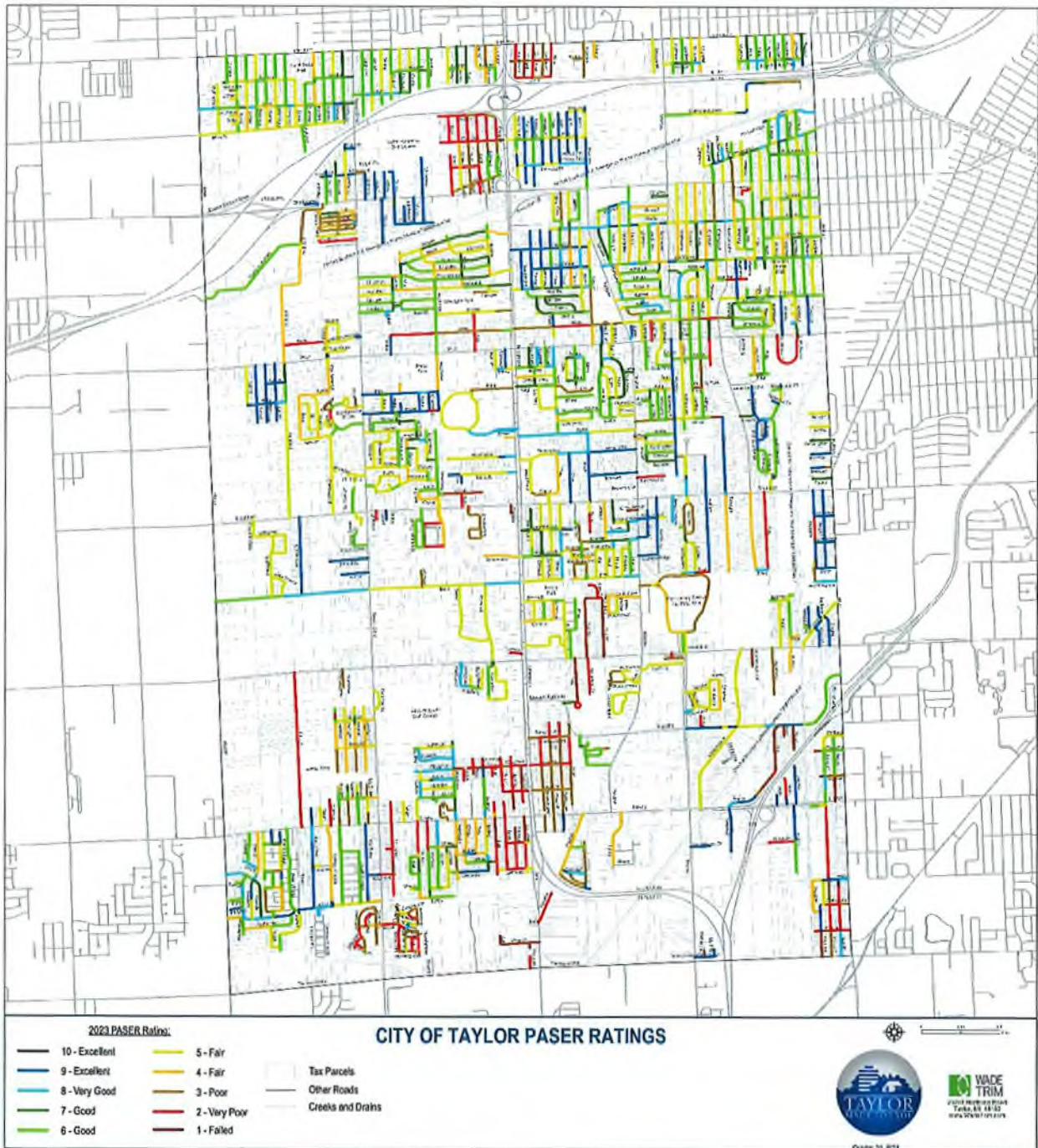


Figure 1-8 provides a map illustrating the geographic location of paved roads and their respective PASER condition. An online version of the most recent PASER data is located at <https://www.mcgi.state.mi.us/tamcMap/>.

Figure 1-8. Map of the Current Paved Road Condition



The City is working towards increasing the overall condition of their road network. Historically, the overall quality of Taylor's paved city major roads have been increasing, as can be observed in **Figure 1-9**. This is due to the fact that Talor has been actively working towards applying the right fix for a road segment at the right time.

Comparing Taylor's paved city major road condition trends illustrated in **Figure 1-9** with overall statewide condition trends for similarly-classified roads, which are illustrated in **Figure 1-10**, shows a similar trend locally as in the rest of the state.

This similar trend can be seen in recent years due to the fact that Taylor is changing their repair and maintenance methods to align better with other communities in the state.

Figure 1-9. Historical Taylor Paved City Major Road Network Condition Trend

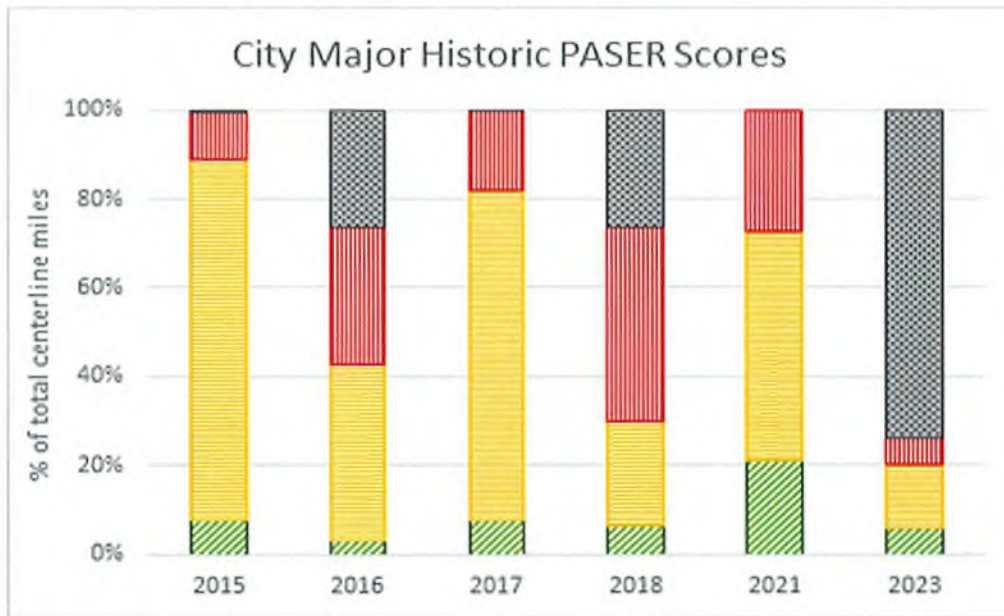
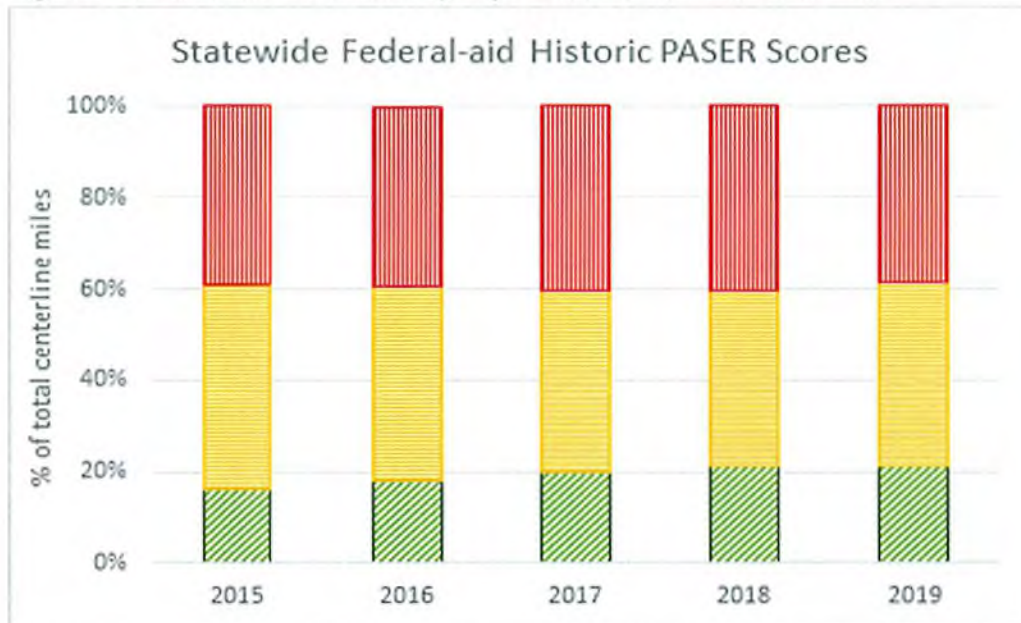


Figure 1-10: Historical Statewide City Major Road Network Condition Trend



Historically, the overall quality of Taylor’s paved city local roads have been increasing, because they have invested money to improve the system. **Figure 1-11** illustrates the condition of the paved city local road network in Taylor while **Figure 1-12** illustrates these conditions statewide.

Comparing Taylor’s paved city local road condition trends illustrated in **Figure 1-11** with overall statewide condition trends for all paved city local roads illustrated in **Figure 1-12** indicates a similar trend locally as in the rest of the state.

Figure 1-11. Historical Taylor Paved City Local Road Network Condition Trend

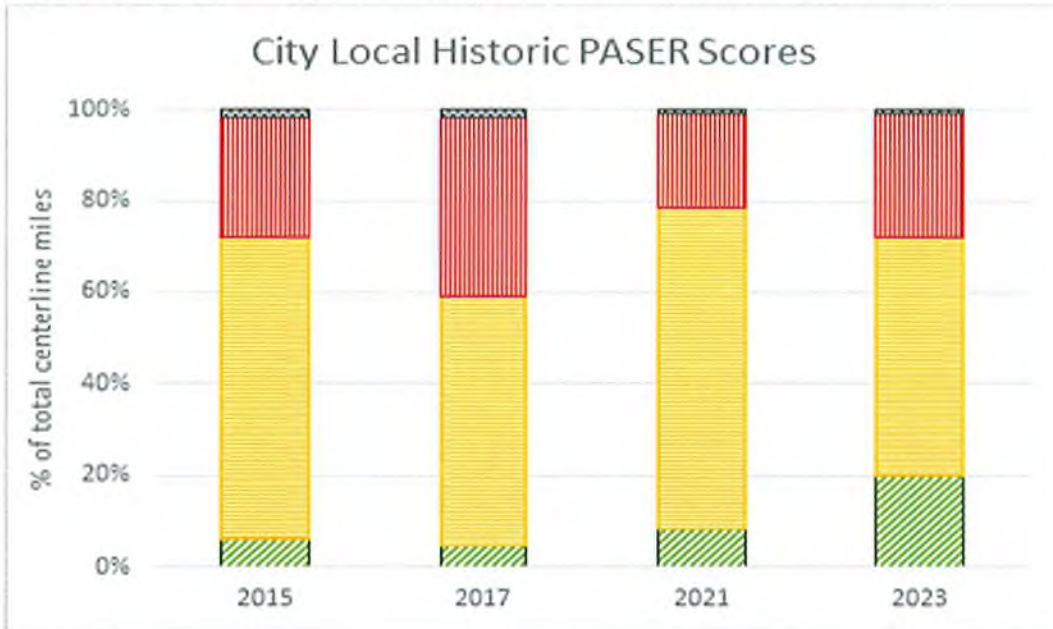
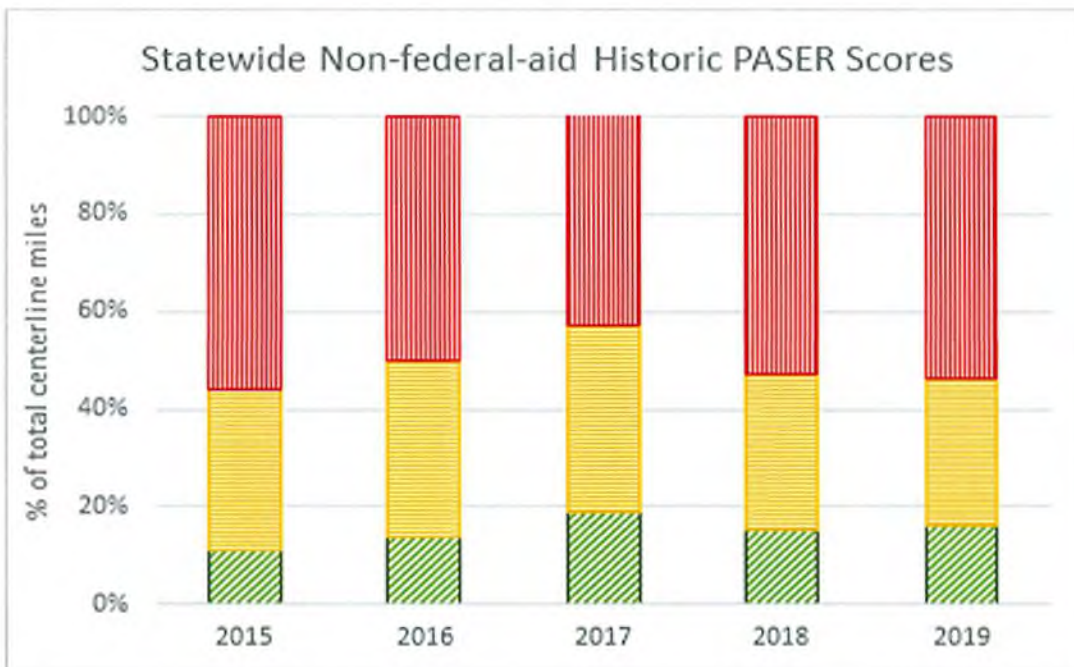


Figure 1-12. Historical Statewide Paved City Local Road Network Condition Trend



1.2 Goals

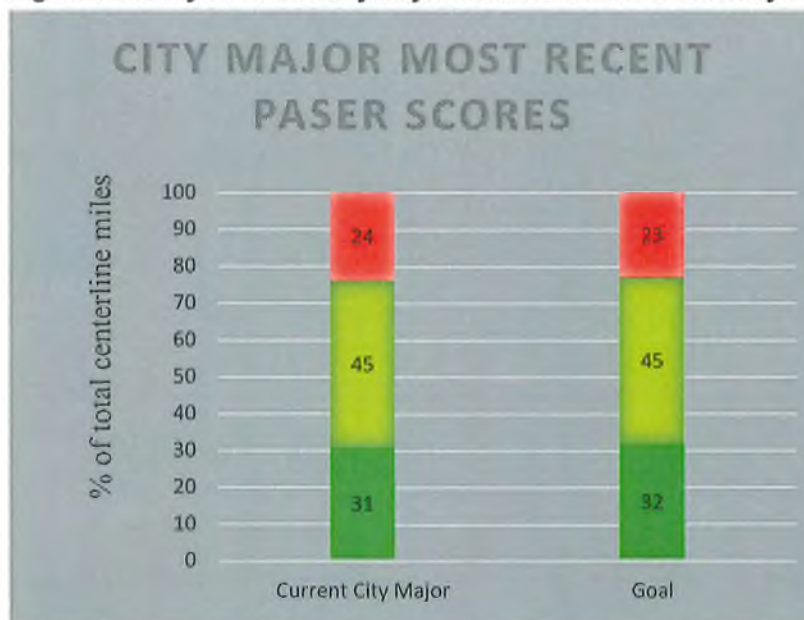
Goals help set expectations to how pavement conditions will change in the future. Pavement condition changes are influenced by water infiltration, soil conditions, sunlight exposure, traffic

loading, and repair work performed. Taylor is not able to control any of these factors fully due to seasonal weather changes, traffic pattern changes, and its limited budget. In spite of the uncontrollable variables, it is still important to set realistic network condition goals that efficiently use budget resources to build and maintain roads meeting taxpayer expectations. An assessment of the progress toward these goals is provided in **Section 1.5 - Pavement Assets: Gap Analysis** section of this plan

1.2.1 Goals for Paved City Major Roads

The overall goal for Taylor’s paved city major road network is to maintain or improve road conditions network-wide at 2024 levels. The baseline condition for this goal is illustrated in **Figure 1-14**.

Figure 1-13. Taylor’s 2024 City Major Road Network Condition by Percentage of Good/Fair/Poor



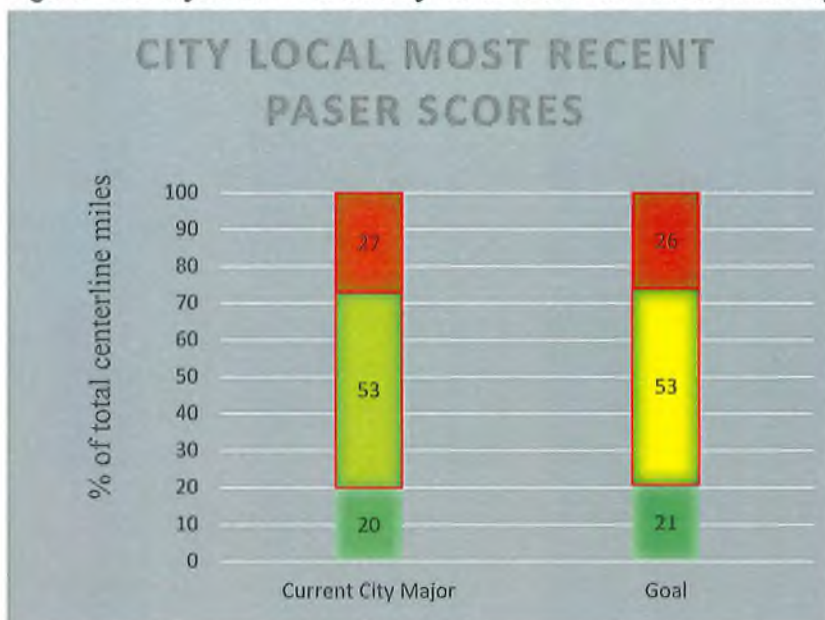
Taylor’s network-level pavement condition strategy for paved city major roads is:

1. Prevent its good and fair (PASER 10 - 5) paved city major from becoming poor (PASER 4 - 1).

1.2.2 Goals for Paved City Local Roads

The overall goal for Taylor’s paved city local road network is to maintain or improve road conditions network-wide at 2024 levels. The baseline condition for this goal is illustrated in **Figure 1-14**.

Figure 1-14. Taylor 2024 Paved City Local Road Network Condition by Percentage of Good/Fair/Poor



Taylor's network-level pavement condition strategy for paved city local roads is:

1. Prevent its good and fair (PASER 10 - 5) paved city local roads from becoming poor (PASER 4 - 1).

1.3 Modelled Trends

Roads age and deteriorate just like any other asset. All pavements are damaged by water, traffic weight, freeze/thaw cycles, sunlight, and traffic weight. To offset natural deterioration and normal wear-and-tear on the road, Taylor must complete treatment projects that either protect and/or add life to its pavements. The year-end condition of the whole network depends upon changes or preservation of individual road section condition that preservation treatments have affected.

Taylor uses many types of repair treatments for its roads, each selected to balance costs, benefits, and road life expectancy. When agency trends are modelled, any gap between goals and accomplishable work becomes evident. Financial resources influence how much work can be accomplished across the network within agency budget and what treatments and strategies can be afforded; a full discussion of Taylor's financial resources can be found in **Section 2.0 - Financial Resources**.

Treatments and strategies that counter pavement-damaging forces include reconstruction, structural improvement, capital preventive maintenance, innovative treatments, and maintenance. Correlating with each PASER score are specific types of treatments best performed either to protect the pavement (CPM) or to add strength back into the pavement (structural improvement) (Table 1). MDOT provides guidance regarding when a specific pavement may be a candidate for a particular

treatment. These identified PASER scores “trigger” the timing of projects appropriately to direct the right pavement fix at the right time, thereby providing the best chance for a successful project. The information provided in **Table 1-1** is a guide for identifying potential projects; however, this table should not be the sole criteria for pavement treatment selection. Other information such as future development, traffic volume, utility projects, and budget play a role in project selection. This table should not be a substitute for engineering judgement.

Table 1-1. Service Life Extension (in Years) for Pavement Types Gained by Fix Type¹

Fix Type	Life Extension (in years)*			PASER
	Flexible	Composite	Rigid	
HMA crack treatment	1-3	1-3	N/A	6-7
Overband crack filling	1-2	1-2	N/A	6-7
One course non-structural HMA overlay	5-7	4-7	N/A	4-5****
Mill and one course non-structural HMA overlay	5-7	4-7	N/A	3-5
Single course chip seal	3-6	N/A	N/A	5-7†
Double chip seal	4-7	3-6	N/A	5-7†
Single course microsurface	3-5	**	N/A	5-6
Multiple course microsurface	4-6	**	N/A	4-6****
Ultra-thin HMA overlay	3-6	3-6	N/A	4-6****
Paver placed surface seal	4-6	**	N/A	5-7
Full-depth concrete repair	N/A	N/A	3-10	4-5***
Concrete joint resealing	N/A	N/A	1-3	5-8
Concrete spall repair	N/A	N/A	1-3	5-7
Concrete crack sealing	N/A	N/A	1-3	4-7
Diamond grinding	N/A	N/A	3-5	4-6
Dowel bar retrofit	N/A	N/A	2-3	3-5***
Longitudinal HMA wedge/scratch coat with surface treatment	3-7	N/A	N/A	3-5****
Flexible patching	**	**	N/A	N/A
Mastic joint repair	1-3	1-3	N/A	4-7
Cape seal	4-7	4-7	N/A	4-7
Flexible interlayer “A”	4-7	4-7	N/A	4-7
Flexible interlayer “B” (SAMI)	4-7	4-7	N/A	3-7
Flexible interlayer “C”	4-7	4-7	N/A	3-7

	Life Extension (in years)*			
Fiber reinforced flexible membrane	4-7	4-7	N/A	3-7
Fog seal	**	**	N/A	7-10
GSB 88	**	**	N/A	7-10
Mastic surface treatment	**	**	N/A	7-10
Scrub seal	**	**	N/A	4-8
* The time range is the expected life extending benefit given to the pavement, not the anticipated longevity of the treatment.				
** Data is not available to quantify the life extension.				
*** The concrete slabs must be in fair to good condition.				
**** Can be used on a pavement with a PASER equal to 3 when the sole reason for rating is rutting or severe raveling of the surface asphalt layer.				
† For PASER 4 or less providing structural soundness exists and that additional pre-treatment will be required for example, wedging, bar seals, spot double chip seals, injection spray patching or other pre-treatments.				
‡ Part of Appendix D-1 from MDOT Local Agency Programs Guidelines for Geometrics on Local Agency Projects 2017 Edition Approved Preventive Maintenance Treatments				

1.4 Planned Projects

Taylor plans construction and maintenance projects several years in advance. A multi-year planning threshold is required due to the time necessary to plan, design, and finance construction and maintenance projects on the paved city major road network. This includes planning and programming requirements from state and federal agencies that must be met prior to starting a project and can include studies on environmental and archeological impacts, review of construction and design documents and plans, documentation of rights-of-way ownership, planning and permitting for storm water discharges, and other regulatory and administrative requirements.

Per PA 499 of 2002 (later amended by PA 199 of 2007), road projects for the upcoming three years are required to be reported annually to the TAMC. Planned projects represent the best estimate of future activity; however, changes in design, funding, and permitting may require Taylor to alter initial plans. Project planning information is used to predict the future condition of the road networks that Taylor maintains. **Section 1.3 – Pavement Assets: Modelled Trends** of this plan provides a detailed analysis of the impact of the proposed projects on their respective road networks.

Table 1-2. Planned Projects

Project	FY	Budget
Non-Motorized	2027	\$200,000
Mortenview (Wick to Ecorse)	2026	\$800,000
Byers (Telegraph to Ecorse)	2026	\$400,000
Superior (Pardee to Racho)	2027	\$2,500,000

Project	FY	Budget
Westlake (Eureka to Dead End South)	2025	\$800,000
Lange (Eureka to Northline)	2026	\$800,000

1.5 Gap Analysis

The current funding levels that Taylor receives are not sufficient to meet the goals for the paved city major road network, the paved city local road network, and the unpaved road network. **Section 1.2 - Pavement Assets: Goals** section of this plan provides further detail about the goals and **Section 1.3 - Pavement Assets: Modelled Trends** section provides further detail on the shortfall given the current budget. However, Taylor believes that the overall condition of this network can be maintained or improved with additional funding for construction and maintenance. An alternate strategy may be used to overcome the current shortfall and meet the goals on the paved city major road network, the paved city local road network, and the unpaved road network:

2.0 FINANCIAL RESOURCES

Public entities must balance the quality and extent of services they can provide with the tax resources provided by citizens and businesses, all while maximizing how efficiently funds are used. Taylor will overview its general expenditures and financial resources currently devoted to pavement maintenance and construction. This financial information is not intended to be a full financial disclosure or a formal report. Michigan agencies are required to submit an Act 51 Report to the Michigan Department of Transportation each year; this is a full financial report that outlines revenues and expenditures. This report can be obtained on our website at <https://www.cityoftaylor.com/177/Budgets-Financial-Reports>.

Taylor has a total budget for pavement asset management of \$2,000,000.

2.1 City Major Network

Taylor has historically spent \$1,000,000 annually on pavement-related projects. Over the next three years, Taylor plans to spend \$1,000,000 on city major-network projects consisting of, but not limited to, reconstruction, overlay, culvert replacement, and preventive maintenance. Spending on projects depends on revenue from Michigan Transportation Fund (MTF), bonds, and federal/state programs.

2.2 City Local Network

Taylor has historically spent \$1,000,000 annually on pavement-related projects. Over the next three years, Taylor plans to spend \$1,000,000 on city local-network projects consisting of, but not limited to, reconstruction, overlay, culvert replacement, and preventive maintenance. Spending on projects depends on revenue from Michigan Transportation Fund (MTF), bonds, and federal/state programs. Many local agencies in Michigan use local tax millages to supplement their road-funding budget. These taxes can provide for additional construction and maintenance for new or existing roads that are also funded using MTF or MDOT funds. Taylor does not have local tax millages in its road-funding budget.

3.0 RISK OF FAILURE ANALYSIS

Transportation infrastructure is designed to be resilient. The system of interconnecting roads and bridges maintained by Taylor provides road users with multiple alternate options in the event of an unplanned disruption of one part of the system. There are, however, key links in the transportation system that may cause significant inconvenience to users if they are unexpectedly closed to traffic. Taylor does not have any critical assets.

4.0 COORDINATION WITH OTHER ENTITIES

An asset management plan provides a significant value for infrastructure owners because it serves as a platform to engage other infrastructure owners using the same shared right of way space. Taylor communicates with both public and private infrastructure owners to coordinate work in the following ways:

Taylor maintains drinking water, sanitary and storm sewer assets in addition to transportation assets. Taylor follows an asset management process for all of its assets by coordinating the upgrade, maintenance, and operation of all major assets.

Planned projects for subsurface infrastructure that Taylor owns are coordinated with the transportation infrastructure plans to maximize value and minimize service disruptions and cost to the public.

Taylor takes advantage of coordinated infrastructure work to reduce cost and maximize value using the following policies:

- Roads which are in poor condition that have a subsurface infrastructure project planned which will destroy more than half the lane width will be rehabilitated or reconstructed full width using transportation funds to repair the balance of the road width.
- Subsurface infrastructure projects which will cause damage to pavements in good condition will be delayed as long as possible, or will consider methods that do not require pavement cuts.
- Subsurface utility projects will be coordinated to allow all under pavement assets to be upgraded in the same project regardless of ownership.
- Road reconstruction projects will not be completed until agency owned sub surface utilities are upgraded to have at least a 40 years of remaining service life.



**Appendix A1.
A Quick Check of Your Highway Network Health**

A Quick Check of Your Highway Network Health

*By Larry Galehouse, Director, National Center for Pavement Preservation
and*

Jim Sorenson, Team Leader, FHWA Office of Asset Management

Historically, many highway agency managers and administrators have tended to view their highway systems as simply a collection of projects. By viewing the network in this manner, there is a certain comfort derived from the ability to match pavement actions with their physical/functional needs. However, by only focusing on projects, opportunities for strategically managing entire road networks and asset needs are overlooked. While the “bottom up” approach is analytically possible, managing networks this way can be a daunting prospect. Instead, road agency administrators have tackled the network problem from the “top down” by allocating budgets and resources based on historical estimates of need. Implicit in this approach, is a belief that the allocated resources will be wisely used and prove adequate to achieve desirable network service levels.

Using a quick checkup tool, road agency managers and administrators can assess the needs of their network and other highway assets and determine the adequacy of their resource allocation effort. A quick checkup is readily available and can be usefully applied with minimum calculations.

It is essential to know whether present and planned program actions (reconstruction, rehabilitation, and preservation) will produce a net improvement in the condition of the network. However, before the effects of any planned actions on the highway network can be analyzed, some basic concepts should be considered.

Assume every lane-mile segment of road in the network was rated by the number of years remaining until the end of life (terminal condition). Remember that terminal condition does not mean a failed road. Rather, it is the level of deterioration that management has set as a minimum operating condition for that road or network. Consider the rated result of the current network condition as shown in Figure 1.

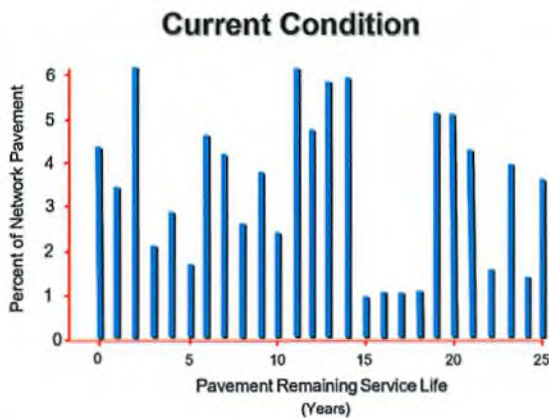


Figure 1 – Current Condition

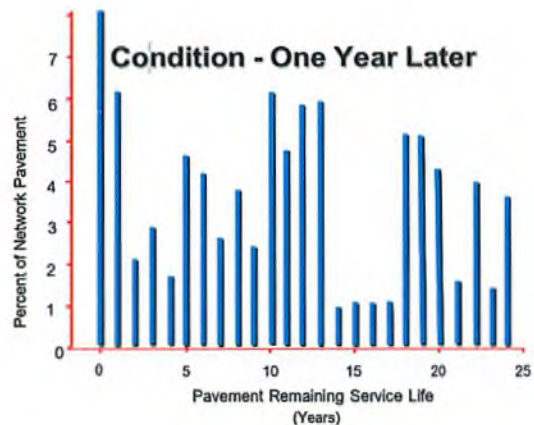


Figure 2 – Condition 1-Year Later

If no improvements are made for one year, then the number of years remaining until the end of life will decrease by one year for each road segment, except for those stacked at zero. The zero- stack will increase significantly because it maintains its previous balance and also becomes the recipient of those roads having previously been stacked with one year remaining. Thus, the entire network will age one year to the condition shown in Figure 2, with the net lane-miles in the zero stack raised from 4% to 8% of the network.

Some highway agencies still subscribe to the old practice of assigning their highest priorities to the reconstruction or rehabilitation of the worst roads. This practice of “worst first”, i.e., continually addressing only those roads in the zero-stack, is a proven death spiral strategy because reconstruction and rehabilitation are the most expensive ways to maintain or restore serviceability. Rarely does sufficient funding exist to sustain such a strategy.

The measurable loss of pavement life can be thought of as the network’s total lane-miles multiplied by 1 year, i.e., lane-mile-years. Consider the following quantitative illustration. Suppose your agency’s highway network consisted of 4,356 lane-miles. Figure 3 shows that without intervention, it will lose 4,356 lane-mile-years per year.

Agency Highway Network = 4,356 lane miles

Each year the network will lose

4,356 lane-mile-years

Figure 3 – Network Lane Miles

To offset this amount of deterioration over the entire network, the agency would need to annually perform a quantity of work equal to the total number of lane-mile-years lost just to maintain the status quo. Performing work which produces fewer than 4,356 lane-mile-years would lessen the natural decline of the overall network, but still fall short of maintaining the

status quo. However, if the agency produces more than 4,356 lane-mile-years, it will improve the network.

In the following example, an agency can easily identify the effect of an annual program consisting of reconstruction, rehabilitation, and preservation projects on its network. This assessment involves knowing the only two components for reconstruction and rehabilitation projects: lane-miles and design life of each project fix. Figure 4 displays the agency's programmed activities for reconstruction and Figure 5 displays it for rehabilitation.

Reconstruction Evaluation

Projects this Year = 2

Project	Design Life	Lane Miles	Lane Mile Years	Lane Mile Cost	Total Cost
No. 1	25 yrs	22	550	\$463,425	\$10,195,350
No. 2	30 yrs	18	540	\$556,110	\$10,009,980
Total =			1,090		\$20,205,330

Figure 4 - Reconstruction

Rehabilitation Evaluation

Projects this Year = 3

Project	Design Life	Lane Miles	Lane Mile Years	Lane Mile Cost	Total Cost
No. 10	18 yrs	22	396	\$263,268	\$5,791,896
No. 11	15 yrs	28	420	\$219,390	\$6,142,920
No. 12	12 yrs	32	384	\$115,848	\$3,707,136
Total =			1,200		\$15,641,952

Figure 5 – Rehabilitation

When evaluating pavement preservation treatments in this analysis, it is appropriate to think in terms of “extended life” rather than design life. The term design life, as used in the reconstruction and rehabilitation tables, relates better to the new pavement’s structural adequacy to handle repetitive loadings and environmental factors. This is not the goal of pavement preservation. Each type of treatment/repair has unique benefits that should be targeted to the specific mode of pavement deterioration. This means that life extension depends on factors such as type and severity of distress, traffic volume, environment, etc. Figure 6 exhibits the agency’s programmed activities for preservation.

Preservation Evaluation

Project	Life Extension	Lane Miles	Lane Mile Years	Lane Mile Cost	Total Cost
No. 101	2 yrs	12	24	\$2,562	\$30,744
No. 102	3 yrs	22	66	\$7,743	\$170,346
No. 103	5 yrs	26	130	\$13,980	\$363,480
No. 104	7 yrs	16	112	\$29,750	\$476,000
No. 105	10 yrs	8	80	\$54,410	\$435,280
Total =			412		\$1,475,850

Figure 6 – Preservation

To satisfy the needs of its highway network, the agency must accomplish 4,356 lane-mile-years of work per year. The agency's program will derive 1,090 lane-mile-years from reconstruction, 1,200 lane-mile-years from rehabilitation, and 412 lane-mile-years from pavement preservation, for a total of 2,702 lane-mile-years. Thus, these programmed activities fall short of the minimum required to maintain the status quo, and hence would contribute to a net loss in network pavement condition of 1,653 lane-mile-years. The agency's programmed tally is shown in Figure 7.

Network Trend

Programmed Activity	Lane-Mile-Years	Total Cost
Reconstruction	1,090	\$20,205,330
Rehabilitation	1,200	\$15,641,952
Preservation	412	\$1,475,850
Total	2,702	\$37,323,132
Network Needs (Loss)	(-) 4,356	
Deficit =	- 1,654	

Figure 7 – Programmed Tally

This exercise can be performed for any pavement network to benchmark its current trend. Using this approach, it is possible to see how various long-term strategies could be devised and evaluated against a policy objective related to total-network condition.

Once the pavement network is benchmarked, an opportunity exists to correct any shortcomings in the programmed tally. A decision must first be made whether to improve the

network condition or just to maintain the status quo. This is a management decision and system goal.

Continuing with the previous example, a strategy will be proposed to prevent further network deterioration until additional funding is secured.

The first step is to modify the reconstruction and rehabilitation (R&R) programs. An agonizing decision must be made about which projects to defer, eliminate, or phase differently with multi-year activity. In Figure 8, reductions are made in the R&R programs to recover funds for less costly treatments in the pavement preservation program. The result of this decision recovered slightly over \$6 million.

Program Modification

<u>Programmed Activity</u>	<u>Lane-Mile-Years</u>	<u>Cost Savings</u>
Reconstruction <i>31 lane miles</i> (-40 lane-miles)	<i>820</i> (-1,090)	\$5,004,990
Rehabilitation <i>77 lane miles</i> (-82 lane-miles)	<i>1,125</i> (-1,200)	\$1,096,950
Pavement Preservation (84 lane-miles)	(412)	0
Total =	<i>2,357</i> (2,702)	\$6,101,940

Figure 8 – Revised R & R Programs

Modifying the reconstruction and rehabilitation programs has reduced the number of lane-mile-years added to the network from 2,702 to 2,357 lane-mile-years. However, using less costly treatments elsewhere in the network to address roads in better condition will increase the number of lane-mile-years added to the network. A palette of pavement preservation treatments, or mix of fixes, is available to address the network needs at a much lower cost than traditional methods.

Preservation treatments are only suitable if the right treatment is used on the right road at the right time. In Figure 9, the added treatments used include concrete joint resealing, thin hot-mix asphalt (HMA) overlay (≤ 1.5”), microsurfacing, chip seal, and crack seal. By knowing the cost per lane-mile and the treatment life-extension, it is possible to create a new strategy (costing \$36,781,144) that satisfies the network need. In this example, the agency saved in excess of \$500,000 from traditional methods (costing \$37,323,132), while erasing the 1,653 lane-mile-year deficit produced by the initial program tally. Network Strategy

Programmed Activity	Lane Mile Years	Total Cost
Reconstruction (31 lane-miles)	820	\$15,200,340
Rehabilitation (77 lane-miles)	1,125	\$14,545,002
Pavement Preservation (84 lane-miles)	412	\$1,475,850
Concrete Resealing (4 years x 31 lane-miles)	124	\$979,600
Thin HMA Overlay (10 years x 16 lane-miles)	160	\$870,560
Microsurfacing (7 years x 44 lane-miles)	308	\$1,309,000
Chip Seal (5 years x 79 lane-miles)	395	\$1,104,420
Crack Seal (2 years x 506 lane-miles)	1,012	\$1,296,372
Total =	4,356	\$36,781,144

Figure 9 – New Program Tally

In a real-world situation, the highway agency would program its budget to achieve the greatest impact on its network condition. Funds allocated for reconstruction and rehabilitation projects must be viewed as investments in the infrastructure. Conversely, funds directed for preservation projects must be regarded as protecting and preserving past infrastructure investments.

Integrating reconstruction, rehabilitation, and preservation in the proper proportions will substantially improve network conditions for the taxpayer while safeguarding the highway investment.



**Appendix B1.
Meeting Minutes Verifying Plan Acceptance by Governing Body**

TIM WOOLLEY
Mayor

MICHELLE TOCCO
Treasurer

City of Taylor

OFFICE OF THE CITY CLERK

CYNTHIA A. BOWER

23555 GODDARD ROAD
TAYLOR, MICHIGAN 48180-4116

PHONE: (734) 374-1474 FAX: (734) 374-1343

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CITY COUNCIL

DOUGLAS A. GEISS
Chairman

JILL BRANDANA
Chairwoman Pro-Tem

TINA DANIELS
CHARLES JOHNSON
LINDSEY ROSE
ANGELA WINTON

At the regular meeting of the Taylor City Council held on January 21, 2025 the following Resolution was adopted:

Motion by: Rose, supported by: Daniels

Resolved: Motion to approve the submission of the 2024 Transportation Asset Management Plan to the State of Michigan, and authorize the execution of the required Proof of Acceptance.
Unanimously carried.


CCR #: 1.27-25

Ayes: Geiss, Brandana, Daniels, Johnson, Rose, Winton

Nays: None

Absent: None

I, Cynthia A. Bower, City Clerk of the City of Taylor, hereby certify that the foregoing is a true and complete copy of a Resolution as adopted by the City Council of the City of Taylor, County of Wayne, Michigan, at a regular meeting held on January 21, 2025 and that said meeting was conducted and public notice of said meeting was given pursuant to and in full compliance with the Open Meetings Act, being Act 267, Public Acts of Michigan, 1976, and that the Minutes of said meeting were kept and will be or have been made available as required by said Act.


Cynthia A. Bower


Date: January 22, 2025



Appendix B. Bridge Safety Inspection Reports

Copies of the 2023 bridge inspection reports are attached for reference purposes.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635		CULVERT SAFETY INSPECTION REPORT		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	

CULVERT INSPECTION KA80

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/02/2024

GENERAL NOTES

Assisted by Adam Budzinski and Blake Cain; NE guardrail end damaged and dates back to 2016. 2'x3'x2'deep sinkhole just north of culvert, along driveway to the east.

NBI INSPECTION

	12/20	12/22	12/24	
1. Culvert Rating (SIA-62)	7	7	7	(12/24) (12/22) (12/20)
2. Channel (SIA-61)	6	6	5	Flow is West to East; Heavy sediment build-up in north and south pipe approx 2ft. Heavy tree branches and garbage build-up at upstream side. Large trees fallen into downstream channel. Heavy brush/small trees growing behind wingwalls at each quadrant. (12/24) Flow is West to East; Heavy sediment build-up in north and south pipe approx 2ft. Heavy tree branches and garbage build-up at upstream side. Large trees fallen into downstream channel. Heavy brush growing behind wingwalls at each quadrant. (12/22) Heavy sediment build-up in north and south pipe ~2ft. Heavy debris build-up at upstream side. Large trees fallen into downstream channel. Due to overnight and current rain, CMP pipes flowing 50% full. (12/20)
3. Scour	6	6	5	Uneven, silty mucky streambed with hole 2 ft deep, at downstream east end of north span pipe. Silt build-up noted at south and north pipe ~ 1ft to 1.5 ft depth. Very mucky bottom from probing. No evidence of undermining of invert. (12/24) Uneven, silty mucky streambed with hole 2 ft deep, at downstream east end of north span pipe. Silt build-up noted at south and north pipe ~ 1ft to 1.5 ft depth. Very mucky bottom from probing. No evidence of undermining of invert. (12/22) Possible scour hole 2 ft deep, near north barrel at downstream end. Silt build-up noted at south and north pipe ~ 1ft to 1.5 ft depth. Very mucky bottom from probing. (12/20)

AASHTO ELEMENTS (English Units)

Element Number	Element Name	Total Quantity	Unit	Good CS1	Fair CS2	Poor CS3	Severe CS4
240	Steel Culvert	44	ft	44 100%	0 0%	0 0%	0 0%
2 span, 2x6 Corrugated steel pipe (8 gage steel) in good condition with staining along water line in each pipe. Silt build-up in both barrels approx 1.5 ft deep. minor rust spots along seam of north barrel. North Culvert has small damage where it meets east headwall.							
861	Culvert Wingwall	4		4 100%	0 0%	0 0%	0 0%
Concrete flared wingwalls in each corner, cast with concrete headwall							
863	Culvert Headwall	2		2 100%	0 0%	0 0%	0 0%

Concrete headwall at each side of structure; minor hairline cracking along vertical construction joint at each headwall. East Headwall has (3) radial cracks around the South barrel, and (5) radial cracks around the north barrel.

MISCELLANEOUS

Guard Rail	Other Items

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	

<u>Item</u>	<u>Rating</u>	<u>Item</u>	<u>Rating</u>
36A. Bridge Railings	0	71. Water Adequacy	8
36B. Transitions	0	72. Approach Alignment	8
36C. Approach Guardrail	0	Special Insp. Equipment	2
36D. Approach Guardrail Ends	0	Underwater Insp. Method	1

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Channel Repair	M	Remove garbage and heavy debris build-up including large tree branches in waterway at both sides of culvert.
Approach Repair	H	Repair guardrail at NE quadrant
Brush Cut	H	Remove brush and trees growing in between roadway pavement and headwalls and behind wingwalls at each corner - clear brush within ROW at culvert. Remove small trees and bushes growing along guardrail and covering signs for full length of guardrail.
Railing Repair	H	Evaluate approach and departing guardrail design for each side of roadway and replace with guardrail meeting current standards.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	

SUPPORTING IMAGES

KA80 12/02/2024



Document Name: RIMG2869.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north at culvert



Document Name: RIMG2870.JPG
 Category: Approach
 Span Number:
 Comments: South Approach looking south



Document Name: RIMG2875.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south at culvert



Document Name: RIMG2876.JPG
 Category: Approach
 Span Number:
 Comments: North approach looking north

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	



Document Name: RIMG2871.JPG
 Category: Railing
 Span Number:
 Comments: West guardrail



Document Name: RIMG2874.JPG
 Category: Railing
 Span Number:
 Comments: East guardrail



Document Name: RIMG2897.JPG
 Category: Railing
 Span Number:
 Comments: NE guardrail end damaged and dates back to 2016



Document Name: RIMG2883.JPG
 Category: Culvert
 Span Number:
 Comments: North Barrel of Culvert

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG2885.JPG
 Category: Culvert
 Span Number:
 Comments: Damage to North barrel at headwall



Document Name: RIMG2888.JPG
 Category: Culvert
 Span Number:
 Comments: south barrel



Document Name: RIMG2873.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking west



Document Name: RIMG2879.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking east

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

CULVERT SAFETY INSPECTION REPORT

Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG2886.JPG
 Category: Channel
 Span Number:
 Comments: scour at north barrel of culvert



Document Name: RIMG2877.JPG
 Category: Other
 Span Number:
 Comments: 2'x3'x2'deep sinkhole just north of culvert, along driveway to the east.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

STRUCTURE INVENTORY AND APPRAISAL

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)
Feature	Length / Width / Spans	Owner	
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing



Bridge History, Type, Materials

27 - Year Built	1965
106 - Year Reconstructed	
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	3 19
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	9
79 - Rail Type	1
80 - Post Type	
107 - Deck Type	N
108A - Wearing Surface	N
108B - Membrane	0
108C - Deck Protection	0

Structure Dimensions

34 - Skew	0
35 - Struct Flared	0
45 - Num Main Spans	2
46 - Num Apprs Spans	0
48 - Max Span Length	17.7
49 - Structure Length	34.8
50A - Width Left Curb/SW	0
50B - Width Right Curb/SW	0
33 - Median	0
51 - Width Curb to Curb	0
52 - Width Out to Out	
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/02/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	1
58 - Deck Rating	N
58A/B - Deck Surface/Bottom	
59 - Superstructure Rating	N
59A - Paint Rating	
60 - Substructure Rating	N
61 - Channel Rating	5
62 - Culvert Rating	7

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	7
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	9.06
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016548 17
19 - Detour Length	0
20 - Toll Facility	3
26 - Functional Class	14
28A - Lanes On	2
29 - ADT	1750
30 - Year of ADT	2006
32 - Appr Roadway Width	36.1
32A/B - Ap Pvt Type/Width	6 35.99
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	44.0
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	1
109 - Truck %	3
110 - Truck Network	0
114 - Future ADT	2013
115 - Year Future ADT	2026
Freeway	0

Structure Appraisal

36A - Bridge Railing	0
36B - Rail Transition	0
36C - Approach Rail	0
36D - Rail Termination	0
67 - Structure Evaluation	7
68 - Deck Geometry	N
69 - Underclearance	N
71 - Waterway Adequacy	8
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	8

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	99.9
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	


Proposed Improvements

75 - Type of Work	31 1
76 - Length of Improvement	260.2
94 - Bridge Cost	224
95 - Roadway Cost	22
96 - Total Cost	263
97 - Year of Cost Estimate	1980

Load Rating and Posting

31 - Design Load	5
41 - Open, Posted, Closed	A
63 - Fed Oper Rtg Method	6
64F - Fed Oper Rtg Load	2.46
64MA - Mich Oper Rtg Method	6
64MB - Mich Oper Rtg	2.46
64MC - Mich Oper Truck	19
65 - Inv Rtg Method	6
66 - Inventory Load	2.43
70 - Posting	5
141 - Posted Loading	
193 - Overload Class	A N

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635		WORK RECOMMENDATIONS		
Facility HURON STREET	Latitude / Longitude 42.1973 / -83.2351	MDOT Structure ID 824664382191C05	Structure Condition Good Condition(7)	
Feature FRANK & POET DRAIN	Length / Width / Spans 34.8 /	Owner City: TAYLOR(6643)		
Location S OF EUREKA	Built / Recon. / Paint / Ovly. 1965 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 3 Steel / 19 Culvert	Last NBI Inspection 12/02/2024 / KA80	Scour Evaluation 8 Stable Above Footing	


WORK RECOMMENDATIONS

KA80

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 11635

REQUEST FOR ACTION

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
HURON STREET	42.1973 / -83.2351	824664382191C05	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
FRANK & POET DRAIN	34.8 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
S OF EUREKA	1965 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	3 Steel / 19 Culvert	12/02/2024 / KA80	8 Stable Above Footing	

No inspections available for bridge key 824664382191C05

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		CULVERT SAFETY INSPECTION REPORT		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

CULVERT INSPECTION UH0Y

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024

GENERAL NOTES

Assisted by Adam Budzinski and Blake Cain.

42T/66T/74T load posting signs in place Missing at south approach and also both advance warning signs missing. Guardrail at approach side only.

Weight limit signs in place on both ends of bridge NO

Required advance warning weight limit signs in place NO

Frequency Justification Comments (required when Poor Condition and frequency is equal to 24 months)

Culvert is in overall good condition-structure load posted; inspection frequency is at 24 months

NBI INSPECTION

	12/20	12/22	12/24	
1. Culvert Rating (SIA-62)	7	7	7	(12/24) (12/22) (12/20)
2. Channel (SIA-61)	6	6	6	Heavy sediment accumulating on south side. Channel bottom very silty. Erosion and washout @ SW, NE and SE wingwalls @ end of interface with gabions. Channel at severe skew to road. Culvert skewed to match creek skew (12/24) Heavy sediment accumulating on south side. Channel bottom very silty. Erosion and washout @ SW, NE and SE wingwalls @ end of interface with gabions. Channel at severe skew to road. Culvert skewed to match creek skew (12/22) Heavy sediment accumulating on south side. Channel bottom very silty. Erosion and washout @ SW, NE and SE wingwalls @ end of interface with gabions. Channel at severe skew to road. Culvert skewed to match creek skew (12/20)
3. Scour	7	7	7	No scour, but some loss of concrete at large drain in northeast quadrant. Washout & erosion on NE, SE & SW quads at wingwalls. Gabion baskets along north bank, east of culvert in need of some repair. (12/24) No scour, but some loss of concrete at large drain in northeast quadrant. Washout & erosion on NE, SE & SW quads at wingwalls. Gabion baskets along north bank, east of culvert in need of some repair. (12/22) No scour, but some loss of concrete at large drain in northeast quadrant. Washout & erosion on NE, SE & SW quads at wingwalls. Gabion baskets along north bank, east of culvert in need of some repair. (12/20)

AASHTO ELEMENTS (English Units)

Element Number	Element Name	Total Quantity	Unit	Good CS1	Fair CS2	Poor CS3	Severe CS4
851	Reinf Conc Culvert 3-Sided	33	ft	21 64%	12 36%	0 0%	0 0%

HMA surface hides condition of top of concrete.
Leaking between 2W and 3W
1" settlement between 3W and 4W
3E is displaced 1/4" down
1" gap between beams 3E and 4E
Concrete soffit in good condition with only minor leakage at some joints.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514 CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

861	Culvert Wingwall	4	4	0	0	0
			100%	0%	0%	0%

Reinforced concrete wingwalls at each corner; wingwalls are angled 90 degrees to fascias at SW and NE corners, 0 degrees-parallel to fascia at NW corners and 136 degrees-parallel to reference line at SE corner; no deficiencies noted;

Scour Countermeasure

830	Plain Riprap	640	sq.ft	480	160	0	0
				75%	25%	0%	0%

Riprap channel protection in place through structure

834	Gabion	100	ft	90	10	0	0
				90%	10%	0%	0%

Gabion Basket retaining walls along downstream banks - approx 50 ft long at each bank; undermining of baskets at NE quadrant adjacent to wingwall with storm outlet;

Other Elements

331	Re Conc Bridge Railing	80	ft	80	0	0	0
				100%	0%	0%	0%

MDOT Type 4 Reinforced Concrete Bridge Railing at each side; no deficiencies noted

MISCELLANEOUS

Guard Rail

Item	Rating
36A. Bridge Railings	1
36B. Transitions	0
36C. Approach Guardrail	0
36D. Approach Guardrail Ends	0

Other Items

Item	Rating
71. Water Adequacy	7
72. Approach Alignment	8
Special Insp. Equipment	2
Underwater Insp. Method	0

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Brush Cut	M	Clear weeds away from barrier railing and wingwalls.
Slope Repair	H	Backfill, geotextile and armor slopes behind all wingwalls with rip rap.
Railing Repair	H	Place departing end guardrails to meet standards.
HMA Overlay	L	Remove existing overlay, reseal culvert joints apply geotextile membrane, add HMA overlay for full clear roadway width; apply concrete surface coating to bridge railings and culvert fascias
Other	H	Add advanced warning load posting sign for SB traffic at SW quadrant of Wick/Mortenview intersection-north of bridge; add advanced warning load posting sign for NB traffic at NE corner of Goddard/Mortenview intersection; move load posting signs near bridge to within 50 ft of SE and NW corners of bridge; Update sign placement and height configuration per MMUTCD.

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		CULVERT SAFETY INSPECTION REPORT		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)	Operational Status B Posting Recommended(B)	
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)		
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

SUPPORTING IMAGES UH0Y 12/03/2024



Document Name: RIMG3068.JPG
 Category: Elevation
 Span Number:
 Comments: east fascia



Document Name: RIMG3080.JPG
 Category: Elevation
 Span Number:
 Comments: west fascia



Document Name: RIMG3060.JPG
 Category: Posting
 Span Number:
 Comments: 42T/66T/74T load posting sign at north approach




Document Name: RIMG3049.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)	Operational Status B Posting Recommended(B)	
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)		Scour Evaluation 8 Stable Above Footing
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y		



Document Name: RIMG3050.JPG
 Category: Approach
 Span Number:
 Comments: south approach looking south



Document Name: RIMG3058.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south



Document Name: RIMG3059.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking north



Document Name: RIMG3052.JPG
 Category: Railing
 Span Number:
 Comments: West railing

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG3062.JPG
 Category: Railing
 Span Number:
 Comments: East railing



Document Name: RIMG3070.JPG
 Category: Culvert
 Span Number:
 Comments: 3E is displaced 1/4" down



Document Name: RIMG3071.JPG
 Category: Culvert
 Span Number:
 Comments: 3E is displaced 1/4" down
 1" gap between beams 3E and 4E



Document Name: RIMG3073.JPG
 Category: Culvert
 Span Number:
 Comments: 1" gap between beams 3E and 4E

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG3074.JPG
 Category: Culvert
 Span Number:
 Comments: 1" gap between beams 3E and 4E



Document Name: RIMG3075.JPG
 Category: Culvert
 Span Number:
 Comments: south view of culvert



Document Name: RIMG3076.JPG
 Category: Culvert
 Span Number:
 Comments: north view of culvert



Document Name: RIMG3078.JPG
 Category: Culvert
 Span Number:
 Comments: Leaking between 2W and 3W
 1" settlement between 3W and 4W

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

CULVERT SAFETY INSPECTION REPORT

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UHOY	Scour Evaluation 8 Stable Above Footing	



Document Name: RIMG3057.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking west



Document Name: RIMG3065.JPG
 Category: Channel
 Span Number:
 Comments: channel looking east



Document Name: RIMG3076.JPG
 Category:
 Span Number:
 Comments:

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514

STRUCTURE INVENTORY AND APPRAISAL

Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

Bridge History, Type, Materials

27 - Year Built	1999
106 - Year Reconstructed	
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	1 19
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	0
79 - Rail Type	8
80 - Post Type	
107 - Deck Type	N
108A - Wearing Surface	N
108B - Membrane	N
108C - Deck Protection	N

Structure Dimensions

34 - Skew	45
35 - Struct Flared	N
45 - Num Main Spans	1
46 - Num Apprs Spans	0
48 - Max Span Length	40
49 - Structure Length	40
50A - Width Left Curb/SW	0
50B - Width Right Curb/SW	0
33 - Median	0
51 - Width Curb to Curb	0
52 - Width Out to Out	32.8
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/03/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	0
58 - Deck Rating	N
58A/B - Deck Surface/Bottom	
59 - Superstructure Rating	N
59A - Paint Rating	
60 - Substructure Rating	N
61 - Channel Rating	6
62 - Culvert Rating	7

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	0
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	0
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016566 10
19 - Detour Length	4
20 - Toll Facility	3
26 - Functional Class	19
28A - Lanes On	2
29 - ADT	3935
30 - Year of ADT	2000
32 - Appr Roadway Width	29.9
32A/B - Ap Pvt Type/Width	0 29.99
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	39.4
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	2
109 - Truck %	15
110 - Truck Network	0
114 - Future ADT	5847
115 - Year Future ADT	2018
Freeway	0

Structure Appraisal

36A - Bridge Railing	1
36B - Rail Transition	0
36C - Approach Rail	0
36D - Rail Termination	0
67 - Structure Evaluation	6
68 - Deck Geometry	N
69 - Underclearance	N
71 - Waterway Adequacy	7
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	8

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	99.9
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	



Proposed Improvements

75 - Type of Work	
76 - Length of Improvement	
94 - Bridge Cost	
95 - Roadway Cost	
96 - Total Cost	
97 - Year of Cost Estimate	

Load Rating and Posting



31 - Design Load	5
41 - Open, Posted, Closed	B
63 - Fed Oper Rtg Method	6
64F - Fed Oper Rtg Load	1.25
64MA - Mich Oper Rtg Method	6
64MB - Mich Oper Rtg	.96
64MC - Mich Oper Truck	17
65 - Inv Rtg Method	6
66 - Inventory Load	.76
70 - Posting	4
141 - Posted Loading	426782
193 - Overload Class	D N

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		WORK RECOMMENDATIONS		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	


WORK RECOMMENDATIONS **UH0Y**

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12514		REQUEST FOR ACTION		
Facility MORTENVIEW DRIVE	Latitude / Longitude 42.2284 / -83.2402	MDOT Structure ID 824664300025B01	Structure Condition Good Condition(7)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 40 / 32.8 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1999 / / /	TSC Taylor(25)	Operational Status B Posting Recommended(B)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / UH0Y	Scour Evaluation 8 Stable Above Footing	

No inspections available for bridge key 824664300025B01

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		BRIDGE SAFETY INSPECTION REPORT		
Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Brn/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	

NBI INSPECTION **G118**

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024


GENERAL NOTES

Assisted by Adam Budzinski and Blake Cain Residential street.

DECK

	12/20	12/22	12/24	
1. Surface (SIA-58A)	7	7	7	Tined concrete surface. 3 full length longitudinal cracks and rusting bleeding throughout deck surface. (12/24) Tined concrete surface with isolated longitudinal cracks and rusting bleeding throughout deck surface. (12/22) Tined concrete surface with isolated longitudinal cracks and rusting bleeding throughout deck surface. (12/20)
2. Expansion Joints		6	6	North & south seals cracking full length of sealant. Some loss of material near gutter line and at midspan. (12/24) North & south seals cracking full length of sealant. Some loss of material near gutter line. (12/22) (12/20)
3. Other Joints	6	N	N	N/A; Moved to expansion-end joints rating (12/24) N/A; Moved to expansion-end joints rating (12/22) North & south seals cracking full length of sealant. Some loss of material near gutter line. (12/20)
4. Railings	7	7	7	Intermittent vertical cracks along both railings, due to temp and shrinkage and very fine map cracking. Minor alligator cracking both concrete rails. No spalling. Minor cracking occurring at NW railing block where tubes fasten to concrete (12/24) Intermittent vertical cracks along both railings, due to temp and shrinkage and very fine map cracking. Minor alligator cracking both concrete rails. No spalling. Minor cracking occurring at NW railing block where tubes fasten to concrete (12/22) Intermittent vertical cracks along both railings, due to temp and shrinkage and very fine map cracking. Minor alligator cracking both concrete rails. No spalling. Minor cracking occurring at NW railing block where tubes fasten to concrete (12/20)
5. Sidewalks or Curbs	7	7	7	Minor wear along east sidewalk; trip hazards present due to settled approach sidewalk at each corner (12/24) Minor wear along east sidewalk; trip hazards present due to settled approach sidewalk at each corner (12/22) Minor wear along east side. (12/20)
6. Deck Bottom Surface (SIA-58B)	7	N	N	Deck bottom surface cannot be observed; Side by side prestressed box beams with grouted joints in place; No noted grout loss at joints. (12/24) Deck bottom surface cannot be observed; Side by side prestressed box beams with grouted joints in place; No noted grout loss at joints. (12/22) No Noted Deficiencies; box beams with grouted joints. No noted grout loss at joints. (12/20)
7. Deck (SIA-58)	7	7	7	Tined concrete surface. 3 full length longitudinal cracks and rusting bleeding throughout deck surface. Fascia no noted deficiencies, Bottom surface not visible-SBS box beams. (12/24) Surface-small tight cracks, minor rusting is leaching. Fascia no noted deficiencies, Bottom surface not visible-SBS box beams. (12/22) Surface-small tight cracks, minor rusting is leaching. Fascia no noted deficiencies, Bottom-SBS box beams. (12/20)

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		BRIDGE SAFETY INSPECTION REPORT		
Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	

8. Drainage Runoff to curb and gutter with Bridge at Crest Curve. (12/24)
Runoff to curb and gutter with Bridge at Crest Curve. (12/22)
Runoff to curb and gutter with Bridge at Crest Curve. With rain at time, some ponding showing at bridge/approach interface. No spread into lanes. (12/20)


SUPERSTRUCTURE

	12/20	12/22	12/24	
9. Stringer (SIA-59)	8	8	8	6 - 12"x73.5" adjacent prestressed concrete box beams. No noted deficiencies. (12/24) 6 - 12"x73.5" adjacent prestressed concrete box beams. No noted deficiencies. (12/22) Side-by-side box beams. No noted problems (12/20)
10. Paint (SIA-59A)	N	N	N	(12/24) (12/22) (12/20)
11. Section Loss	N	3	3	No deterioration or leakage through joints along box beam pavement seats at each end (12/24) No deterioration or leakage through joints along box beam pavement seats at each end (12/22) (12/20)
12. Bearings	7	7	7	Elastomeric pads, some movement of joint filler between bearings. Fascia beam exterior edges and bottom of deck past fascias not allowed to slide. Hairline cracks forming in concrete - monitor. (12/24) Elastomeric pads, some movement of joint filler between bearings. Fascia beam exterior edges and bottom of deck past fascias not allowed to slide. Hairline cracks forming in concrete - monitor. (12/22) Elastomeric pads, some movement of joint filler between bearings. Fascia beam exterior edges and bottom of deck past fascias not allowed to slide. Hairline cracks forming in concrete - monitor. (12/20)

SUBSTRUCTURE

	12/20	12/22	12/24	
13. Abutments (SIA-60)	7	7	6	Reinf Conc Solid Wall abutments with reconstructed beam seat; Vertical cracks at interface of wingwall and abutment. Vertical cracking at all four quadrants. SW quad has spall/crack (12/24) Reinf Conc Solid Wall abutments with reconstructed beam seat; Minor vertical cracks at interface of wingwall and abutment. Vertical cracking at all four quadrants. (12/22) Top of abutments were reconstructed for new beam seats with patching also included. Minor vertical cracks at interface of wingwall and abutment. Vertical cracking at all four quadrants. (12/20)
14. Piers (SIA-60)	N	N	N	N/A (12/24) N/A (12/22) (12/20)
15. Slope Protection	7	N	N	N/A; Bridge over waterway (12/24) N/A; Bridge over waterway (12/22) Riprap on slopes against wingwalls, along north bank. Some minor movement of riprap. South side riprap is more sporadic. (12/20)

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		BRIDGE SAFETY INSPECTION REPORT		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)	
Feature	Length / Width / Spans	Owner		
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures	

16. Channel (SIA-61)	8	7	7	Channel flows West to East; Riprap channel protection in place from Abutment to Abutment; Stream flow runs under south half of bridge against South abutment; Some minor movement of riprap. Bank erosion in northeast beyond riprap. (12/24) Channel flows West to East; Riprap channel protection in place from Abutment to Abutment; Stream flow runs under south half of bridge against South abutment; Some minor movement of riprap. Bank erosion in northeast beyond riprap. (12/22) Riprap added to channel, along south abutment and up slope to face of north abutment. Some minor movement of riprap. Stream flow is towards south half of streambed up to south abutment Bank erosion in northeast quadrant past riprap Should relocate some riprap to SW quadrant. (12/20)
17. Scour Inspection	7	7	7	No noted scour. Riprap is stable. (12/24) No noted scour. Riprap is stable. (12/22) No noted scour. Rip rap is stable. (12/20)

APPROACH

	12/20	12/22	12/24	
18. Approach Pavement	7	7	7	Concrete pavement with no significant problems. Some minor cracking. Previous concrete patching repairs done. (12/24) Concrete pavement with no significant problems. Some minor cracking. Previous concrete patching repairs done. (12/22) Concrete pavement with no significant problems. Some minor cracking. Previous concrete patching repairs done. (12/20)
19. Approach Shoulders Sidewalks	7	6	4	Concrete sidewalks at each quadrant. Settlement with potential trip hazard at each corner at first approach sidewalk section against bridge sidewalk. Settlement is 1 1/4" SW quad; 3/4" at NW quad; 3/4" at SE quad; 1" at NE quad (12/24) Concrete sidewalks at each quadrant. Settlement with potential trip hazard at each corner at first approach sidewalk section against bridge sidewalk. Settlement is 1 1/4" SW quad; 3/4" at NW quad; 3/4" at SE quad; 1" at NE quad (12/22) Concrete sidewalks at each quadrant. Settlement of 1/2" at SE, NE and NW quads. 3/4" at interface of SW sidewalk with bridge walk. Possible trip hazard. (12/20)
20. Approach Slopes				Flat lawns. (12/24) Flat lawns. (12/22) Flat lawns. (12/20)
21. Utilities				Buried gas line along west side of bridge. (12/24) Buried gas line along west side of bridge. (12/22) Buried gas line along west side of bridge. (12/20)
22. Drainage Culverts				(12/24) (12/22) (12/20)

MISCELLANEOUS

Guard Rail		Other Items	
Item	Rating	Item	Rating
36A. Bridge Railings	1	71. Water Adequacy	8
36B. Transitions	1	72. Approach Alignment	8
36C. Approach Guardrail	1	Temporary Support	0 No Temporary Supports
36D. Approach Guardrail Ends	1	High Load Hit (M)	No
		Special Insp. Equipment	2
		Underwater Insp. Method	1

False Decking (Timber) Removed to Complete Inspection N/A - No False Decking
 Modified by: SPENCEA7890 on 12/04/2024 Printed on 12/05/2024 Page 3 of 10

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	

Critical Feature Inspections (SIA-92)

	<u>Freq</u>	<u>Date</u>
92A. Fracture Critical		
92B. Underwater		
92C. Other Special		
92D. Fatigue Sensitive		

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)	
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Operational Status A Open, no restriction(A)
			Scour Evaluation 7 Countermeasures

SUPPORTING IMAGES

G118 12/03/2024



Document Name: RIMG3031.JPG
 Category: Elevation
 Span Number:
 Comments: east fascia



Document Name: RIMG3041.JPG
 Category: Elevation
 Span Number:
 Comments: west fascia



Document Name: RIMG3005.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north



Document Name: RIMG3006.JPG
 Category: Approach
 Span Number:
 Comments: south approach looking south

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3016.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south



Document Name: RIMG3017.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking north



Document Name: RIMG3046.JPG
 Category: Approach
 Span Number:
 Comments: typical view of trip hazard at sidewalk approaches



Document Name: RIMG3011.JPG
 Category: Railing
 Span Number:
 Comments: West railing

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3023.JPG
 Category: Railing
 Span Number:
 Comments: East railing



Document Name: RIMG3022.JPG
 Category: Deck
 Span Number:
 Comments: Typical view of deck



Document Name: RIMG3010.JPG
 Category: Joints
 Span Number:
 Comments: West joint



Document Name: RIMG3020.JPG
 Category: Joints
 Span Number:
 Comments: East joint

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3021.JPG
 Category: Joints
 Span Number:
 Comments: Missing sealant at mid-span in joint



Document Name: RIMG3035.JPG
 Category: Superstructure
 Span Number:
 Comments: Box beams/deck bottom



Document Name: RIMG3033.JPG
 Category: Substructure
 Span Number:
 Comments: South abutment



Document Name: RIMG3037.JPG
 Category: Substructure
 Span Number:
 Comments: North abutment

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3042.JPG
 Category: Substructure
 Span Number:
 Comments: Vertical cracks at interface of wingwall and abutment



Document Name: RIMG3044.JPG
 Category: Substructure
 Span Number:
 Comments: SW quad has spall/crack



Document Name: RIMG3012.JPG
 Category: Channel
 Span Number:
 Comments: view of channel looking west



Document Name: RIMG3038.JPG
 Category: Channel
 Span Number:
 Comments: view of channel looking east

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515 BRIDGE SAFETY INSPECTION REPORT

Facility CONTINENTAL DRIVE	Latitude / Longitude 42.2282 / -83.2926	MDOT Structure ID 824664300701B01	Structure Condition Fair Condition(6)	
Feature SEXTON-KILFOIL DRAIN	Length / Width / Spans 31.9 / 36.9 / 1	Owner City: TAYLOR(6643)		
Location IN TAYLOR	Built / Recon. / Paint / Ovly. 1970 / 2007 / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	Last NBI Inspection 12/03/2024 / G118	Scour Evaluation 7 Countermeasures	



Document Name: RIMG3009.JPG
 Category: Other
 Span Number:
 Comments: bridge plaque

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

STRUCTURE INVENTORY AND APPRAISAL

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)
Feature	Length / Width / Spans	Owner	
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovlv.	TSC	Operational Status
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures



Bridge History, Type, Materials

27 - Year Built	1970
106 - Year Reconstructed	2007
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	5 05
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	0
79 - Rail Type	7
80 - Post Type	
107 - Deck Type	1
108A - Wearing Surface	1
108B - Membrane	0
108C - Deck Protection	1

Structure Dimensions

34 - Skew	14
35 - Struct Flared	N
45 - Num Main Spans	1
46 - Num Apprs Spans	0
48 - Max Span Length	28.9
49 - Structure Length	31.9
50A - Width Left Curb/SW	5.2
50B - Width Right Curb/SW	4.9
33 - Median	0
51 - Width Curb to Curb	26.9
52 - Width Out to Out	36.9
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/03/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	1
58 - Deck Rating	7
58A/B - Deck Surface/Bottom	7 N
59 - Superstructure Rating	8
59A - Paint Rating	N
60 - Substructure Rating	6
61 - Channel Rating	7
62 - Culvert Rating	N

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	0
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	0
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016581 06
19 - Detour Length	2
20 - Toll Facility	3
26 - Functional Class	19
28A - Lanes On	2
29 - ADT	850
30 - Year of ADT	2006
32 - Appr Roadway Width	25.9
32A/B - Ap Pvt Type/Width	6 24.02
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	26.2
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	2
109 - Truck %	1
110 - Truck Network	0
114 - Future ADT	1100
115 - Year Future ADT	2026
Freeway	0

Structure Appraisal

36A - Bridge Railing	1
36B - Rail Transition	1
36C - Approach Rail	1
36D - Rail Termination	1
67 - Structure Evaluation	7
68 - Deck Geometry	5
69 - Underclearance	N
71 - Waterway Adequacy	8
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	7

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	99.9
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	


Proposed Improvements

75 - Type of Work	
76 - Length of Improvement	
94 - Bridge Cost	
95 - Roadway Cost	
96 - Total Cost	
97 - Year of Cost Estimate	

Load Rating and Posting

31 - Design Load	9
41 - Open, Posted, Closed	A
63 - Fed Oper Rtg Method	6
64F - Fed Oper Rtg Load	3.35
64MA - Mich Oper Rtg Method	6
64MB - Mich Oper Rtg	2.24
64MC - Mich Oper Truck	18
65 - Inv Rtg Method	6
66 - Inventory Load	2.01
70 - Posting	5
141 - Posted Loading	
193 - Overload Class	

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515		WORK RECOMMENDATIONS		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)	
Feature	Length / Width / Spans	Owner		
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
IN TAYLOR	1970 / 2007 / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	7 Countermeasures	

WORK RECOMMENDATIONS **G118**

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Approach Repair	H	Re-seal end joints and cracks beyond new approach pavement and repair approach sidewalks to remove trip hazards at each corner
Super Repair	M	Penetrating healer sealer on deck surface to seal cracks.
Substr Repair	M	Repair cracks at wingwalls and abutment interface. Remove brush at northeast corner

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12515

REQUEST FOR ACTION

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
CONTINENTAL DRIVE	42.2282 / -83.2926	824664300701B01	Fair Condition(6)
Feature	Length / Width / Spans	Owner	Operational Status
SEXTON-KILFOIL DRAIN	31.9 / 36.9 / 1	City: TAYLOR(6643)	A Open, no restriction(A)
Location	Built / Recon. / Paint / Ovly.	TSC	Scour Evaluation
IN TAYLOR	1970 / 2007 / /	Taylor(25)	7 Countermeasures
Region / County	Material / Design	Last NBI Inspection	
Metro(7) / Wayne(82)	5 Prestressed Concrete / 05 Box Bm/Gird- Multiple	12/03/2024 / G118	



No inspections available for bridge key 824664300701B01

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978		CULVERT SAFETY INSPECTION REPORT		
Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition	
BEECH DALY ROAD	42.1839 / -83.2868	824664300030B01	Good Condition(7)	
Feature	Length / Width / Spans	Owner		
BLAKELY DRAIN	41 /	City: TAYLOR(6643)		
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status	
CITY OF TAYLOR	2004 / / /	Taylor(25)	A Open, no restriction(A)	
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation	
Metro(7) / Wayne(82)	1 Concrete / 19 Culvert	12/03/2024 / 28HE	5 Stable w/in footing	

CULVERT INSPECTION 28HE

Inspector Name	Agency / Company Name	Insp. Freq.	Insp. Date
Amanda Spence	Wade Trim	24	12/03/2024

GENERAL NOTES

Assisted by Blake Cain
SW guardrail impact attenuator has impact damage.

NBI INSPECTION

	12/20	12/22	12/24	
1. Culvert Rating (SIA-62)	7	7	7	(12/24) (12/22) (12/20)
2. Channel (SIA-61)	7	7	5	Dam full width of channel under culvert, approx 9' from east end. Trees and brush covering channel banks. Riprap channel protection in place around each return wall. Debris both US and DS in waterway including large trees NE Quad - Heavy weeds/trees. (12/24) Trees and brush covering channel banks. Riprap channel protection in place around each return wall. Debris both US and DS in waterway including large trees NE Quad - Heavy weeds/trees. (12/22) Trees and brush covering channel banks. Debris both US and DS in waterway including large trees NE Quad - Heavy weeds/trees. (12/20)
3. Scour	8	8	8	Some rip rap movement but no scour found. Temp sheeting left-in-place holding up well to protect against scour. South culvert wall riprap is sporadic. (12/24) Some rip rap movement but no scour found. Temp sheeting left-in-place holding up well to protect against scour. South culvert wall riprap is sporadic. (12/22) Some rip rap movement but no scour found. Temp sheeting left-in-place holding up well to protect against scour. South culvert wall riprap is sporadic. (12/20)


AASHTO ELEMENTS (English Units)

Element Number	Element Name	Total Quantity	Unit	Good CS1	Fair CS2	Poor CS3	Severe CS4
Culvert							
851	Reinf Conc Culvert 3-Sided	72	ft	72 100%	0 0%	0 0%	0 0%
38 ft span x 7 ft rise culvert 3-sided hyspan culvert structure x 11 sections; Minor leakage evident in culvert joints with no noted movements; Reflective cracking along culvert joints evident on deck. Staining along joints and along vertical legs at each weep hole; Concrete pavement surface with no earth fill-previous patching along end joints ~8sf; South end joint missing 80% of joint length							
857	Culvert Joints	10		0 0%	10 100%	0 0%	0 0%
10 joints with minor leakage evident; no differential between sections							
861	Culvert Wingwall	4		4 100%	0 0%	0 0%	0 0%
Conc capped steel sheet pile wingwalls at each corner; no deterioration evident; heavy brush growing between guardrail and return wall at each corner							

Scour Countermeasure

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978 CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	

830	Plain Riprap	960	sq.ft	860	100	0	0
				90%	10%	0%	0%

Approx 6x80 x 2 sides; minor movement along each side; areas of channel aggradation with riprap buried

836	Sheet Piling	160	ft	160	0	0	0
				100%	0%	0%	0%

Existing steel sheet piling left in place from original bridge; Top 6 inches visible in few locations along each end of bridge

Other Elements

330	Metal Bridge Railing	60	ft	60	0	0	0
				100%	0%	0%	0%

Metal 2-tube railing attached to posts and end walls; Bridge Railing, Aesthetic Parapet Tube attached to concrete slab at each fascia; Galvanized Coating in good condition at each railing (Galv 2.2 sft per ft)

515	Steel Protective Coating	130	sq.ft	130	0	0	0
				100%	0%	0%	0%

331	Re Conc Bridge Railing	82	ft	78	0	4	0
				95%	0%	5%	0%

Bridge Railing, Aesthetic Parapet Tube attached to concrete slab at each fascia; Minor hairline cracks in bridge railing along each side; 4 ft long diagonal through crack at Northwest end wall-at metal railing anchorage point with concrete end wall

840	Reinf Conc Sidewalk	588	sq.ft	588	0	0	0
				100%	0%	0%	0%

7'-2" wide sidewalks at each side x 41'-0" length; debris and dirt build up along toe of railing at each sidewalk; minor scaling along curb each sidewalk.

MISCELLANEOUS

Guard Rail

Item	Rating
36A. Bridge Railings	1
36B. Transitions	1
36C. Approach Guardrail	1
36D. Approach Guardrail Ends	0

Other Items

Item	Rating
71. Water Adequacy	8
72. Approach Alignment	8
Special Insp. Equipment	2
Underwater Insp. Method	0

RECOMMENDATIONS & ACTION ITEMS

Recommendation Type	Priority	Description
Channel Repair	H	Remove dam from under culvert that is restricting flow
Approach Repair	M	Repair NW and SW guardrail end terminals from previous accidents.
Brush Cut	M	Cut weeds down at each side of bridge, and along lengths of guardrail all quadrants.
Slope Repair	M	Replace riprap along north bank where geotextile fabric is exposed.
Bridge Washing	M	Wash approach and bridge sidewalks of debris and gravel build up at each side of road.
Joint Repair	M	Fill end joints and patch concrete pavement at each end of bridge; Fill joints between culvert sections to prevent leakage and joint degradation; coordinate with healer sealer application
Super Repair	M	Penetrating healer sealer for deck-pavement surface

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978 CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	

SUPPORTING IMAGES

28HE 12/03/2024



Document Name: RIMG2986.JPG
 Category: Elevation
 Span Number:
 Comments: West fascia



Document Name: RIMG2999.JPG
 Category: Elevation
 Span Number:
 Comments: East Fascia



Document Name: RIMG2966.JPG
 Category: Approach
 Span Number:
 Comments: South approach looking north



Document Name: RIMG2967.JPG
 Category: Approach
 Span Number:
 Comments: south approach looking south

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978

CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2976.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking south



Document Name: RIMG2977.JPG
 Category: Approach
 Span Number:
 Comments: north approach looking north



Document Name: RIMG2970.JPG
 Category: Railing
 Span Number:
 Comments: West railing



Document Name: RIMG2979.JPG
 Category: Railing
 Span Number:
 Comments: East railing

MICHIGAN DEPARTMENT OF TRANSPORTATION

STR 12978 CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2981.JPG
 Category: Railing
 Span Number:
 Comments: 4 ft long diagonal through crack at Northwest end wall at metal railing anchorage point with concrete end wall



Document Name: RIMG3004.JPG
 Category: Railing
 Span Number:
 Comments: SW guardrail impact attenuator damage.



Document Name: RIMG2967.JPG
 Category: Deck
 Span Number:
 Comments: typical view of deck



Document Name: RIMG2971.JPG
 Category: Joints
 Span Number:
 Comments: West joint

MICHIGAN DEPARTMENT OF TRANSPORTATION

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CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	



Document Name: RIMG2980.JPG
 Category: Joints
 Span Number:
 Comments: East joint



Document Name: RIMG2992.JPG
 Category: Culvert
 Span Number:
 Comments: view of culvert, south



Document Name: RIMG2994.JPG
 Category: Culvert
 Span Number:
 Comments: view of culvert, north



Document Name: RIMG2982.JPG
 Category: Channel
 Span Number:
 Comments: channel looking east

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CULVERT SAFETY INSPECTION REPORT

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
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Document Name: RIMG2985.JPG
 Category: Channel
 Span Number:
 Comments: Channel looking west



Document Name: RIMG2995.JPG
 Category: Channel
 Span Number:
 Comments: View of dam under culvert



Document Name: RIMG2996.JPG
 Category: Channel
 Span Number:
 Comments: view of dam blocking channel



Document Name: RIMG2968.JPG
 Category: Other
 Span Number:
 Comments: bridge plaque

MICHIGAN DEPARTMENT OF TRANSPORTATION

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STRUCTURE INVENTORY AND APPRAISAL

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
BEECH DALY ROAD	42.1839 / -83.2868	824664300030B01	Good Condition(7)
Feature	Length / Width / Spans	Owner	
BLAKELY DRAIN	41 /	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status
CITY OF TAYLOR	2004 / / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	1 Concrete / 19 Culvert	12/03/2024 / 28HE	5 Stable w/in footing



Bridge History, Type, Materials

27 - Year Built	2004
106 - Year Reconstructed	
202 - Year Painted	
203 - Year Overlay	
43 - Main Span Bridge Type	1 19
44 - Appr Span Bridge Type	
77 - Steel Type	0
78 - Paint Type	0
79 - Rail Type	7
80 - Post Type	
107 - Deck Type	2
108A - Wearing Surface	1
108B - Membrane	0
108C - Deck Protection	1

Structure Dimensions

34 - Skew	0
35 - Struct Flared	0
45 - Num Main Spans	1
46 - Num Apprs Spans	0
48 - Max Span Length	38
49 - Structure Length	41
50A - Width Left Curb/SW	6
50B - Width Right Curb/SW	6
33 - Median	0
51 - Width Curb to Curb	0
52 - Width Out to Out	
112 - NBIS Length	Y

Inspection Data

90 - Inspection Date	12/03/2024
91 - Inspection Freq	24
92A - Frac Crit Req/Freq	N
93A - Frac Crit Insp Date	
92B - Und Water Req/Freq	N
93B - Und Water Insp Date	
92C - Oth Spec Insp Req/Freq	N
93C - Oth Spec Insp Date	
92D - Fatigue Req/Freq	N
93D - Fatigue Insp Date	
176A - Und Water Insp Method	0
58 - Deck Rating	N
58A/B - Deck Surface/Bottom	
59 - Superstructure Rating	N
59A - Paint Rating	
60 - Substructure Rating	N
61 - Channel Rating	5
62 - Culvert Rating	7

Navigation Data

38 - Navigation Control	0
39 - Vertical Clearance	0
40 - Horizontal Clearance	0
111 - Pier Protection	
116 - Lift Brdg Vert Clear	

Route Carried By Structure(ON Record)

5A - Record Type	1
5B - Route Signing	5
5C - Level of Service	0
5D - Route Number	00000
5E - Direction Suffix	0
10L - Best 3m Unclr-Lt	0 0
10R - Best 3m Unclr-Rt	99 99
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	0
13 - LRS Route-Subroute	0000016929 03
19 - Detour Length	2
20 - Toll Facility	3
26 - Functional Class	16
28A - Lanes On	4
29 - ADT	6100
30 - Year of ADT	2003
32 - Appr Roadway Width	60
32A/B - Ap Pvt Type/Width	6 60.01
42A - Service Type On	1
47L - Left Horizontal Clear	0.0
47R - Right Horizontal Clear	62.0
53 - Min Vert Clr Ov Deck	99 99
100 - STRAHNET	0
102 - Traffic Direct	2
109 - Truck %	0
110 - Truck Network	0
114 - Future ADT	8200
115 - Year Future ADT	2023
Freeway	0

Structure Appraisal

36A - Bridge Railing	1
36B - Rail Transition	1
36C - Approach Rail	1
36D - Rail Termination	0
67 - Structure Evaluation	7
68 - Deck Geometry	N
69 - Underclearance	N
71 - Waterway Adequacy	8
72 - Approach Alignment	8
103 - Temporary Structure	
113 - Scour Criticality	5

Miscellaneous

37 - Historical Significance	5
98A - Border Bridge State	
98B - Border Bridge %	
101 - Parallel Structure	N
EPA ID	
Stay in Place Forms	
143 - Pin & Hanger Code	1
148 - No. of Pin & Hangers	

Route Under Structure (UNDER Record)

5A - Record Type	
5B - Route Signing	
5C - Level of Service	
5D - Route Number	
5E - Direction Suffix	
10L - Best 3m Unclr-Lt	
10R - Best 3m Unclr-Rt	
PR Number	
Control Section	
11 - Mile Point	
12 - Base Highway Network	
13 - LRS Route-Subroute	
19 - Detour Length	
20 - Toll Facility	
26 - Functional Class	
28B - Lanes Under	
29 - ADT	
30 - Year of ADT	
42B - Service Type Under	5
47L - Left Horizontal Clear	
47R - Right Horizontal Clear	
54A - Left Feature	
54B - Left Underclearance	99 99
54C - Right Feature	
54D - Right Clearance	99 99
Under Clearance Year	
55A - Reference Feature	N
55B - Right Horiz Clearance	0
56 - Left Horiz Clearance	0
100 - STRAHNET	
102 - Traffic Direct	
109 - Truck %	
110 - Truck Network	
114 - Future ADT	
115 - Year Future ADT	
Freeway	

Proposed Improvements

75 - Type of Work	
76 - Length of Improvement	
94 - Bridge Cost	
95 - Roadway Cost	
96 - Total Cost	
97 - Year of Cost Estimate	

Load Rating and Posting

31 - Design Load	9
41 - Open, Posted, Closed	A
63 - Fed Oper Rtg Method	1
64F - Fed Oper Rtg Load	66.8
64MA - Mich Oper Rtg Method	
64MB - Mich Oper Rtg	109
64MC - Mich Oper Truck	
65 - Inv Rtg Method	1
66 - Inventory Load	40.1
70 - Posting	5
141 - Posted Loading	
193 - Overload Class	

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WORK RECOMMENDATIONS

Facility BEECH DALY ROAD	Latitude / Longitude 42.1839 / -83.2868	MDOT Structure ID 824664300030B01	Structure Condition Good Condition(7)	
Feature BLAKELY DRAIN	Length / Width / Spans 41 /	Owner City: TAYLOR(6643)		
Location CITY OF TAYLOR	Built / Recon. / Paint / Ovly. 2004 / / /	TSC Taylor(25)	Operational Status A Open, no restriction(A)	
Region / County Metro(7) / Wayne(82)	Material / Design 1 Concrete / 19 Culvert	Last NBI Inspection 12/03/2024 / 28HE	Scour Evaluation 5 Stable w/in footing	

WORK RECOMMENDATIONS

28HE

MICHIGAN DEPARTMENT OF TRANSPORTATION

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REQUEST FOR ACTION

Facility	Latitude / Longitude	MDOT Structure ID	Structure Condition
BEECH DALY ROAD	42.1839 / -83.2868	824664300030B01	Good Condition(7)
Feature	Length / Width / Spans	Owner	
BLAKELY DRAIN	41 /	City: TAYLOR(6643)	
Location	Built / Recon. / Paint / Ovly.	TSC	Operational Status
CITY OF TAYLOR	2004 / / /	Taylor(25)	A Open, no restriction(A)
Region / County	Material / Design	Last NBI Inspection	Scour Evaluation
Metro(7) / Wayne(82)	1 Concrete / 19 Culvert	12/03/2024 / 28HE	5 Stable w/in footing



No inspections available for bridge key 824664300030B01



**Appendix C.
Glossary & Acronyms**

Glossary

Alligator Cracking: Cracking of the surface layer of an asphalt pavement that creates a pattern of interconnected cracks resembling alligator hide. This is often due to overloading a pavement, sub-base failure, or poor drainage.⁵

Asset Management: A process that uses data to manage and track road assets in a cost-effective manner using a combination of engineering and business principles. Public Act 325 of 2018 provides a legal definition: “an ongoing process of maintaining, preserving, upgrading, and operating physical assets cost effectively, based on a continuous physical inventory and condition assessment and investment to achieve established performance goals”.⁶

Biennial Inspection: Inspection of an agency's bridges every other year, which happens in accordance with National Bridge Inspection Standards and Michigan Department of Transportation requirements.

Bridge Inspection Program: A program implemented by a local agency to inspect the bridges within its jurisdiction systematically in order to ensure proper functioning and structural soundness.

Capital Preventative Maintenance: Also known as CPM, a planned set of cost-effective treatments to address of fair-rated infrastructure before the structural integrity of the system has been severely impacted. These treatments aim to slow deterioration and to maintain or improve the functional condition of the system without significantly increasing the structural capacity. Light capital preventive maintenance is a set of treatments designed to seal isolated areas of the pavement from water, such as crack and joint sealing, to protect and restore pavement surface from oxidation with limited surface thickness material, such as fog seal; generally, application of a light CPM treatment does not provide a corresponding increase in a segment's PASER score. Heavy capital preventive maintenance is a set of surface treatments designed to protect pavement from water intrusion or environmental weathering without adding significant structural strength, such as slurry seal, chip seal, or thin (less than 1.5-inch) overlays for bituminous surfaces or patching or partial-depth (less than 1/3 of pavement depth) repair for concrete surfaces.

Chip Seal: An asphalt pavement treatment method consisting of, first, spraying liquid asphalt onto the old pavement surface and, then, a single layer of small stone chips spread onto the wet asphalt layer.

City Major: A road classification, defined in Michigan Public Act 51, that encompasses the generally more important roads in a city or village. City major roads are designated by a municipality's governing body and are subject to approval by the State Transportation Commission. These roads do not include roads under the jurisdiction of a county road commission or trunkline highways.

City Minor: A road classification, defined in Michigan Public Act 51, that encompasses the generally less important roads in a city or village. These roads include all city or village roads that are not city major road and do not include roads under the jurisdiction of a county road commission.

Composite Pavement: A pavement consisting of concrete and asphalt layers. Typically, composite pavements are old concrete pavements that were overlaid with HMA in order to gain more service life.

⁵ https://en.wikipedia.org/wiki/Crocodile_cracking

⁶ Inventory-based Rating System for Gravel Roads: Training Manual

Concrete Joint Resealing: Resealing the joints of a concrete pavement with a flexible sealant to prevent moisture and debris from entering the joints. When debris becomes lodged inside a joint, it inhibits proper movement of the pavement and leads to joint deterioration and spalling.

Concrete Pavement: Also known as rigid pavement, a pavement made from Portland Cement concrete. Concrete pavement has an average service life of 30 years and typically does not require as much periodic maintenance as HMA.

Cost per Lane Mile: Associated cost of construction, measured on a per lane, per mile basis. Also see *lane-mile segment*.

County Local: A road classification, defined in Michigan Public Act 51, that encompasses the generally less important and low-traffic roads in a county. This includes all county roads that are not classified as county primary roads.

County Primary: A road classification, defined in Michigan Public Act 51, that encompasses the generally more important and high-traffic roads in a county. County primary roads are designated by board members of the county road commissions and are subject to approval by the State Transportation Commission.

CPM: See *Capital preventive maintenance*.

Crack and Seat: A concrete pavement treatment method that involves breaking old concrete pavement into small chunks and leaving the broken pavement in place to provide a base for a new surface. This provides a new wear surface that resists water infiltration and helps prevent damaged concrete from reflecting up to the new surface.

Crack Seal: A pavement treatment method for both asphalt and concrete pavements that fills cracks with asphalt materials, which seals out water and debris and slows down the deterioration of the pavement. Crack seal may encompass the term "crack filling".

Crush and Shape: An asphalt pavement treatment method that involves pulverizing the existing asphalt pavement and base and then reshaping the road surface to correct imperfections in the road's profile. Often, a layer of gravel is added along with a new wearing surface such as an HMA overlay or chip seal

Crust: A very tightly compacted surface on an unpaved road that sheds water with ease but takes time to be created.

Culvert: A pipe or structure used under a roadway that allows cross-road drainage while allowing traffic to pass without being impeded; culverts span up to 20 feet.⁷

Dowel Bar Retrofit Repair: A concrete pavement treatment method that involves cutting slots in a cracked concrete slab, inserting steel bars into the slots, and placing concrete to cover the new bars and fill the slots. It aims to reinforce cracks in a concrete pavement.

Dust Control: A gravel road surface treatment method that involves spraying chloride or other chemicals on the gravel surface to reduce dust loss, aggregate loss, and maintenance. This is a relatively short-term fix that helps create a crusted surface.

⁷ Adapted from Inventory-based Rating System for Gravel Roads: Training Manual

Expansion Joint: Joints in a bridge that allow for slight expansion and contraction changes in response to temperature. Expansion joints prevent the build up of excessive pressure, which can cause structural damage to the bridge.

Federal Highway Administration: Also known as FHWA, this is an agency within the U.S. Department of Transportation that supports state and local governments in the design, construction, and maintenance of the nation's highway system.⁸

Federal-Aid Network: Portion of road network that is comprised of federal-aid routes. According to Title 23 of the United States Code, federal-aid-eligible roads are "highways on the federal-aid highways systems and all other public roads not classified as local roads or rural minor collectors".⁹ Roads that are part of the federal-aid network are eligible for federal gas-tax monies.

FHWA: See *Federal Highway Administration*.

Flexible Pavement: See *hot-mix asphalt pavement*.

Fog seal: An asphalt pavement treatment method that involves spraying a liquid asphalt coating onto the entire pavement surface to fill hairline cracks and prevent damage from sunlight and oxidation. This method works best for good to very good pavements.

Full-Depth Concrete Repair: A concrete pavement treatment method that involves removing sections of damaged concrete pavement and replacing it with new concrete of the same dimensions in order to restore the riding surface, delay water infiltration, restore load transfer from one slab to the next, and eliminate the need to perform costly temporary patching.

Geographic Divides: Areas where a geographic feature (e.g., river, lake, mountain) limits crossing points of the feature.

Grants: Competitive funding gained through an application process and targeted at a specific project type to accomplish a specific purpose. Grants can be provided both on the federal and state level and often make up part of the funds that a transportation agency receives.

Gravel Surfacing: A low-cost, easy-to-maintain road surface made from aggregate and fines.

Heavy Capital Preventive Maintenance: See *Capital preventive maintenance*.

HMA: See *hot-mix asphalt pavement*.

Hot-Mix Asphalt Overlay: Also known as HMA overlay, this a surface treatment that involves layering new asphalt over an existing pavement, either asphalt or concrete. It creates a new wearing surface for traffic and to seal the pavement from water, debris, and sunlight damage, and it often adds significant structural strength.

Hot-Mix Asphalt Pavement: Also known as HMA pavement, this type of asphalt creates a flexible pavement composed of aggregates, asphalt binder, and air voids. HMA is heated for placement and compaction at high temperatures. HMA is less expensive to construct than concrete pavement,

⁸ Federal Highway Administration webpage <https://www.fhwa.dot.gov/>

⁹ Inventory-based Rating System for Gravel Roads: Training Manual

however it requires frequent maintenance activities and generally lasts 18 years before major rehabilitation is necessary. HMA makes up the vast majority of local-agency-owned pavements.

IBR: See *IBR element, IBR number, and/or Inventory-based Rating System™*.

IBR Element: A feature used in the IBR System™ for assessing the condition of roads. The system relies on assessing three elements: surface width, drainage adequacy, and structural adequacy.¹⁰

IBR Number: The 1-10 rating determined from assessments of the weighted IBR elements. The weighting relates each element to the intensity road work needed to improve or enhance the IBR element category.¹¹

Interstate Highway System: The road system owned and operated by each state consisting of routes that cross between states, make travel easier and faster. The interstate roads are denoted by the prefix "I" or "U.S." and then a number, where odd routes run north-south and even routes run east-west. Examples are I-75 or U.S. 2.¹²

Inventory-Based Rating System™: Also known as the IBR System™, a rating system designed to assess the capabilities of gravel and unpaved roads to support intended traffic volumes and types year round. It assesses roads based on how three IBR elements, or features—surface width, drainage adequacy, and structural adequacy—compare to a baseline, or "good", road.¹³

Investment Reporting Tool: Also known as IRT, a web-based system used to manage the process for submitting required items to the Michigan Transportation Asset Management Council. Required items include planned and completed maintenance and construction activity for roads and bridges and comprehensive asset management plans.

IRT: See *Investment Reporting Tool*.

Jurisdiction: Administrative power of an entity to make decisions for something. In Michigan, the three levels of jurisdiction classification for transportation assets are state highways, county roads, and city and village streets. State highways are under the jurisdiction of the Michigan Department of Transportation, county roads are under the jurisdiction of the road commission for the county in which the roads are located, and city and village streets are under the jurisdiction of the municipality in which the roads are located.

Jurisdictional Borders: Borders between two road-owning-agency jurisdictions, or where the roads owned by one agency turn into roads owned by another agency. Examples of jurisdictional borders are township or county lines.

Lane-Mile Segment: A segment of road that is measured by multiplying the centerline miles of a roadway by the number of lanes present.

Lane-Mile-Years: A network's total lane-miles multiplied by one year; a method to quantify the measurable loss of pavement life.

¹⁰ Inventory-based Rating System for Gravel Roads: Training Manual

¹¹ Inventory-based Rating System for Gravel Roads: Training Manual

¹² <https://www.fhwa.dot.gov/interstate/faq.cfm#question3>

¹³ Adapted from Inventory-based Rating System for Gravel Roads: Training Manual

Light Capital Preventive Maintenance: See *Capital preventive maintenance*.

Limited Access Areas: Areas—typically remote areas—served by few or seasonal roads that require long detours routes if servicing roads are closed.

Main Access to Key Commercial Districts: Areas where large number or large size business will be significantly impacted if a road is unavailable.

Maintenance Grading: A surface treatment method for unpaved roads that involves re-grading the road to remove isolated potholes, washboarding, and ruts, and then restoring the compacted crust layer.

MDOT: See *Michigan Department of Transportation*.

MDOT's Local Bridge Program Call for Projects: A call for project proposals for replacement, rehabilitation, and/or preventive maintenance of local bridges that, if granted, receives bridge funding from the Michigan Department of Transportation. The Call for Projects is made by the Local Bridge Program.

MGF: See *Michigan Geographic Framework*.

Michigan Department of Transportation: Also known as MDOT, this is the state of Michigan's department of transportation, which oversees roads and bridges owned by the state or federal government in Michigan.

Michigan Geographic Framework: Also known as MGF, this is the state of Michigan's official digital base map that contains location and road information necessary to conduct state business. The Michigan Department of Transportation uses the MGF to link transportation assets to a physical location.

Michigan Public Act 51 of 1951: Also known as PA 51, this is a Michigan legislative act that served as the foundation for establishing a road funding structure by creating transportation funding distribution methods and means. It has been amended many times.¹⁴

Michigan Public Act 325 of 2018: Also known as PA 325, this legislation modified PA 51 of 1951 in regards to asset management in Michigan, specifically 1) re-designating the TAMC under Michigan Infrastructure Council (MIC); 2) promoting and overseeing the implementation of recommendations from the regional infrastructure asset management pilot program; 3) requiring local road three-year asset management plans beginning October 1, 2020; 4) adding asset classes that impact system performance, safety or risk management, including culverts and signals; 5) allowing MDOT to withhold funds if no asset management plan submitted; and 6) prohibiting shifting funds from a county primary to a county local, or from a city major to a city minor if no progress toward achieving the condition goals described in its asset plan.¹⁵

Michigan Public Act 499 of 2002: Also known as PA 499, this legislation requires road projects for the upcoming three years to be reported to the TAMC.

Michigan Transportation Asset Management Council: Also known as the TAMC, a council comprised of professionals from county road commissions, cities, a county commissioner, a township official,

¹⁴ Inventory-based Rating System for Gravel Roads: Training Manual

¹⁵ Inventory-based Rating System for Gravel Roads: Training Manual

regional and metropolitan planning organizations, and state transportation department personnel. The council reports directly to the Michigan Infrastructure Council.¹⁶ The TAMC provides resources and support to Michigan's road-owning agencies, and serves as a liaison in data collection requirements between agencies and the state.

Michigan Transportation Fund: Also known as MTF, this is a source of transportation funding supported by vehicle registration fees and the state's per-gallon gas tax.

Microsurface Treatment: An asphalt pavement treatment method that involves applying modified liquid asphalt, small stones, water, and Portland cement for the purpose of protecting a pavement from damage caused by water and sunlight.

Mill and Hot-Mix Asphalt Overlay: Also known as a mill and HMA overlay, this is a surface treatment that involves the removal of the top layer of pavement by milling and the replacement of the removed layer with a new HMA layer.

Mix-of-Fixes: A strategy of maintaining roads and bridges that includes generally prioritizes the spending of money on routine maintenance and capital preventive maintenance treatments to impede deterioration and then, as money is available, performing reconstruction and rehabilitation.

MTF: See *Michigan Transportation Fund*.

National Bridge Inspection Standards: Also known as NBIS, standards created by the Federal Highway Administration to locate and evaluate existing bridge deficiencies in the federal-aid highway system to ensure the safety of the traveling public. The standards define the proper safety for inspection and evaluation of all highway bridges.¹⁷

National Center for Pavement Preservation: Also known as the NCPP, a center that offers education, research, and outreach in current and innovative pavement preservation practices. This collaborative effort of government, industry, and academia entities was established at Michigan State University.

National Functional Class: Also known as NFC, a federal grouping system for public roads that classifies roads according to the type of service that the road is intended to provide.

National Highway System: Also known as NHS, this is a network of roads that includes the interstate highway system and other major roads managed by state and local agencies that serve major airports, marine, rail, pipelines, truck terminals, railway stations, military bases, and other strategic facilities.

NBIS: See *National Bridge Inspection Standards*.

NCPP: See *National Center for Pavement Preservation*.

NCPP Quick Check: A system created by the National Center for Pavement Preservation that works under the premise that a one-mile road segment loses one year of life each year that it is not treated with a maintenance, rehabilitation, or reconstruction project.

NFC: See *National Functional Class*.

¹⁶ Inventory-based Rating System for Gravel Roads: Training Manual

¹⁷ <https://www.fhwa.dot.gov/bridge/nbis/>

Non-Trunkline: A local road intended to be used over short distances but not recommended for long-distance travel.

Other Funds: Expenditures for equipment, capital outlay, debt principal payment, interest expense, contributions to adjacent governmental units, principal, interest and bank fees, and miscellaneous for cities and villages.

PA: See *Michigan Public Act 51*, *Michigan Public Act 325*, and/or *Michigan Public Act 499*.

Partial-Depth Concrete Repair: A concrete pavement treatment method that involves removing spalled or delaminated areas of concrete pavement, usually near joints and cracks, and replacing with new concrete. This is done to provide a new wearing surface in isolated areas, to slow down water infiltration, and to help delay further freeze-thaw damage.

PASER: See *Pavement Surface Evaluation and Rating system*.

Pavement Reconstruction: A complete removal of the old pavement and base and construction of an entirely new road. This is the most expensive rehabilitation of the roadway and also the most disruptive to traffic patterns.

Pavement Surface Evaluation and Rating system: Also known as the PASER system, the PASER system rates surface condition on a 1-10 scale, where 10 is a brand new road with no defects, 5 is a road with distress but that is structurally sound and requires only preventative maintenance, and 1 is a road with extensive surface and structural distresses that is in need of total reconstruction. This system provides a simple, efficient, and consistent method for evaluating the condition of paved roads.¹⁸

Pothole: A defect in a road that produces a localized depression.¹⁹

Preventive Maintenance: Planned treatments to an existing asset to prevent deterioration and maintain functional condition. This can be a more effective use of funds than the costly alternative of major rehabilitation or replacement.

Proactive Preventive Maintenance: Also known as PPM, a method of performing capital preventive maintenance treatments very early in a pavement's life, often before it exhibits signs of pavement defect.

Public Act 51: See *Michigan Public Act 51 of 1951*

Public Act 325: See *Michigan Public Act 325 of 2018*

Public Act 499: See *Michigan Public Act 499 of 2002*

Reconstruction and Rehabilitation Programs: Programs intended to reconstruct and rehabilitate a road.

Restricted Load Postings: A restriction enacted on a bridge structure when is incapable of transporting a state's legal vehicle loads.

¹⁸ Adapted from *Inventory-based Rating System for Gravel Roads: Training Manual*

¹⁹ *Inventory-based Rating System for Gravel Roads: Training Manual*

Rights-of-Way Ownership: The owning of the right-of-way, which is the land over which a road or bridge travels. In order to build a road, road agencies must own the right-of-way or get permission to build on it.

Rigid Pavement: See *concrete pavement*.

Road Infrastructure: An agency's road network and assets necessary to make it function, such as traffic signage and ditches.

Road: The area consisting of the roadway (i.e., the travelled way or the portion of the road on which vehicles are intended to drive), shoulders, ditches, and areas of the right of way containing signage.²⁰

Roadsoft: An asset management software suit that enables agencies to manage road and bridge related infrastructure. The software provides tools for collecting, storing, and analyzing data associated with transportation infrastructure. Built on an optimum combination of database engine and GIS mapping tools, Roadsoft provides a quick, smooth user experience and almost unlimited data handling capabilities.²¹

Ruts/Rutting: Deformation of a road that usually forms as a permanent depression concentrated under the wheel path parallel to the direction of travel.²²

Scheduled Maintenance: Low-cost, day-to-day activities applied to bridges on a scheduled basis that mitigates deterioration.²³

Sealcoat Pavement: A gravel road that has been sealed with a thin asphalt binder coating that has stone chips spread on top.

Service Life: Time from when a road or treatment is first constructed to when it reaches a point where the distresses present change from age-related to structural-related (also known as the critical distress point).²⁴

Slurry Seal: An asphalt pavement treatment method that involves applying liquid asphalt, small stones, water, and Portland Cement in a very thin layer with the purpose of protecting an existing pavement from being damaged by water and sunlight.

Structural Improvement: Pavement treatment that adds strength to the pavement. Roads requiring structural improvement exhibit alligator cracking and rutting and are considered poor by the TAMC definitions for condition.

Subsurface Infrastructure: Infrastructure maintained by local agencies that reside underground, for example, drinking water distribution systems, wastewater collection systems, and storm sewer systems.

TAMC: See *Michigan Transportation Asset Management Council*.

²⁰ Inventory-based Rating System for Gravel Roads: Training Manual

²¹ Inventory-based Rating System for Gravel Roads: Training Manual

²² Paving Class Glossary

²³ Inventory-based Rating System for Gravel Roads: Training Manual

²⁴ Inventory-based Rating System for Gravel Roads: Training Manual

TAMC Pavement Condition Dashboard: Website for viewing graphs of pavement and bridge conditions, traffic and miles travelled, safety statistics, maintenance activities, and financial data for Michigan's cities and villages, counties, and regions, as well as the state of Michigan.

TAMC's Good/Fair/Poor Condition Classes: Classification of road conditions defined by the Michigan Transportation Asset Management Council based on bin ranges of PASER scores and similarities in defects and treatment options. Good roads have PASER scores of 8, 9, or 10, have very few defects, and require minimal maintenance. Fair roads have PASER scores of 5, 6, or 7, have good structural support but a deteriorating surface, and can be maintained with CPM treatments. Poor roads have PASER scores of 1, 2, 3, or 4, exhibit evidence that the underlying structure is failing, such as alligator cracking and rutting. These roads must be rehabilitated with treatments like heavy overlay, crush and shape, or total reconstruction.

Tax Millages: Local tax implemented to supplement an agency's budget, such as road funding.

Thin Hot-Mix Asphalt Overlay: Application of a thin layer of hot-mix asphalt on an existing road to re-seal the road and protect it from damage caused by water. This also improves the ride quality and provides a smoother, uniform appearance that improves visibility of pavement markings.²⁵

Transportation Infrastructure: All of the elements that work together to make the surface transportation system function including roads, bridges, culverts, traffic signals, and signage.

Trigger: When a PASER score gives insight to the preferred timeline of a project for applying the correct treatment at the correct time.

Trunkline Abbreviations: The prefixes *M*-, *I*-, and *US* indicate roads in Michigan that are part of the state trunkline system, the Interstate system, and the US Highway system. These roads consist of anything from 10-lane urban freeways to two-lane rural highways and even one non-motorized highway; they cover 9,668 centerline miles. Most of the roads are maintained by MDOT.

Trunkline Bridges: Bridge present on a trunkline road, which typically connects cities or other strategic places and is the recommended rout for long-distance travel.²⁶

Trunkline Maintenance Funds: Expenditures under a maintenance agreement with MDOT for maintenance activities performed on MDOT trunkline routes.

Trunkline: Major road that typically connects cities or other strategic places and is the recommended route for long-distance travel.²⁷

Washboarding: Ripples in the road surface that are perpendicular to the direction of travel.²⁸

Wedge/Patch Sealcoat Treatment: An asphalt pavement treatment method that involves correcting the damage frequently found at the edge of a pavement by installing a narrow, 2- to 6-foot-wide wedge along the entire outside edge of a lane and layering with HMA. This extends the life of an HMA pavement or chip seal overlay by adding strength to significantly settled areas of the pavement.

²⁵ [second sentence] <http://www.kentcountyroads.net/road-work/road-treatments/ultra-thin-overlay>

²⁶ https://en.wikipedia.org/wiki/Trunk_road

²⁷ https://en.wikipedia.org/wiki/Trunk_road

²⁸ Inventory-based Rating System for Gravel Roads: Training Manual

Worst-First Strategy: Asset management strategy that treats only the problems, often addressing the worst problems first, and ignoring preventive maintenance. This strategy is the opposite of the “mix of fixes” strategy. An example of a worst-first approach would be purchasing a new automobile, never changing the oil, and waiting till the engine fails to address any deterioration of the car.

List of Acronyms

- CPM: capital preventive maintenance
- FHWA: Federal Highway Administration
- HMA: hot-mix asphalt
- I: trunkline abbreviation for routes on the Interstate system
- IBR: Inventory-based Rating
- M: trunkline abbreviation for Michigan state highways
- MDOT: Michigan Department of Transportation
- MTF: Michigan Transportation Fund
- NBIS: National Bridge Inspection Standards
- NCPP: National Center for Pavement Preservation
- NHS: National Highway System
- PA 51: Michigan Public Act 51 of 1951
- PASER: Pavement Surface Evaluation and Rating
- R&R: reconstruction and rehabilitation programs
- TAMC: (Michigan) Transportation Asset Management Council
- US: trunkline abbreviation for routes on the US Highway system

MICHIGAN DEPARTMENT OF ENVIRONMENT, GREAT LAKES, AND ENERGY
Storm Water Fees

INVOICE

Issued under authority of Public Act 451 of 1994.
Federal ID # 38-6000134

City of Taylor
Attn: Accounts Payable, Chris Gibbs
23555 Goddard Road
Taylor, MI 48180

Invoice Number:	11387777
Invoice Date:	01/26/2026
Notice Date:	02/01/2026
Total Due:	\$5,000.00

Taylor MS4-Wayne
23555 Goddard Road
Taylor, MI 48180

Failure to submit payment by the date due will result in a penalty as prescribed by law.

Account No.:
MI0060078

Invoice Item	Description	Total Cost
Base Charge	Stormwater Annual Permit Fee MSW Annual-Population of more than 50,000 people but fewer than 75,001 2026 for MI0060078	\$5,000.00

PAY ONLINE: Visit <https://mienviro.michigan.gov>
Using your MiEnviro Portal account, select Financials from the side panel to make a payment.

If you have questions regarding Municipal Separate Storm Sewer System (MS4) fees please call (517) 525-9437, or email at stilesj1@michigan.gov. Additional information about MiEnviro can be found at <https://www.michigan.gov/egle/maps-data/mienviroportal>.

Subtotal:	\$5,000.00
Adjustments:	\$0.00
Late Penalty:	\$0.00
Total Invoice:	\$5,000.00
Payments:	\$0.00
Balance Due:	\$5,000.00
Payment Due	March 15, 2026

REMIT PAYMENT TO: **STATE OF MICHIGAN**
TO ENSURE PROPER CREDIT, SEND THIS PORTION WITH
PAYMENT TO:

**EGLE-SWPF
CASHIERS OFFICE
PO BOX 30657
LANSING, MI 48909-8157**

Reference No.:
MI0060078

INVOICE NUMBER
11387777
WRD-SWPF

(Please note or make any address corrections below.)

ACCOUNTS PAYABLE
Taylor MS4-Wayne
23555 Goddard Road
Taylor, MI 48180

Total Due: \$5,000.00





GRETCHEN WHITMER
GOVERNOR

STATE OF MICHIGAN
DEPARTMENT OF
ENVIRONMENT, GREAT LAKES, AND ENERGY
WATER RESOURCE DIVISION



PHILLIP D. ROOS
DIRECTOR

February 1, 2026

Dear Permittee:

SUBJECT: Storm Water Discharge Annual Permit Fee – Invoice

Section 3118 of the Natural Resources and Environmental Protection Act, 1994 PA 451, as amended (NREPA), requires that permittees with a Storm Water Discharge Permit pay an annual permit fee. Municipal entities with a regulated Municipal Separate Storm Sewer System (MS4) that held a Storm Water Discharge Permit on January 1, 2026, are required to pay the fee for the 2026 fiscal year. The annual fee shown on your invoice is based on the population served by the regulated MS4. Please note for MS4 permits that are reissued in 2023 and after, the annual fee will include the fee for any nested jurisdictions included in the permit. For example, a city with an annual fee of \$3,000 that is nesting a school district with an annual fee of \$500 will be assessed an annual fee of \$3,500.

The Department of Environment, Great Lakes, and Energy (EGLE) has three forms of payment. You may pay online with a credit card (American Express, Discover, Mastercard, or Visa) or e-check in your MiEnviro Portal (formerly MiWaters) account at MiEnviro.Michigan.gov. There is a two percent (2%) processing fee for credit card payments. You may also pay with a check or money order made payable to the State of Michigan and it should be mailed with the lower portion of the invoice no later than March 15, 2026, to:

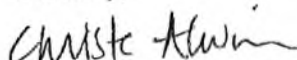
Michigan Department of Environment, Great Lakes, and Energy
Cashier's Office
P.O. Box 30657
Lansing, Michigan 48909-8157

In order to pay your invoice by credit card or e-check in MiEnviro Portal, you will need to have an account set up and be an authorized user for your site. If you need to be added as an authorized user, please contact the district compliance staff found under the "Contacts" section of the Industrial Stormwater Program website Michigan.gov/EGLE/About/Organization/Water-Resources/Municipal-Stormwater. You can find more information on MiEnviro Portal at Michigan.gov/EGLE/Maps-Data/MiEnviroPortal.

Please note that unpaid invoices are subject to late payment penalties as set forth in Section 3120(8) of the NREPA. Delinquent invoices will be subject to EGLE's and the State of Michigan's collection procedures.

For questions related to the status of your permit, how the fee amount was determined, or any other questions related to your permit, please access our National Pollutant Discharge Elimination System (NPDES) Fee web site page at Michigan.gov/EGLE/About/Organization/Water-Resources/NPDES/Fees or contact Jessica Stiles at StilesJ1@michigan.gov, or 517-525-9437. For questions on the fee payment process contact Karen Gutting at GuttingK1@michigan.gov, or 517-898-4136.

Sincerely,


Christe Alwin, Supervisor
Storm Water Permits Unit
Permits Section
Water Resources Division



CITY OF TAYLOR

COUNCIL

Timothy Woolley
MAYOR

Cynthia A. Bower
CLERK

Nicone Dragone Sr.
TREASURER



23555 Goddard Road
Taylor, MI 48180
(734) 287 6550
www.cityoftaylor.com

Charley Johnson
CHAIRMAN

Ron Thiede
CHAIR PRO-TEM

Christian Armstrong
Chris Clark
William Patts
Gerald P. Thomas
Dan Wallace

PURCHASING DEPARTMENT MEMORANDUM

TO: Honorable Mayor and Members of the City Council

FROM: Purchasing Department

SUBJECT: Recommendation to Update Charter Purchasing Thresholds from \$2,000 to \$5,000

DATE: February 23, 2026

Purpose

This memorandum recommends updating the City Charter’s purchasing and public-improvement thresholds—currently set at **\$2,000** to **\$5,000**. The proposed change aligns Taylor with modern municipal practice, reduces administrative burden for low-dollar purchases, and preserves transparency, competitive opportunity, and Council oversight for higher-value procurements.

Background

Sections **15.1 through 15.4** of the City Charter establish a uniform threshold of **\$2,000** for:

- Requiring sealed proposals for public improvements (Sec. 15.1)
- Requiring competitive bidding for material purchases (Sec. 15.2)
- Requiring Council approval of specifications for public improvements (Sec. 15.3)
- Requiring Council approval of contracts exceeding \$2,000 (Sec. 15.4)

These thresholds were last amended in **1997**, nearly three decades ago. Since that time:

- Costs of materials, equipment, and services have increased substantially

- Peer municipalities have modernized their procurement thresholds
- The City’s operational needs have expanded
- The \$2,000 limit now captures routine, low-risk purchases that previously fell below the threshold.
- As a result, the current Charter language creates unnecessary administrative workload and delays for minor purchases that do not warrant formal bidding or Council action.

Peer Municipal Comparison

A review of similarly sized Michigan cities shows that Taylor’s \$2,000 threshold is significantly lower than common practice. Most peer cities require formal bidding at **\$5,000–\$30,000**, and none use a threshold as low as \$2,000.

Typical Michigan Practice

- Micro-purchase thresholds commonly range from **\$3,000–\$5,000**
- Informal quotes are typically required between **\$5,000–\$25,000**
- Formal sealed bids generally begin between **\$10,000–\$30,000**
- Council approval is tied to higher-value procurements, not routine purchases

Taylor’s \$2,000 limit is now an outlier and creates a disproportionate administrative burden compared to peer cities.

Issues With the Current \$2,000 Threshold

1. Administrative Burden

Routine purchases such as small tools, replacement parts, safety equipment, and minor repairs frequently exceed \$2,000. Requiring competitive bidding and Council approval for these low-risk items slows operations and increases staff workload.

2. Misalignment With Modern Costs

Inflation and market changes since 1997 mean that many basic operational purchases now exceed \$2,000. The threshold no longer reflects realistic pricing.

3. Inefficient Use of Council Time

Council agendas regularly include low-dollar, low-risk items that could be handled administratively if the threshold were modernized.

4. Reduced Operational Flexibility

Departments must delay minor work or purchases while awaiting bid cycles or Council meetings, even when the need is urgent and the cost is modest.

Recommended Charter Update

The Purchasing Department recommends updating all Charter references to **\$2,000** in Sections 15.1–15.4 to **\$5,000**.

Updated Threshold Language (Conceptual)

- **Section 15.1:** Require sealed proposals for public improvements exceeding **\$5,000**
- **Section 15.2:** Require competitive bidding for material purchases exceeding **\$5,000**
- **Section 15.3:** Require Council-approved specifications for public improvements exceeding **\$5,000**
- **Section 15.4:** Require Council approval for contracts exceeding **\$5,000**

This update preserves all existing safeguards—competitive bidding, Council approval, bonding requirements, and anti-splitting protections—while modernizing the dollar amount to reflect current economic conditions.

Rationale for Increasing the Threshold to \$5,000

1. Aligns With Peer Cities

A \$5,000 threshold is consistent with regional norms and modern municipal procurement standards.

2. Reduces Administrative Burden

Raising the threshold allows staff to process routine, low-risk purchases more efficiently while focusing formal processes on higher-value procurements.

3. Maintains Transparency and Oversight

All competitive bidding, Council approval, and bonding requirements remain intact—only the dollar amount triggering them changes.

4. Improves Operational Responsiveness

Departments can address minor repairs, equipment needs, and operational issues more quickly without compromising accountability.

5. Reflects Modern Pricing

A \$5,000 threshold better matches current costs for materials, tools, and small public-improvement work.

Conclusion

Updating the Charter's purchasing thresholds from **\$2,000 to \$5,000** is a practical, responsible modernization that aligns Taylor with peer municipalities, reduces administrative burden, and maintains strong fiscal oversight. The Purchasing Department recommends that the City Council approve the proposed Charter amendment to update Sections 15.1 through 15.4 accordingly.

Best Regards,

Ben Clayton Jr. MA
Purchasing Manager
City of Taylor

Motion to approve to direct Corporation Counsel to draft a ballot proposal to amend the City of Taylor Charter, Chapter 15, Section 15.2, Section 15.3, and Section 15.4 (a) changing each “limit” from \$2,000 to \$5,000, per the attached document.

Attached documentation:

ATTACHED “Threshold Recommendations.docx”

Section 15.2. Material Purchases.

No contract shall be entered into by the City for the purchase of any material, tools, apparatus or any other thing or things, the consideration or cost of which shall exceed ~~Two Thousand (\$2,000.00)~~ **Five Thousand (\$5,000.00)** Dollars until after an opportunity for competitive bidding; the City shall have the right to reject any or all proposals.

Section 15.3. Specifications.

No public improvement costing more than ~~Two Thousand (\$2,000.00)~~ **Five Thousand (\$5,000.00)** Dollars shall be contracted for/or commenced until drawings, profiles and estimates for the same shall have been submitted to the Council and approved by it; and a copy thereof shall thereafter remain on file in the office of the Clerk subject to inspection by the public.

Section 15.4. Approval of Contract: Review: Appropriation.

(a) No contract shall be entered into by the City for the making of any public improvement or for the purchase of any materials, tools, apparatus, or any other thing or things, the consideration or cost of which shall exceed ~~Two Thousand (\$2,000.00)~~ **Five Thousand (\$5,000.00)** Dollars until the same shall have been approved by a majority of the Council elect. All contracts prior to submission to the Council shall be reviewed by the Mayor. No bids shall be received unless accompanied by a bid bond in the sum of five percent (5%) of the bid nor shall any contract be entered into unless accompanied by a performance and material bond in the amount of the contract.

(b) No contract shall be made with any person who is in default to the City.

(c) No public work or improvement shall be commenced, nor any contract therefor be let or made, until a valid specific appropriation to pay the cost thereof shall have been made by the Council from funds on hand and legally available for such purpose, or until a tax or assessment shall have been levied or bonds authorized and sold to pay the cost and expense thereof.

CITY OF TAYLOR

RESOLUTION

RESOLUTION IN OPPOSITION TO THE PASSAGE OF MICHIGAN HOUSE OF REPRESENTATIVE BILLS 5529-5532, 5581-5585 REGARDING LOCAL MUNICIPAL ZONING AUTHORITY

At a regular meeting of the CITY COUNCIL of THE CITY OF TAYLOR held on the 7th day of April 2026, the following resolution was adopted by the CITY COUNCIL:

WHEREAS, House Bills 5529-5532 and 5581-5585 have been introduced in the State of Michigan House of Representatives (hereinafter “bills”), and;

WHEREAS, the above referenced bills have been referred to the State House of Representatives committees for consideration, and;

WHEREAS, the bills, if enacted into law, would impose new requirements for studies and documents for city site plan review that will interrupt or otherwise slow down the City’s well-established development plan review process, and;

WHEREAS, if enacted into law would restrict minimum home square footage requirements and add immense pressure on local infrastructure improvements such as water and sewer, and;

WHEREAS, if enacted into law would change parking and mobile home requirements that would disrupt decades of planning that shaped the City’s current residential zoning, and;

WHEREAS, if enacted into law would restrict a local community from deciding the distance between residential homes, and;

WHEREAS, if enacted into law would permit duplexes on parcels currently zoned for single-family residential, and;

WHEREAS, Michigan law and the Michigan Zoning Enabling Act have delegated local Municipal Zoning regulations to cities and townships and the bills would be in conflict with the authority of local units of government to provide local zoning ordinances for the regulation of land development under MCL 125.3201(1);

WHEREAS, local communities can best determine orderly development plans for their city or township and, in turn, enact zoning regulations best suited for its residents and;

WHEREAS, local communities are best situated to assess infrastructure capabilities, such as water and sewer capacity, so that future development may be properly planned and;

WHEREAS, local communities can best determine limits on land use so as to address the requirements of those willing to invest while ensuring the health, safety and enjoyment of the residents that will call the community home and;

NOW, THEREFORE BE IT RESOLVED that the CITY COUNCIL of the CITY OF TAYLOR opposes passage of House Bills 5529-5532 and 5581-5585 on the basis that local municipalities are best situated to enact zoning regulations to ensure the health, safety and welfare of their residents, and;

BE IT FURTHER RESOLVED, that a copy of this Resolution be distributed to all state legislators representing the residents of the CITY OF TAYLOR.

AYES:

NAYS:

ABSENT:

I, Cynthia A. Bower, Clerk of the City of Taylor, do hereby certify that the foregoing is a true and correct copy of a Resolution adopted by the Taylor City Council at its regular meeting held on April 7, 2026.

Cynthia A. Bower
City Clerk
City of Taylor

Dated: _____